

380 KING STREET - CONDOMINIUMS

SITE PLAN APPLICATION 9/17/25

COVER SHEET

PREPARED BY NARRAGANSETT ENGINEERING INC.

PROJECT SUMMARY 380 KING STREET - CONDOMINIUMS

TO REDEFINE EXISTING PARCEL LOT LINES ADJACENT TO THE SUBJECT PARCEL AND TO PROVIDE RESIDENTIAL HOUSING

SHEET INDEX

1. SV-300: EASEMENT PLAN
2. C-000 : EXISTING CONDITIONS PLAN
3. C-001 : EROSION CONTROL AND DEMO PLAN
4. C-100 : PROPOSED OVERVIEW PLAN
5. C-101 : PROPOSED SITE PLAN
6. C-102 : PROPOSED GRADING AND UTILITY PLAN 1
7. C-103 : PROPOSED GRADING AND UTILITY PLAN 2
8. C-104 : DRAINAGE OVERVIEW PLAN
9. C-105 : DRAINAGE DETAIL PLAN
10. C-200 : DETAILS 1
11. C-201 : DETAILS 2
12. C-202 : DETAILS 3
13. C-203 : DETAILS 4
14. C-204 : DETAILS 5
15. C-205 : SOIL PROFILES & BMP DETAILS
16. L-1 : LANDSCAPE PLAN
- 17-18. RAB-1-2 : PHOTOMETRIC PLANS

WAIVER REQUEST SECTION 185-21 Part B Section (3)(a)(i) - PARKING SPACES

REQUIRED 60 Spaces
 PROVIDED : 59 Spaces
 (3)(a)(i) Parking schedule. The number of parking spaces required for a particular use shall be as follows:
 All other zoning districts:
 Residential buildings:
 Dwelling units, regardless of the number of bedrooms: two spaces.

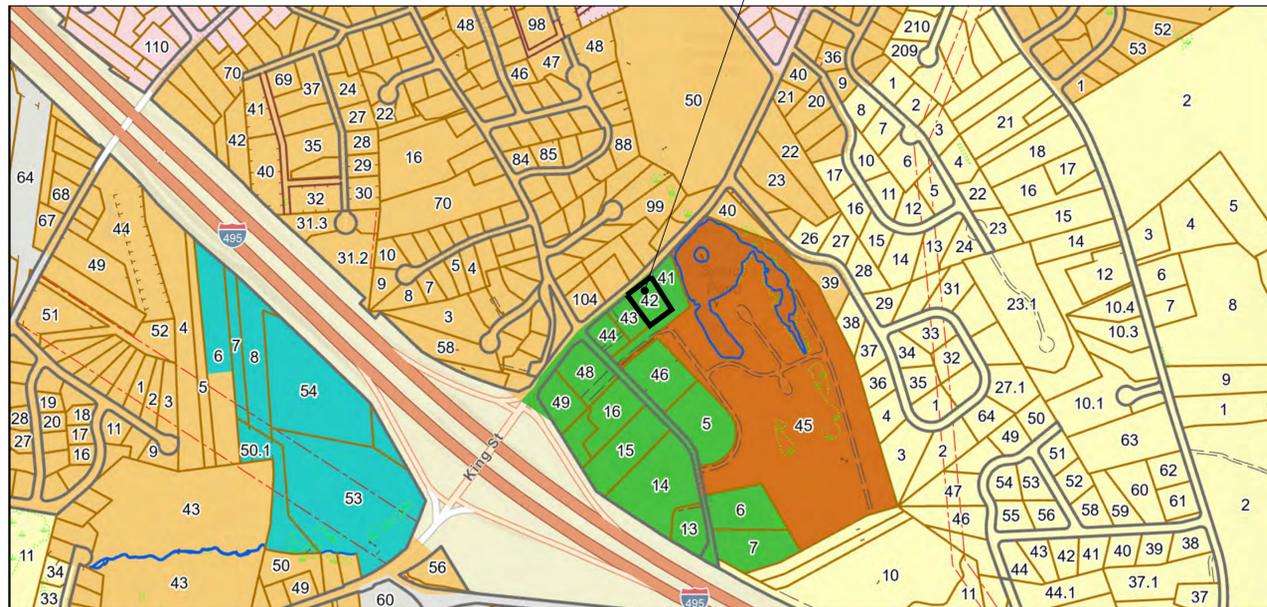
WAIVER REQUEST SECTION 300-11 Part B Section (2)(a) - STORMWATER PIPE MATERIAL AND COVER

REQUIRED RCP piping with 42" min. cover
 PROVIDED : HDPE piping 12" min. cover
 (B)(2)(a) The drainage pipe shall be reinforced concrete, with bell and spigot gasketed joints. The pipe shall be Class III in accordance with ASTM C-76. The gaskets shall be O-ring type in accordance with ASTM C-443. The minimum diameter shall be 12 inches. The pipe shall be laid in undisturbed trenches below the grade of pipes, starting with the downstream end on a firm bedding. All bells shall be facing upstream. Reference bench marks shall be clearly marked to enable the Department of Public Works Director to check the grade and invert elevations. he joints of all concrete pipes shall include a pre-molded neoprene continuous O-ring flexible compression gasket. No backfilling of pipes of culverts shall be done until the installation has been inspected and approved by the Department of Public Works Director. Backfilling shall be in layers not exceeding 12 inches, with each layer compacted by an appropriately sized plate vibrator, regardless of the method of final compaction at the subbase or gravel base level. The minimum cover is 42 inches above the top of pipe.

CONSULTANTS + REPORTS:

- SURVEY : COMMONWEALTH LAND SURVEYORS (CLS)
- SITE CIVIL : NARRAGANSETT ENGINEERING INC (NEI)
- ARCHITECTS : BRIAN FRIGON
- LANDSCAPE ARCHITECTS : NARRAGANSETT ENGINEERING INC (NEI)/ GARDNER + GERRISH (GG)
- PHOTOMETRIC: RAB LIGHTING (RAB)
- BIOLOGY REPORT (WETLANDS) AVIZINIS ENVIRONMENTAL SERVICES INC (AES)

SITE LOCUS MAP
 PER TOWN OF FRANKLIN GIS
 1" = 80'



District	Area (square feet)	Minimum Lot Dimensions			Minimum Yard Dimensions			Maximum Height of Building		Maximum Impervious Coverage of Existing Upland	
		Continuous Frontage (feet)	Depth (feet)	Lot Width (minimum circle diameter)	Front (feet)	Side (feet)	Rear (feet)	Stories	Feet	Structures	Structures Plus Paving ¹
Rural Residential I	40,000	200	200	180 ²	40	40	40	3	35	20	25
Residential VI	40,000	200	200	180 ²	40	40	40	3	35	20	25
Residential VII ¹¹	40,000	200	200	180 ²	40	40	40	3	35	20 ¹²	25 ¹²
Rural Residential II	30,000	150	200	135 ²	40	35	35	3	35	20	25
Single-Family Residential III	20,000	125	160	112.5	40	25	30	3	35	25	35
Single-Family Residential IV	15,000	100	100	90	30	20	20	3	35	30	35
General Residential V	10,000	100	100	90	20	15	20	3	40	45	55
Neighborhood Commercial	18,000	100	100	90	20	30	40	3	35	30	35
Rural Business ¹³	40,000	200	200	180	40	30	40	1.5	30	10	30
Downtown Commercial	5,000	50	50	45	5 ¹⁰	0 ²	15	3 ⁹	40 ⁷	80	90
Commercial I ⁷	5,000	50	50	45	20 ¹⁴	10 ¹⁴	15	3 ¹⁵	40 ¹⁵	80	90
Commercial II	40,000	175	200	157.5	40	30	30	3 ¹⁵	40 ¹⁵	70	80

Principal Uses (cont'd)	DISTRICT										
	RRI	RRII	RVI	RVII	SFRIII	SFRIV	GRV	NC	RB	CI	CH
6. Residential											
6.1 Multifamily or apartment											
a. With four or more housing units ⁴	N ¹	N	N	N	Y ^{2,3}	PB ^{2,4}	N	Y ^{3,3}	N	Y ³	N
b. With three housing units	N	N	N	PB ²	Y ²	Y ²	N	Y ²	Y	Y	Y
6.2 Single-family	Y	Y	Y	Y	Y	Y	Y	Y	Y	Y	Y
6.3 Two-family											
a. New	N	N	N	Y ²	Y ²	Y ²	N	Y	Y	Y	Y
b. By conversion	BA	BA	BA	BA	Y	Y	N	BA	Y	Y	Y

- NOTES:
¹ Except PB in RVI District per § 185-38, and RVII District per § 185-50.
² Lot area must be at least 25% greater than that required for a single-family dwelling.
³ No more than one dwelling unit per 2,250 square feet of lot area may be permitted; additional dwelling units may be allowed by spec Board.
⁴ All multifamily developments with 10 or more housing units are required to address the affordable housing requirements in § 185-51



380 KING ST - CONDOMINIUMS COVER SHEET

Property Record:
 Principal Parcel:
 380 King St, Franklin, MA 02038
 Plat: 303, Lot:42, Area: 0.919 Acres, Zone: C-II
 N/F: Marguerite Margaret, TR, Marguerite Family Trust
 Book/Page: 8145-260
 Also Part of Subject Application:
 370 King St, Franklin, MA 02038
 Plat: 303, Lot:41, Area: 1.8 Acres, Zone: C-II
 N/F: Marguerite Margaret, TR, Marguerite Family Trust
 Book/Page: 8145-260
 390 King St, Franklin, MA 02038
 Plat: 303, Lot:43, Area: 0.919 Acres, Zone: C-II
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 Plat: 303, Lot:44, Area: 0.928 Acres, Zone: C-II
 N/F: DECOY INC.

PROJECT NO.	DATE	BY
24.0168	10.10.24	NKH

DRAWING ISSUE:	
<input type="checkbox"/>	CONCEPT / DISCUSSION
<input checked="" type="checkbox"/>	PERMITTING
<input type="checkbox"/>	CONSTRUCTION
<input type="checkbox"/>	AS-BUILT
<input type="checkbox"/>	OTHER

ONLY PLANS ISSUED FOR CONSTRUCTION SHALL BE USED FOR CONSTRUCTION

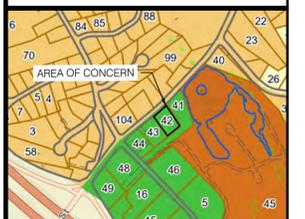
INTERNAL REVIEW

No	CHECK	CAD	DESCRIPTION/NOTES
1	BY DATE	EB 11.26.24	PARKING LAYOUT
2	BY DATE	EB 2.28.25	STORMWATER DESIGN
3	BY DATE	EB 3.11.25	UTILITY REVIEW PENDING
4	BY DATE	EB 3.19.25	GENERAL PLAN REVISIONS
5	BY DATE	EB 3.19.25	GENERAL AND STORMWATER PLAN REVISIONS
6	BY DATE	EB 4.28.25	STORMWATER PLAN REVISIONS
7	BY DATE	EB 5.1.25	COORDINATION MEETING
			REVISIONS: EASEMENT PLAN
			APPARATUS CIRCULATION PLAN
			WATER LINE DESIGN.
			LANDSCAPE REVISIONS
7	BY DATE	EB 5.7.25	STORMWATER DIVERSION STRUCTURE REVISIONS

FORMAL PLAN REVISIONS

No	DATE	STAGE/DESCRIPTION	BY
1	7.11.25	EXPANDED COURTYARD ADDED RETAINING WALL	TB
2	7.23.25	CLOSED FRONT ENTRANCE ADDED BIKE RACK, UPDATED CIRCULATION PLAN	EB
3	8.5.25	ADDED TRAFFIC CIRCULATION TO OVERVIEW	EB
4	8.14.25	REVISED TRAFFIC CIRCULATION ON OVERVIEW	JM
5	9.4.25	PEER REVIEW REVISIONS	EB
6	10.2.25	PEER REVIEW 2ND ROUND REVISIONS	EB
7	10.9.25	REVISED SEWER CONNECTION LOCATION	JM

ne-i-cds.com
 DRAWINGS MUST BE PRINTED IN COLOR TO BE VALID. THIS NOTE SHOULD BE BLUE. IF THIS NOTE IS NOT BLUE, PLEASE REPRINT IN COLOR OR CONTACT NEI.



COVER



PLAT 303 LOT 45
N/F: SPRUCE POND
VILLAGE
13 SPRUCE POND ROAD
33.78 ACRE

ZONE RESIDENTIAL VI ZONE RESIDENTIAL VI ZONE RESIDENTIAL VI
ZONE COMMERCIAL II ZONE COMMERCIAL II ZONE COMMERCIAL II

NOTES:

1. A "TRAVEL" EASEMENT SHALL BE PROVIDED FOR THE SHARED ACCESS AISLES BETWEEN PLAT 303 LOT 41, 42, AND 43 FOR TRAVEL EGRESS PRIOR TO FINAL RECORDING.
2. UTILITY EASEMENTS FOR MEANS OF ACCESS AND MAINTENANCE BETWEEN PLAT 303 LOT 41, 42, AND 43 SHALL BE PROVIDED PRIOR TO FINAL RECORDING.
3. PLOTTED IN COLOR FOR PRESENTATION PURPOSES ONLY.

NEI
Narragansett
Engineering Inc.
Civil - Survey Structural Environmental Design
3102 East Main Road, Portsmouth RI 02871
Tel. 401.683.6630 www.nei-cds.com

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380 KING ST - CONDOMINIUMS
EASEMENT PLAN

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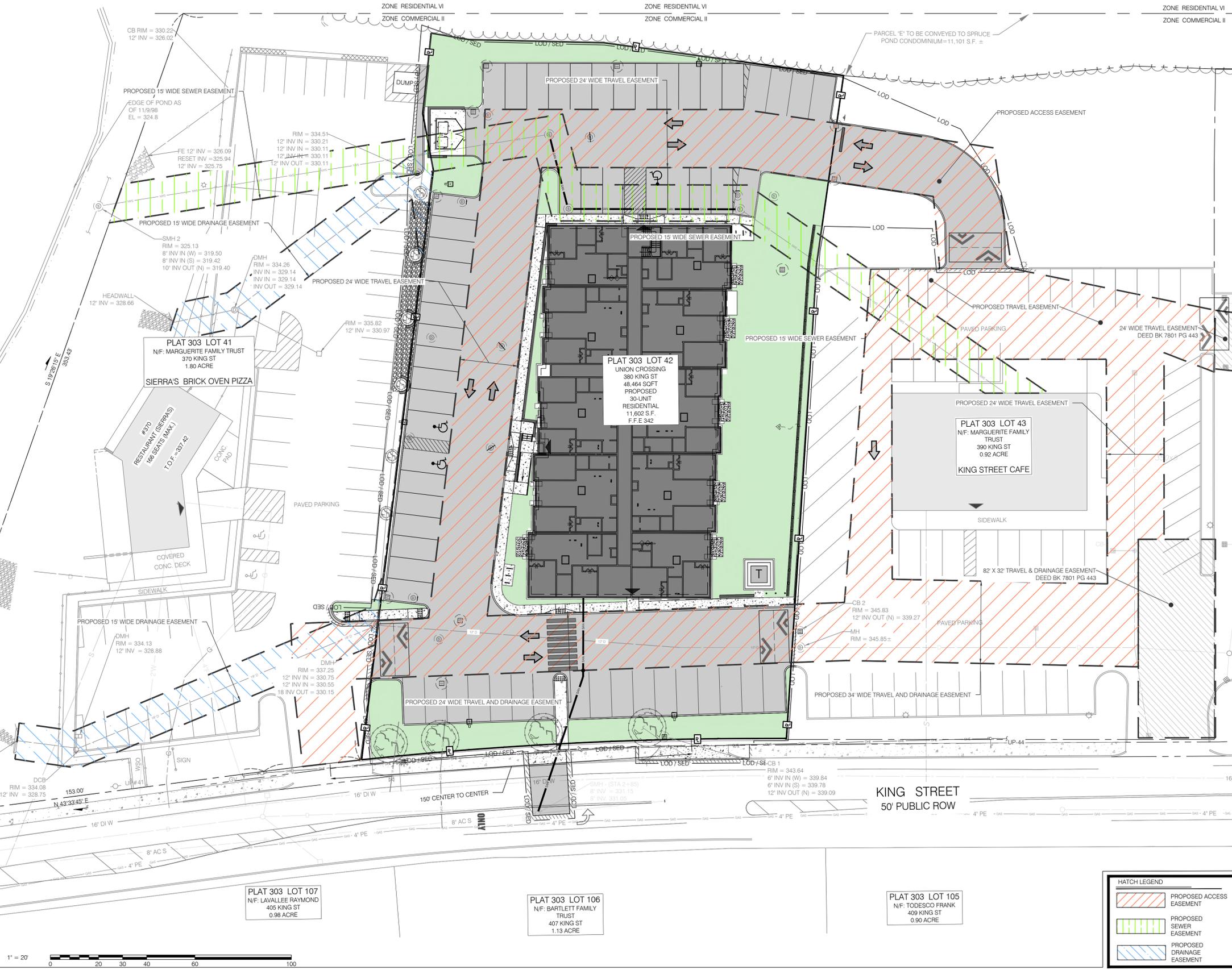
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LOCUS MAP

AREA OF CONCERN

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SCALE 1"=20' SV-300



LEGEND

100.00'	DIMENSION - EXISTING
100.00'	DIMENSION - PROPOSED
100.00' (D)	PLAN / DEED DIMENSION
100.00' (S)	SURVEY DIMENSION
---	PROPERTY LINE - ABUTTING
---	PROPERTY LINE - EXISTING
---	PROPERTY LINE - PROPOSED
---	ETXBCKS
---	GRADE CONTOUR - EXISTING
---	GRADE CONTOUR - PROPOSED
---	ELECTRIC - OVERHEAD (OHE)
---	ELECTRIC - TELEPHONE - CABLE (ETC)
---	ELECTRIC - UNDERGROUND (UGE)
---	GAS (G)
---	SANITARY SEWER (S)
---	STORM DRAIN (SD)
---	WATER
---	LIMIT OF DISTURBANCE (LOD)
---	SEDIMENT CONTROL (SED)
---	LOD / SED
---	EDGE OF PAVEMENT - EXISTING
---	FENCE - METAL
---	FENCE - WOOD
---	STONE WALL
---	BRUSH LINE (APPROXIMATE)
---	WETLAND LIMIT
[Symbol]	CATCH BASIN
[Symbol]	DRAINAGE MANHOLE
[Symbol]	SANITARY MANHOLE
[Symbol]	ELECTRICAL MANHOLE
[Symbol]	TELEPHONE MANHOLE
[Symbol]	WELL
[Symbol]	GATE VALVE
[Symbol]	WATER SHUT OFF
[Symbol]	FIRE HYDRANT
[Symbol]	ELECTRIC BOX (ETC)
[Symbol]	UTILITY POLE
[Symbol]	DOWNSPOUT
[Symbol]	BENCHMARK
[Symbol]	DRILL HOLE
[Symbol]	SPIKE
[Symbol]	STRUCTURE, EXISTING
[Symbol]	STRUCTURE, PROPOSED
[Symbol]	SPOT GRADE - EXISTING
[Symbol]	SPOT GRADE - PROPOSED
[Symbol]	EXISTING TREE
[Symbol]	PROPOSED TREE
[Symbol]	GRANITE BOUND
[Symbol]	REBAR / STEEL PIPE FOUND
[Symbol]	WETLAND FLAG
[Symbol]	COASTAL FEATURE FLAG
[Symbol]	COASTAL BUFFER POST
[Symbol]	FLOW ARROW
[Symbol]	SOIL EVALUATION

HATCH LEGEND

- [Hatched Box] PROPOSED ACCESS EASEMENT
- [Hatched Box] PROPOSED SEWER EASEMENT
- [Hatched Box] PROPOSED DRAINAGE EASEMENT

1" = 20'

N:\PROJECTS\24.0168 - MARGUERITE COMMONWEALTH UNION CROSSING\SITE CIVIL\24.0168 FRANKLIN CROSSING CIVIL.DWG SV-300 NEI-Standard.cd 10/9/2025 Joni Malo



PLAT 303 LOT 45
N/F: SPRUCE POND
VILLAGE
13 SPRUCE POND ROAD
33.78 ACRE

- SURVEY NOTES:**
1. SURVEY OF EXISTING CONDITIONS AND PARCEL SUBDIVISION PROVIDED BY COMMONWEALTH LAND SURVEYORS INC. REFER TO PLAN NAME UNION CROSSING SUBDIVISION.
 2. ZONING INFORMATION SHOW FROM RECORD INFORMATION, MAPS AND / OR GIS. ZONING DATA MAY VARY BASED ON USE, LOT SIZE, ORIENTATION AND OTHER FACTORS AND IS SHOWN FOR REFERENCE INFORMATION ONLY.
 3. ZONING DATA MUST BE CONFIRMED WITH ZONING OFFICIAL AND / OR LEGAL COUNSEL FOR USE IN DESIGN OR PERMITTING.
 4. THE SUBJECT PARCEL MAY BE OPERATING UNDER AN EXISTING SPECIAL USE PERMIT. NEI SUGGESTS A REVIEW OF ZONING WITH COUNSEL AND/OR ZONING OFFICER FOR CLARIFICATION ON PERMITTING PROCESS FOR IMPROVEMENTS.
 5. PLAN IS NOT AS-BUILT UTILITY PLAN.
 6. ABUTTING PROPERTY LINES SHOWN APPROXIMATELY PER TOWN GIS OR BEST AVAILABLE INFORMATION.
 7. CONTRACTOR TO CALL DIG SAFE AND/OR APPLICABLE UTILITY COMPANIES PRIOR TO ANY CONSTRUCTION. DIG SAFE TEL # 1-800-344-7233 (1-800-DIG-SAFE).
- UTILITY NOTES:**
- A. WATER - SURFACE FEATURES PER FIELD INVESTIGATION MARCH 10TH AND FRANKLIN GIS PER MICHAEL MAGLIO TOWN OF FRANKLIN WATER AND SEWER DIVISION CORRESPONDENCE MARCH 6TH
 - B. SEWER - SURFACE FEATURES PER FIELD INVESTIGATION MARCH 10TH AND FRANKLIN GIS PER MICHAEL MAGLIO TOWN OF FRANKLIN WATER AND SEWER DIVISION CORRESPONDENCE MARCH 6TH
 - C. GAS - SURFACE FEATURES PER FIELD INVESTIGATION MARCH 10TH AND EVERSOURCE GIS PER ADAM BISTRAN CORRESPONDENCE MARCH 7TH
 - D. ELECTRIC - SURFACE FEATURES PER FIELD INVESTIGATION MARCH 10TH
 - E. STORM DRAIN - SURFACE FEATURES PER FIELD INVESTIGATION MARCH 10TH AND PER MICHAEL MAGLIO TOWN OF FRANKLIN WATER AND SEWER DIVISION CORRESPONDENCE MARCH 6TH
 - F. SEWER INFORMATION IN KING STREET TAKEN FROM RECORD DRAWING KING STREET STA. 1+10 TO STA. 11+33 DATED JULY 1977 BY CAMP DRESSER & MCKEE INC.

NEI
Narragansett
Engineering Inc.

Civil - Survey Structural Environmental Design
3102 East Main Road, Portsmouth RI 02871
Tel. 401.683.6630 www.nei-cds.com

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380 KING ST - CONDOMINIUMS
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refer to plan by Commonwealth Land Surveyors / NEI for all property boundary, monuments, and requisite land survey information. Boundary lines are displayed from this plan

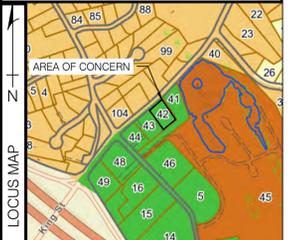
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SCALE
1"=20'
C-000

N:\PROJECTS\24.0168 - MARGUERITE COMMONWEALTH (UNION CROSSING)\SITE CIVIL\24.0168 FRANKLIN CROSSING CIVIL.DWG C-000 NEI-Standard.ctb 10/09/2025 Job Malo





PLAT 303 LOT 45
N/F: SPRUCE POND
VILLAGE
13 SPRUCE POND ROAD
33.78 ACRE

DEMOLITION NOTES:

1. THE CONTRACTOR SHALL COORDINATE ALL DEMOLITION OF STRUCTURES, PAVEMENT, CONCRETE MATERIALS AND UTILITIES WITH THE PROPOSED SITE, UTILITY, LANDSCAPING, AND ARCHITECTURAL DRAWINGS
2. ALL NOTED UTILITIES TO BE REMOVED AND DISPOSED OF, RELOCATED OR CAPPED REPRESENT ALL KNOWN SITE CONDITIONS TO BE DEMOLISHED. CONTRACTOR SHALL COORDINATE ALL UNFORESEEN CONDITIONS WITH THE PROJECT ENGINEER, ARCHITECT, OWNER REPRESENTATIVE AND/OR AFFECTED UTILITIES COMPANY
3. THERE SHALL BE NO INTERRUPTION OF UTILITY SERVICE DURING THE CONSTRUCTION PROCESS WITHOUT THE WRITTEN PERMISSION OF THE OWNER OR OWNERS REPRESENTATIVE
4. WATER, SEWER, DRAINAGE AND OTHER SITE UTILITIES SERVING THE EXISTING FACILITIES ARE TO REMAIN ACTIVE UNTIL PROPOSED IMPROVEMENTS ARE CONSTRUCTED AND ONLINE. WHEN CONNECTION TO ACTIVE OR RELOCATED UTILITIES, SHUTDOWNS SHALL BE MINIMIZED AND COMPLETED OFF HOURS AND COORDINATED WITH THE OWNER OR OWNER REPRESENTATIVE.
5. THE CONTRACTOR SHALL INSPECT AND REHABILITATE THE EXISTING STORMWATER OUTFALLS TO SPRUCE POND AS REQUIRED.

NEI
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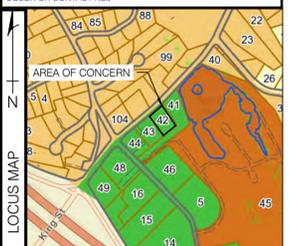
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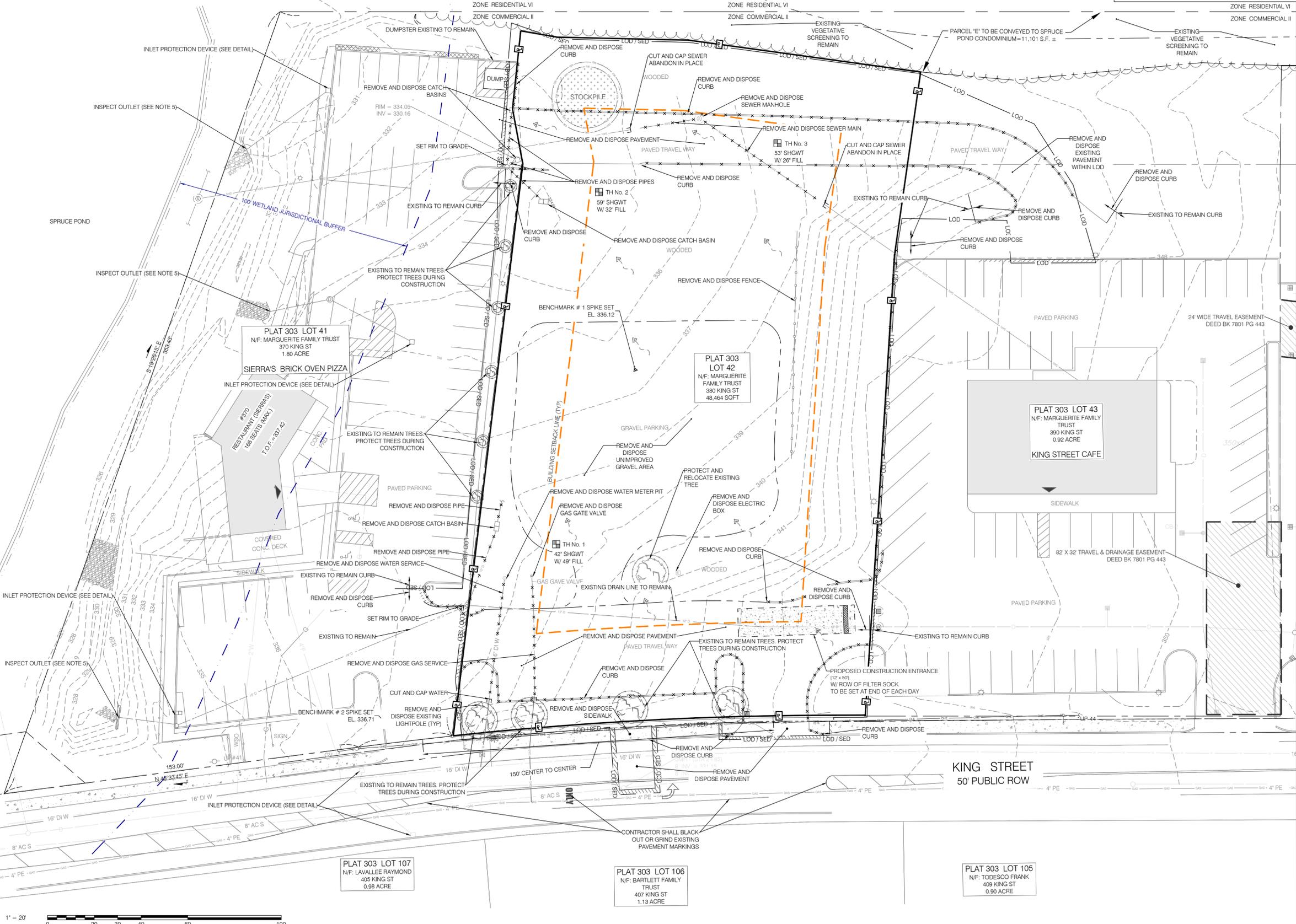
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SCALE 1"=20'

C-001



N:\PROJECTS\24.0168 - MARGUERITE COMMONWEALTH (UNION CROSSING)\SITE CIVIL\24.0168 FRANKLIN CROSSING CIVIL.DWG C-001 NEI Standard.cdw 10/09/2025 Job Malo

1" = 20'

0 20 40 60 80 100



380 KING ST - CONDOMINIUMS
PROPOSED
OVERVIEW PLAN

Property Record:
 Principal Parcel:
 380 King St, Franklin, MA 02038
 Plat: 303, Lot:42, Area: 0.919 Acres, Zone: C-II
 N/F: Marguerite Margaret, TR, Marguerite Family Trust
 Book/Page: 8145-260
 Also Part of Subject Application:
 370 King St, Franklin, MA 02038
 Plat: 303, Lot:41, Area: 1.8 Acres, Zone: C-II
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 Book/Page 8145-260
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 Plat: 303, Lot:43, Area: 0.919 Acres, Zone: C-II
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 Book/Page 8145-260
 400 King St, Franklin, MA 02038
 Plat: 303, Lot:44, Area: 0.928 Acres, Zone: C-II
 N/F: DECOY INC.

PROJECT NO.	DATE	BY
24.0168	10.10.24	NKH

DRAWING ISSUE:

<input type="checkbox"/>	CONCEPT / DISCUSSION
<input checked="" type="checkbox"/>	PERMITTING
<input type="checkbox"/>	CONSTRUCTION
<input type="checkbox"/>	AS-BUILT
<input type="checkbox"/>	OTHER

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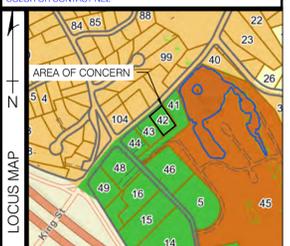
INTERNAL REVIEW

No	CHECK	CAD	DESCRIPTION/NOTES
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2	BY DATE	EB 2.28.25	STORMWATER DESIGN UTILITY REVIEW PENDING
3	BY DATE	EB 3.11.25	GENERAL PLAN REVISIONS
4	BY DATE	EB 3.19.25	GENERAL AND STORMWATER PLAN REVISIONS
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6	BY DATE	EB 5.1.25	COORDINATION MEETING REVISIONS: EASEMENT PLAN, APPARATUS CIRCULATION PLAN, WATER LINE DESIGN, LANDSCAPE REVISIONS
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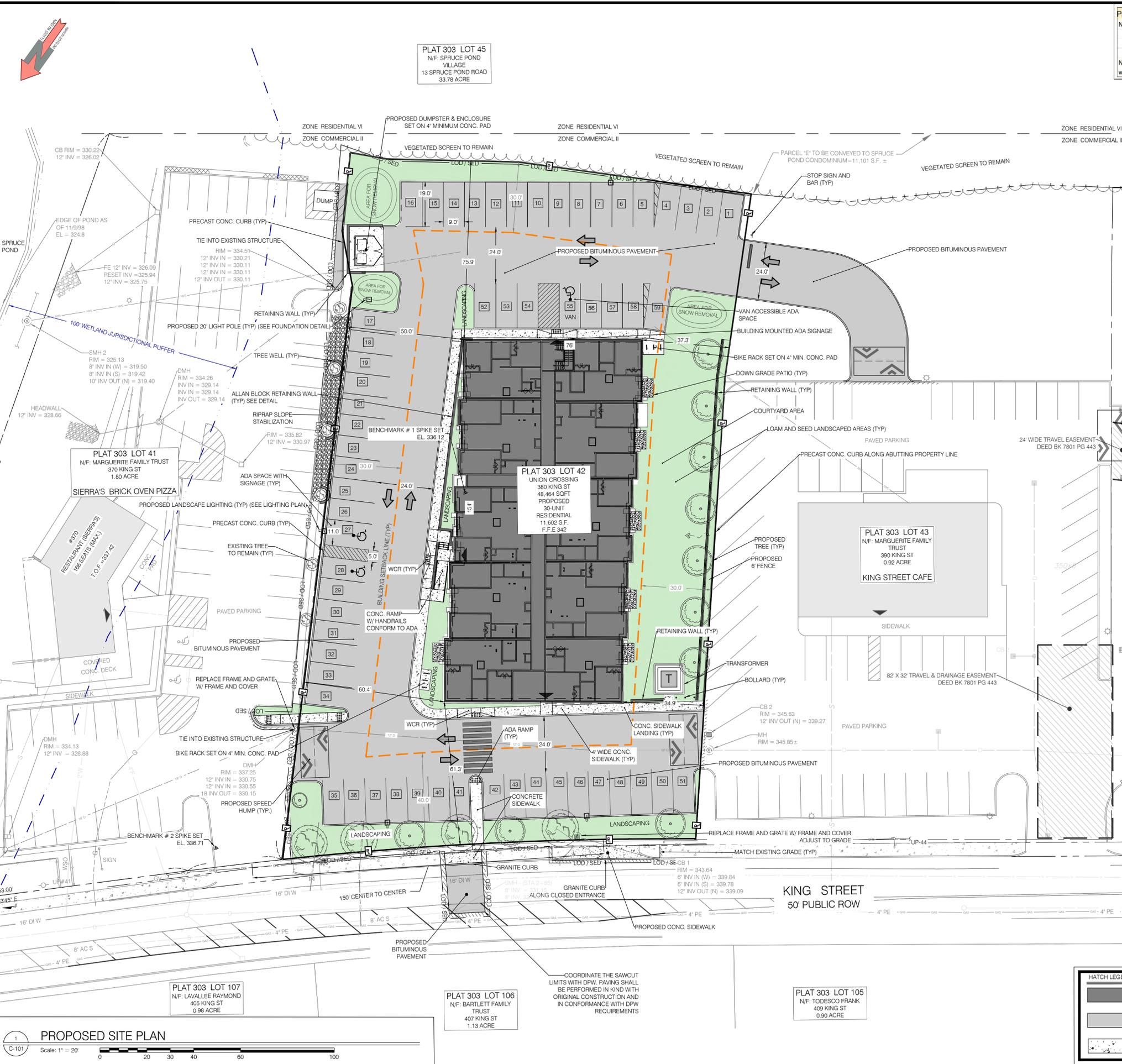
SCALE
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C-100

N:\PROJECTS\24.0168_MARGUERITE-COMMONWEALTH-UNION-CROSSING\CIVIL\24.0168_FRANKLIN-CROSSING-CIVIL.DWG C-100_NEI-Standard.ctb 10/9/2025 Job Malo

1" = 30'
 0 30 45 60 90 150



N:\PROJECTS\24.0168 MARGUERITE COMMONWEALTH UNION CROSSING CIVIL\24.0168 FRANKLIN CROSSING CIVIL\DWG C-101 NEI Standard.cdw 10/09/2025 Job Malo



Proposed Parking Calculation (Franklin Zoning Code Ch 185 Sec 21 Part B(3)(a)(i))

No.	Description	Number	Notes
1	Proposed No. of Units	30	2 parking spots required per unit
2	Proposed Parking Spaces Required	60	30*2
3	Proposed Parking Spaces Provided	59	Including 3 ADA spaces (1 Van Accessible)

Note: One parking space has been removed in lieu of a bike rack with capacity for 12 bicycles

24.0168 380 King St Franklin MA PROPOSED LOT COVERAGE TABLE

Description	Area	Unit	Notes
Plat 303 Lot 42	48,464	Sq Ft	Currently Vacant
Pro. Structure	11,618	Sq Ft	Residential Complex
Lot Coverage	23.97%		
CH Zone - Max 70% structural lot coverage 80% impervious			

24.0168 380 King St Franklin MA PROPOSED PAVEMENT COVERAGE TABLE

Description	Area	Unit	Notes
Plat 303 Lot 42	48,464	Sq Ft	Currently Vacant
Proposed Structure	11,618	Sq Ft	Residential Complex
Proposed Pavement	25,296	Sq Ft	Parking Lot, Access Drives, Sidewalks, utility pads
Proposed Landscaping	11,550	Sq Ft	
Pavement Coverage	76.2%		Includes structure
CH Zone - Max 70% structural lot coverage 80% impervious			

24.0168 380 King St Franklin MA A.P. 303 Lot 42 PARCEL / ZONING DATA

Zoning Requirement	Required	Provided
Minimum Lot Area (SF)	40,000	48,464
Minimum Lot Frontage Width (Feet)	175	178.6
Lot Depth (Feet)	200	275
Lot Width (Circle Dia. In Feet)	157.5	162.5
Minimum Front Setback	40	58
Minimum Side Setback	30	35
Minimum Rear Setback	30	65
Max Building Height (Stories)	3	3
Max Building Height (Feet)	40	39.75
District		Notes
Overlay District	Senior Village	
Overlay District	Sign	
Interim Wellhead Protection Area	No	
Zone II	No	

NEI Narragansett Engineering Inc.
 Civil - Survey Structural Environmental Design
 3102 East Main Road, Portsmouth RI 02871
 Tel. 401.683.6630 www.nei-cds.com

SHEET TITLE - CONDOMINIUMS PROPOSED SITE PLAN

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PROJECT NO. DATE BY

24.0168	10.10.24	NKH
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LEGEND

100.00	DIMENSION - EXISTING	---	STRUCTURE, EXISTING
100.00	DIMENSION - PROPOSED	---	STRUCTURE, PROPOSED
100.00 (D)	PLAN / DEED DIMENSION	---	SPOT GRADE - EXISTING
100.00 (S)	SURVEY DIMENSION	---	SPOT GRADE - PROPOSED
---	PROPERTY LINE - ABUTTING	---	EXISTING TREE
---	PROPERTY LINE - EXISTING	---	PROPOSED TREE
---	PROPERTY LINE - PROPOSED	---	GRANITE BOUND
---	SETBACKS	---	REBAR / STEEL PIPE FOUND
---	GRADE CONTOUR - EXISTING	---	WETLAND FLAG
---	GRADE CONTOUR - PROPOSED	---	COASTAL FEATURE FLAG
---	ELECTRIC - OVERHEAD (OHE)	---	COASTAL BUFFER POST
---	ELECTRIC - TELEPHONE - CABLE (ETC)	---	FLOW ARROW
---	ELECTRIC - UNDERGROUND (UGE)	---	
---	GAS (G)	---	
---	SANITARY SEWER (S)	---	
---	STORM DRAIN (SD)	---	
---	WATER	---	
---	LIMIT OF DISTURBANCE (LOD)	---	
---	SEDIMENT CONTROL (SED)	---	
---	LOD / SED	---	
---	EDGE OF PAVEMENT - EXISTING	---	
---	FENCE - METAL	---	
---	FENCE - WOOD	---	
---	STONE WALL	---	
---	BRUSH LINE (APPROXIMATE)	---	
---	WETLAND LIMIT	---	
---	CATCH BASIN	---	
---	DRAINAGE MANHOLE	---	
---	SANITARY MANHOLE	---	
---	ELECTRICAL MANHOLE	---	
---	TELEPHONE MANHOLE	---	
---	WELL	---	
---	GATE VALVE	---	
---	WATER SHUT OFF	---	
---	FIRE HYDRANT	---	
---	ELECTRIC BOX (ETC)	---	
---	UTILITY POLE	---	
---	DOWNSPOUT	---	
---	BENCHMARK	---	
---	DRILL HOLE	---	
---	SPIKE	---	
---	SOIL SHOW	---	
---	SOIL LEDGE	---	
---	SOIL EVALUATION	---	

HATCH LEGEND

---	PROPOSED STRUCTURE
---	PROPOSED PAVEMENT
---	PROPOSED CONCRETE PAD

PROPOSED SITE PLAN
 Scale: 1" = 20'
 0 20 40 60 100

SCALE 1"=20' **C-101**

LOCUS MAP

AREA OF CONCERN

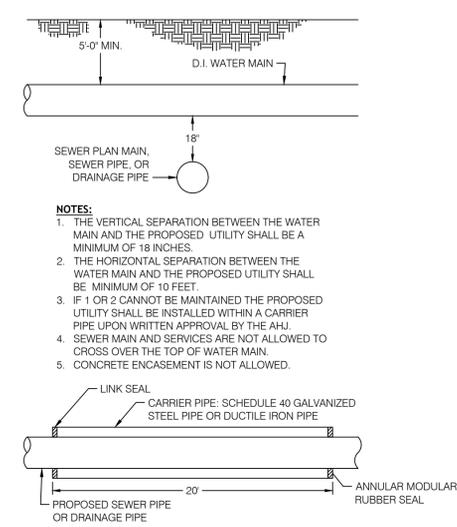
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GRADING AND UTILITIES NOTES:

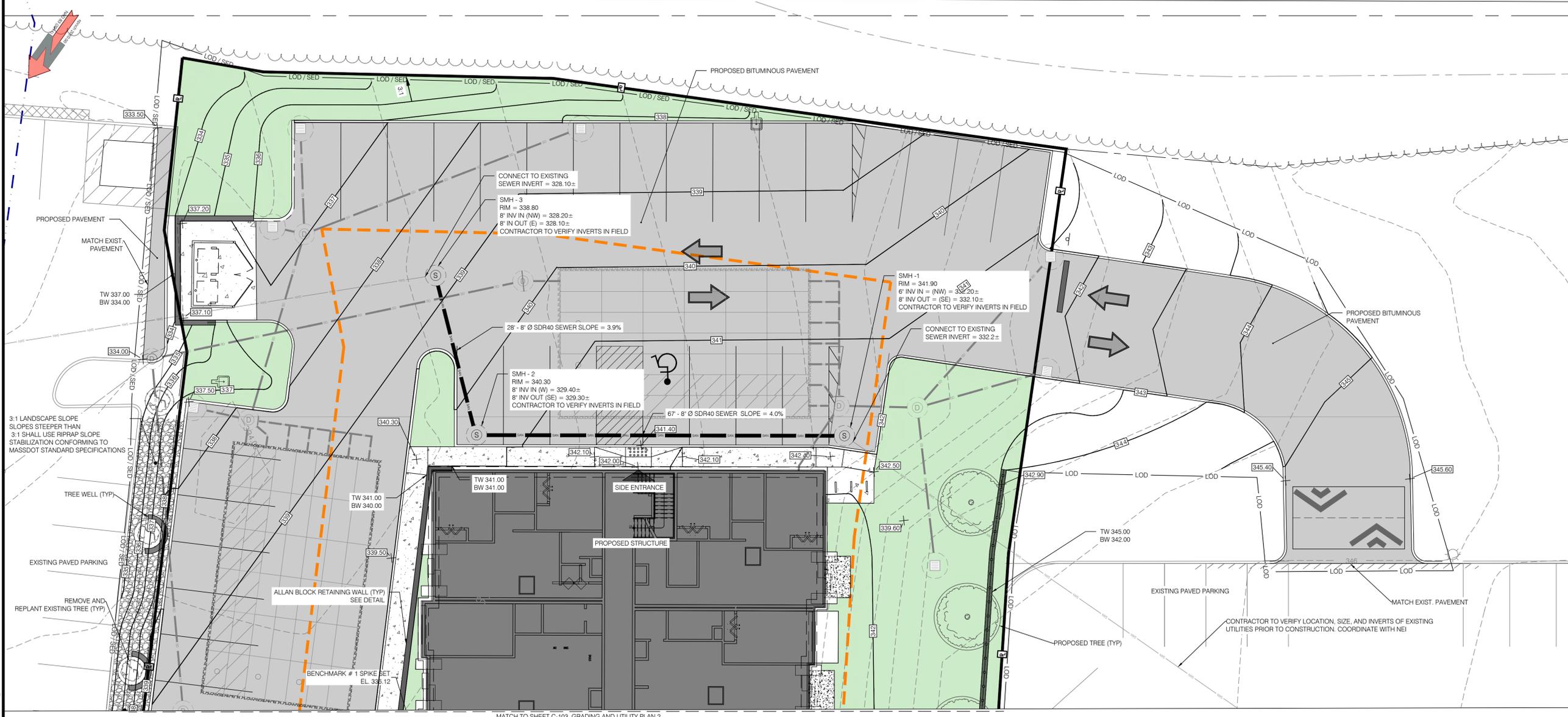
1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THAT THE PROPOSED WORK SHOWN ON THESE PLANS DO NOT CONFLICT WITH ANY EXISTING CONDITIONS OR OTHER PROPOSED WORK. IF CONFLICTS ARISE, THE CONTRACTOR SHALL NOTIFY THE OWNER AND ENGINEER PRIOR TO INSTALLATION OF ANY PORTION OF THE SITE WORK WHICH WOULD BE AFFECTED. NO FIELD ADJUSTMENTS IN THE LOCATION OF SITE ELEMENTS SHALL BE MADE WITHOUT THE ENGINEER'S APPROVAL.
2. WHERE AN EXISTING UTILITY IS FOUND TO CONFLICT WITH WORK, THE LOCATION, ELEVATION, SIZE AND MATERIAL SHALL BE ACCURATELY DETERMINED BY THE CONTRACTOR IMMEDIATELY AND THE INFORMATION FURNISHED TO THE ENGINEER FOR RESOLUTION. THE CONTRACTOR SHALL NOT CONTINUE WORK ON AFFECTED UTILITIES UNTIL THE CONFLICT IS RESOLVED.
3. ALL WORK PERFORMED AND ALL MATERIALS FURNISHED SHALL CONFORM WITH THE LINE AND GRADES ON THE PLANS AND SITE WORK SPECIFICATIONS.
4. AT ALL LOCATIONS WHERE EXISTING CURBING OR PAVEMENT ADJUT NEW CONSTRUCTION, THE EDGE OF THE EXISTING CURB OR PAVEMENT SHALL BE SAW CUT TO A CLEAN, SMOOTH EDGE. BLEND NEW PAVEMENT AND CURBS SMOOTHLY INTO EXISTING BY MATCHING LINES, GRADES AND JOINTS.
5. ALL UTILITY COVERS, GRATES, AND THE LIKE SHALL BE BE FLUSH WITH THE SURROUNDING SURFACE OR PAVEMENT FINISH. RIM ELEVATIONS ARE APPROXIMATE AND FINAL ELEVATIONS ARE TO BE SET FLUSH AND CONSISTENT WITH GRADING.
6. CONTRACTOR TO FIELD VERIFY ALL INVERTS AND PIPE SIZES/TYPES/CONDITIONS IN FIELD PRIOR TO PROCUREMENT OR CONSTRUCTION. REPORT ANY DEVIATIONS FROM PLANS TO NEI IMMEDIATELY. CONTRACTOR IS RESPONSIBLE FOR FIELD LAYOUT AND TO PROVIDE AS BUILT PLANS.
7. THE CONTRACTOR SHALL MAKE ALL ARRANGEMENTS FOR THE ALTERATION OF PRIVATE UTILITIES BY THE UTILITY COMPANY, AS REQUIRED.
8. THE CONTRACTOR SHALL PROTECT ALL UNDERGROUND UTILITY FACILITIES FROM EXCESSIVE VEHICULAR LOADING. ANY DAMAGE RESULTING TO THESE FACILITIES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AT THE CONTRACTORS EXPENSE.
9. ALL WATER WORKS SHALL HAVE 5 FEET OF COVER.
10. GAS, ELECTRIC, AND COMMUNICATIONS ROUTING ARE SUBJECT TO REVIEW AND APPROVAL BY UTILITY COMPANY.
11. DURING CONSTRUCTION CONTRACTOR SHALL PROTECT EXISTING UTILITIES BY PROVIDING TEMPORARY SUPPORTS OR SHEETING AS REQUIRED AT NO ADDITIONAL COST TO THE OWNER.
12. EXCAVATION REQUIRED WITHIN THE PROXIMITY OF EXISTING UTILITY LINES SHALL BE DONE BY HAND. CONTRACTOR SHALL REPAIR ANY DAMAGE TO EXISTING UTILITY LINE OR STRUCTURES INCURRED DURING CONSTRUCTION OPERATION AT NO COST TO THE OWNER.
13. PITCH EVENLY BETWEEN ALL SPOT GRADES.
14. THE CONTRACTOR IS RESPONSIBLE FOR REMOVAL AND DISPOSAL OF ANY, ROCKS, DEBRIS, ORGANICS, OR THE LIKE UNCOVERED IN THE COURSE OF WORK.
15. ALL CONDUIT AND WIRE WORK SHALL BE INSTALLED BY AHJ SPECIFICATIONS TO PROVIDE SERVICE AS DESIGNED TO PROPOSED STRUCTURES.
16. INLET PROTECTION DEVICES SHALL BE UTILIZED ON ALL PROPOSED CATCH BASIN STRUCTURES UNTIL SITE STABILIZATION HAS BEEN ACHIEVED.

LOAM AND SEED:

- GENERAL:**
1. ALL DISTURBED AREAS TO BE LOAMED AND SEEDDED PER "MASSACHUSETTS EROSION AND SEDIMENT CONTROL GUIDELINES" AS AMENDED OR REVISED.
 2. FOR NON-NATIVE PLANTING SEEDING DATES ARE APRIL 1 THROUGH JUNE 15 AND AUGUST 15 THROUGH SEPTEMBER 30. DATE MAY BE EXTENDED 15 DAYS IN NEWPORT COUNTY.
 3. FOR NATIVE PLANTING SEEDING DATES ARE BETWEEN AUGUST AND SEPTEMBER.
- SEED BED PREPARATION:**
1. LOOSEN SOIL TO A DEPTH OF FIVE INCHES BY RAKING, DISCING, DRAGGING OR TRAVERSING WITH TRACKED MACHINERY. OVER COMPACTION SHALL BE AVOIDED. CLEAR MARKS SHALL BE PERPENDICULAR TO ANTICIPATED DIRECTION OF SURFACE WATER FLOW.
 2. APPLY TOPSOIL IF NECESSARY.
 3. APPLY SLOW RELEASE FERTILIZER NOT TO EXCEED 1500 LBS PER ACRE OR 36 LBS PER 1,000 SF USING THE FOLLOWING PERCENTAGES BY WEIGHT:
 - 10% AVAILABLE NITROGEN (N)
 - 20% AVAILABLE PHOSPHORIC ACID (P)
 - 20% AVAILABLE POTASSIUM (K)
 4. REMOVE FROM SURFACE ALL STONES LARGER THAN 2" IN DIAMETER, ALL DEBRIS, TREE ROOTS OR OTHER UNSUITABLE MATERIAL.
 5. INSPECT AREA TO BE SEEDDED IMMEDIATELY PRIOR TO SEEDING FOR ANY COMPACTED AREAS. IF COMPACTED AREAS EXIST SCARIFY PRIOR TO COMMENCING SEEDING.
- SEEDING:**
1. APPLY SELECTED SEED AT RATES PROVIDED UNIFORMLY BY HAND, CYCLONE SEEDER, DRILL, CULTPACKER TYPE SEEDER OR HYDROSEEDER.
 2. NORMAL SEEDING DEPTH IS .25 TO .5 INCH. HYDROSEEDINGS MAY BE LEFT ON THE SOIL SURFACE.
 3. WHERE FEASIBLE, EXCEPT WHERE EITHER CULTPACKER TYPE SEEDER OR HYDROSEEDER IS USED, FIRM THE SEEDBED FOLLOWING SEEDING OPERATION WITH ROLLER, OR LIGHT DRAG.
 4. SEEDING RATES SHALL BE INCREASED BY 10% WHEN USING HYDROSEEDING.
 5. USE OF STRAW MULCH HELD WITH ADHESIVE MATERIALS OR 500 LBS PER ACRE OF WOOD FIBER MULCH IS RECOMMENDED FOR PROTECTION FROM SOIL EROSION.
 6. IF SEEDING CAN NOT BE COMPLETED WITHIN SEEDING DATES USE MULCHING TO PROTECT SITE AND DELAY SEEDING UNTIL THE NEXT RECOMMENDED SEEDING PERIOD.
- RECOMMENDED SEEDING:**
- SEED MIXTURE NO. 1
- KENTUCKY BLUE GRASS 22.5 LBS/AC 0.5 LBS/1000 SF
 - CREEPING RED FESCUE 105 LBS/AC 2.5 LBS/1000 SF
 - PERENNIAL RYEGRASS 22.5 LBS/AC 0.5 LBS/1000 SF
- NOTES:** WHERE CONFUSION OR UNCERTAINTY EXISTS IN LOAMING AND SEEDING CONSULT THE "RHODE ISLAND SOIL EROSION AND SEDIMENT CONTROL HANDBOOK" AS AMENDED OR REVISED, ENGINEER, LANDSCAPE ARCHITECT



TYPICAL UTILITY SEPARATION
SCALE: N.T.S.



NEI
Narragansett
Engineering Inc.
Civil - Survey Structural Environmental Design
3102 East Main Road, Portsmouth RI 02871
Tel. 401.683.6630 www.nei-cds.com

SHEET TITLE
380 KING ST - CONDOMINIUMS
GRADING AND UTILITY
PLAN 1

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24.0168	10.10.24	NKH

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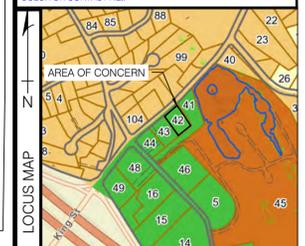
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SCALE
1"=10'
C-102

N:\PROJECTS\24.0168 - MARGUERITE COMMONWEALTH (UNION CROSSING) SITE CIVIL\24.0168 FRANKLIN CROSSING CIVIL DWG C-102 NEI Standard.ctb 10/19/2025 Job Malo



NEI
Narragansett
Engineering Inc.
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SHEET TITLE
 380 KING ST - CONDOMINIUMS
GRADING AND UTILITY
PLAN 2

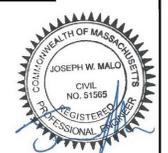
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neh-cds.com
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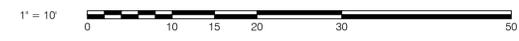
LOCUS MAP

AREA OF CONCERN

SCALE
 1"=10'

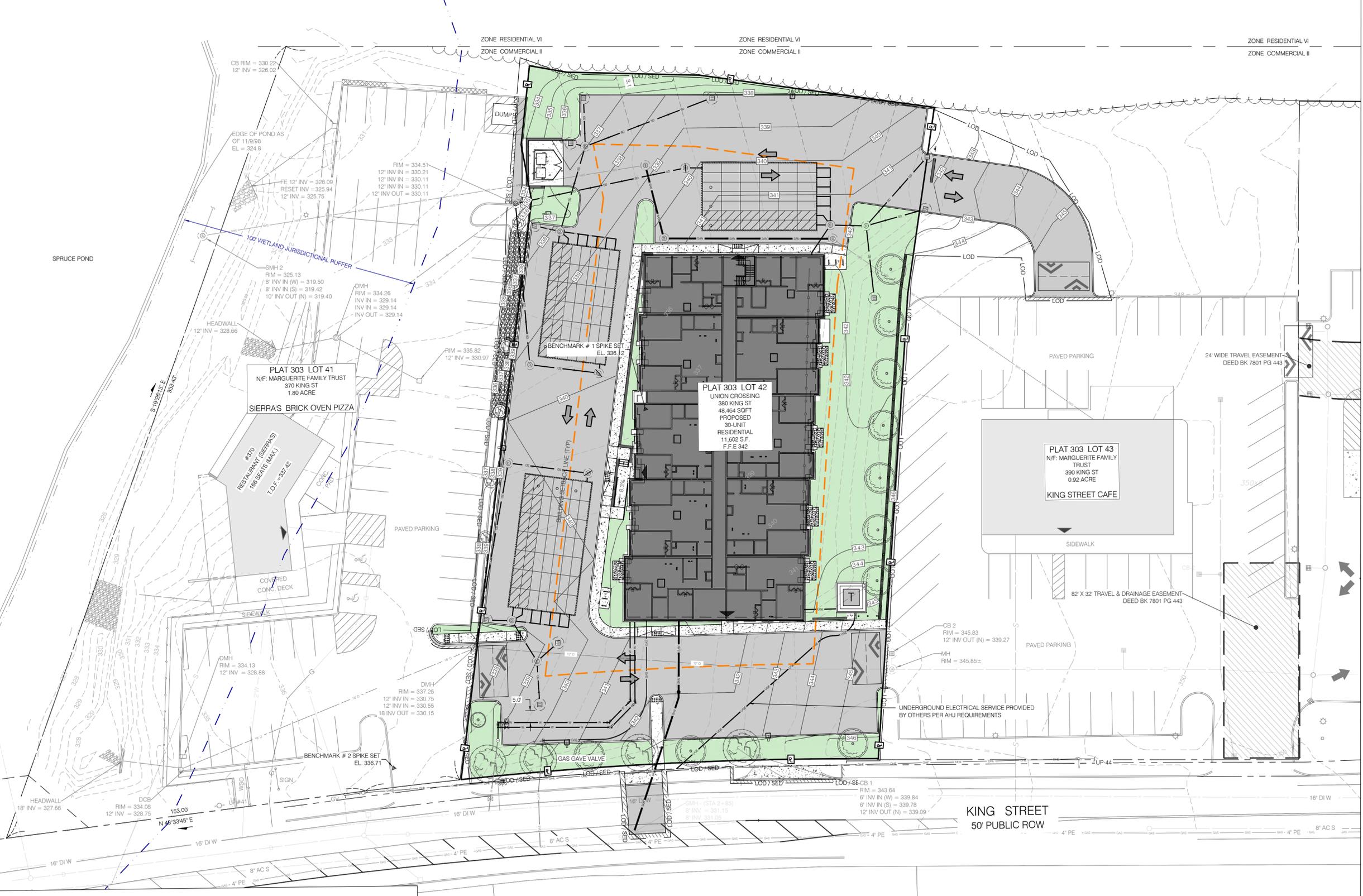
C-103

N:\PROJECTS\24.0168_MARGUERITE-COMMONWEALTH (UNION CROSSING)\SITE CIVIL\24.0168 FRANKLIN CROSSING CIVIL\DWG C-103 NEI-Standard.cdw 10/09/2025-Job Malo





PLAT 303 LOT 45
N/F: SPRUCE POND
VILLAGE
13 SPRUCE POND ROAD
33.78 ACRE



NEI
Narragansett
Engineering Inc.
Civil - Survey Structural Environmental Design
3102 East Main Road, Portsmouth RI 02871
Tel. 401.683.6630 www.nei-cds.com

SHEET TITLE
380 KING ST - CONDOMINIUMS
DRAINAGE AND UTILITY
OVERVIEW PLAN

Property Record:
Principal Parcel:
380 King St, Franklin, MA 02038
Plat: 303, Lot:42, Area: 0.919 Acres, Zone: C-II
N/F: Marguerite Margaret, TR, Marguerite Family Trust
Book/Page: 8145-260
Also Part of Subject Application:
370 King St, Franklin, MA 02038
Plat: 303, Lot:41, Area: 1.8 Acres, Zone: C-II
N/F: Marguerite Margaret, TR, Marguerite Family Trust
Book/Page: 8145-260
390 King St, Franklin, MA 02038
Plat: 303, Lot:43, Area: 0.919 Acres, Zone: C-II
N/F: Marguerite Margaret, TR, Marguerite Family Trust
Book/Page: 8145-260
400 King St, Franklin, MA 02038
Plat: 303, Lot:44, Area: 0.928 Acres, Zone: C-II
N/F: DECOY INC.

PROJECT NO.	DATE	BY
24.0168	10.10.24	NKH

- DRAWING ISSUE:**
- CONCEPT / DISCUSSION
 - PERMITTING
 - CONSTRUCTION
 - AS-BUILT
 - OTHER
- ONLY PLANS ISSUED FOR CONSTRUCTION SHALL BE USED FOR CONSTRUCTION



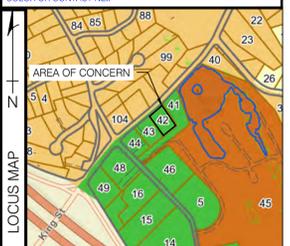
INTERNAL REVIEW

No	CHECK	CAD	DESCRIPTION/NOTES
1	BY DATE	EB 11.26.24	PARKING LAYOUT
2	BY DATE	EB 2.28.25	STORMWATER DESIGN UTILITY REVIEW PENDING
3	BY DATE	EB 3.11.25	GENERAL PLAN REVISIONS
4	BY DATE	EB 3.19.25	GENERAL AND STORMWATER PLAN REVISIONS
5	BY DATE	EB 4.28.25	STORMWATER PLAN REVISIONS
6	BY DATE	EB 5.1.25	COORDINATION MEETING REVISIONS: EASEMENT PLAN, APPARATUS CIRCULATION PLAN, WATER LINE DESIGN, LANDSCAPE REVISIONS
7	BY DATE	EB 5.7.25	STORMWATER DIVERSION STRUCTURE REVISIONS

FORMAL PLAN REVISIONS

No	DATE	STAGE/DESCRIPTION	BY
1	7.11.25	EXPANDED COURTYARD ADDED RETAINING WALL	TB
2	7.23.25	CLOSED FRONT ENTRANCE ADDED BIKE RACK, UPDATED CIRCULATION PLAN	EB
3	8.5.25	ADDED TRAFFIC CIRCULATION TO OVERVIEW	EB
4	8.14.25	REVISED TRAFFIC CIRCULATION ON OVERVIEW	JM
5	9.4.25	PEER REVIEW REVISIONS	EB
6	10.2.25	PEER REVIEW 2ND ROUND REVISIONS	EB
7	10.9.25	REVISED SEWER CONNECTION LOCATION	JM

Drawings must be printed in color to be valid. This note should be blue. If this note is not blue, please reprint in color or contact NEI.



SCALE
1"=10'
C-104

N:\PROJECTS\24.0168 MARGUERITE COMMONWEALTH UNION CROSSING\SITE CIVIL\24.0168 FRANKLIN CROSSING CIVIL.DWG C-104 NEI-Standard.ctb 10/9/2025 Job Malo

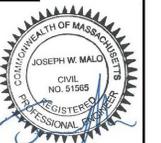
1 DRAINAGE AND UTILITY OVERVIEW PLAN
Scale: 1" = 20'
0 20 40 60 80 100

SHEET TITLE
380 KING ST - CONDOMINIUMS
DRAINAGE AND
UTILITY DETAIL PLAN

Property Record:
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 Plat: 303, Lot:42, Area: 0.919 Acres, Zone: C-II
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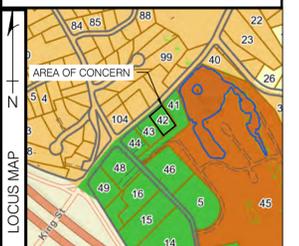
INTERNAL REVIEW

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1	BY	EB	PARKING LAYOUT
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	DATE		5.7.25

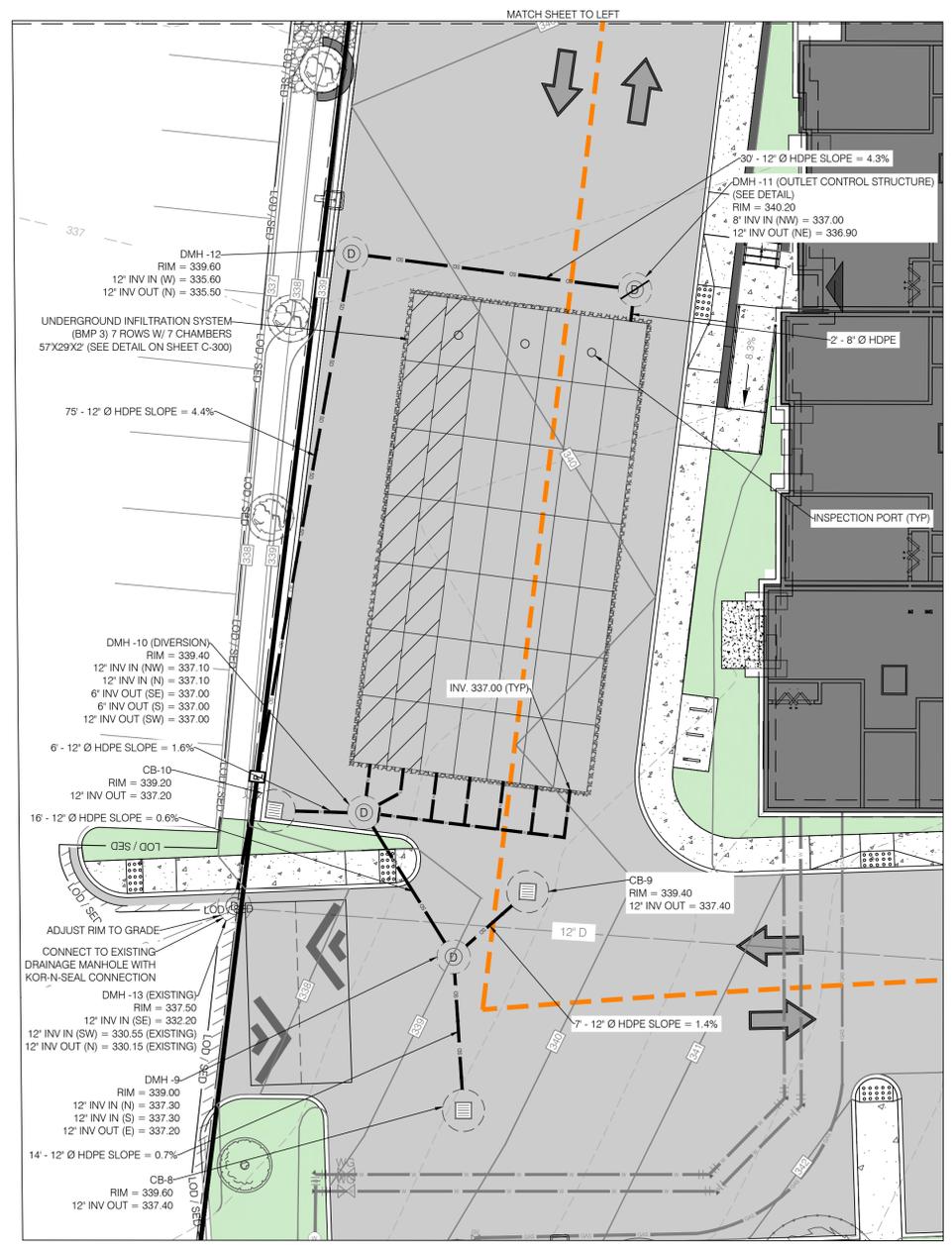
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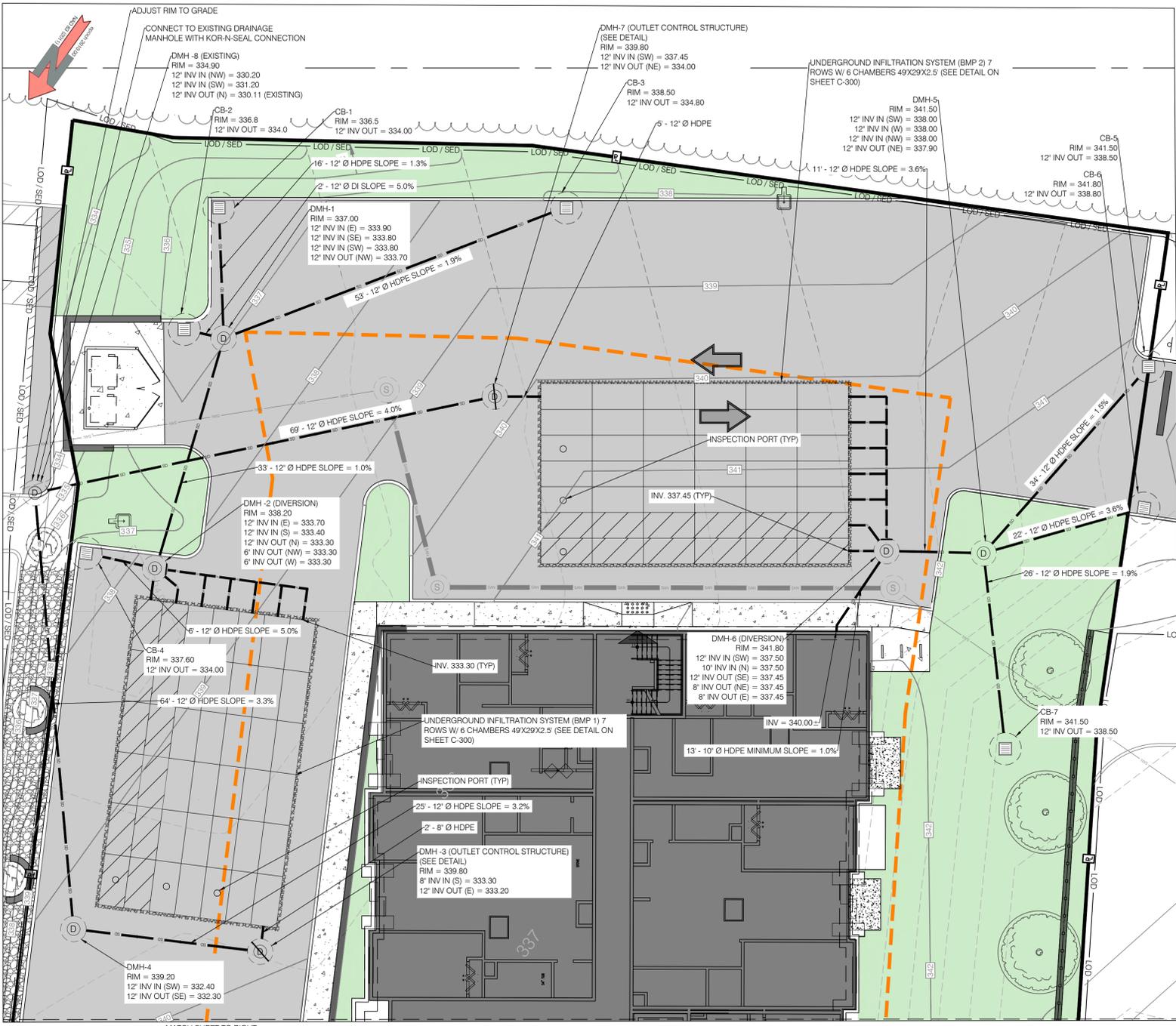
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SCALE
 1"=10'
C-105



8A
C-105
DRAINAGE AND UTILITY DETAIL PLAN 2
 SCALE: 1" = 10'

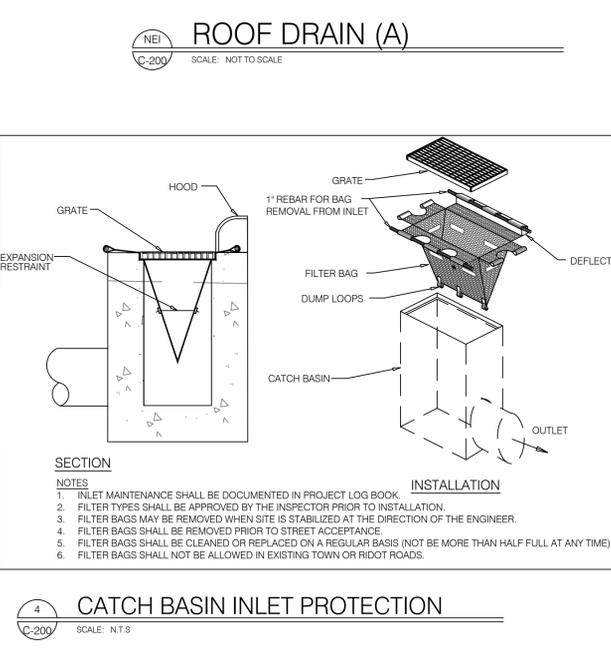
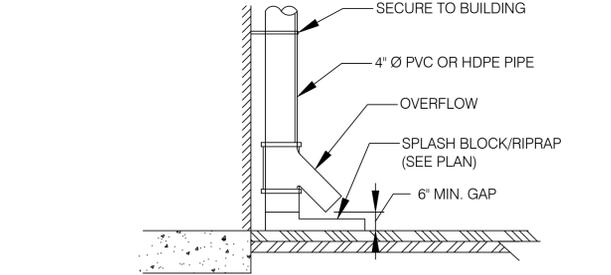
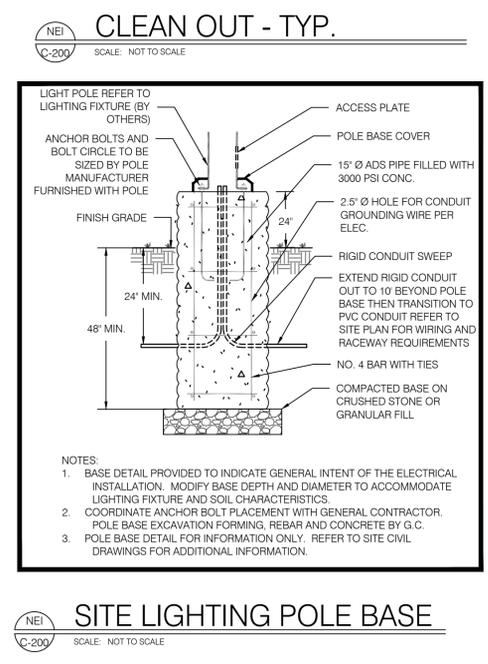
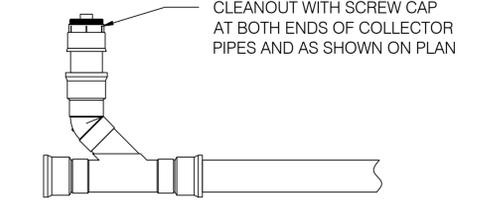
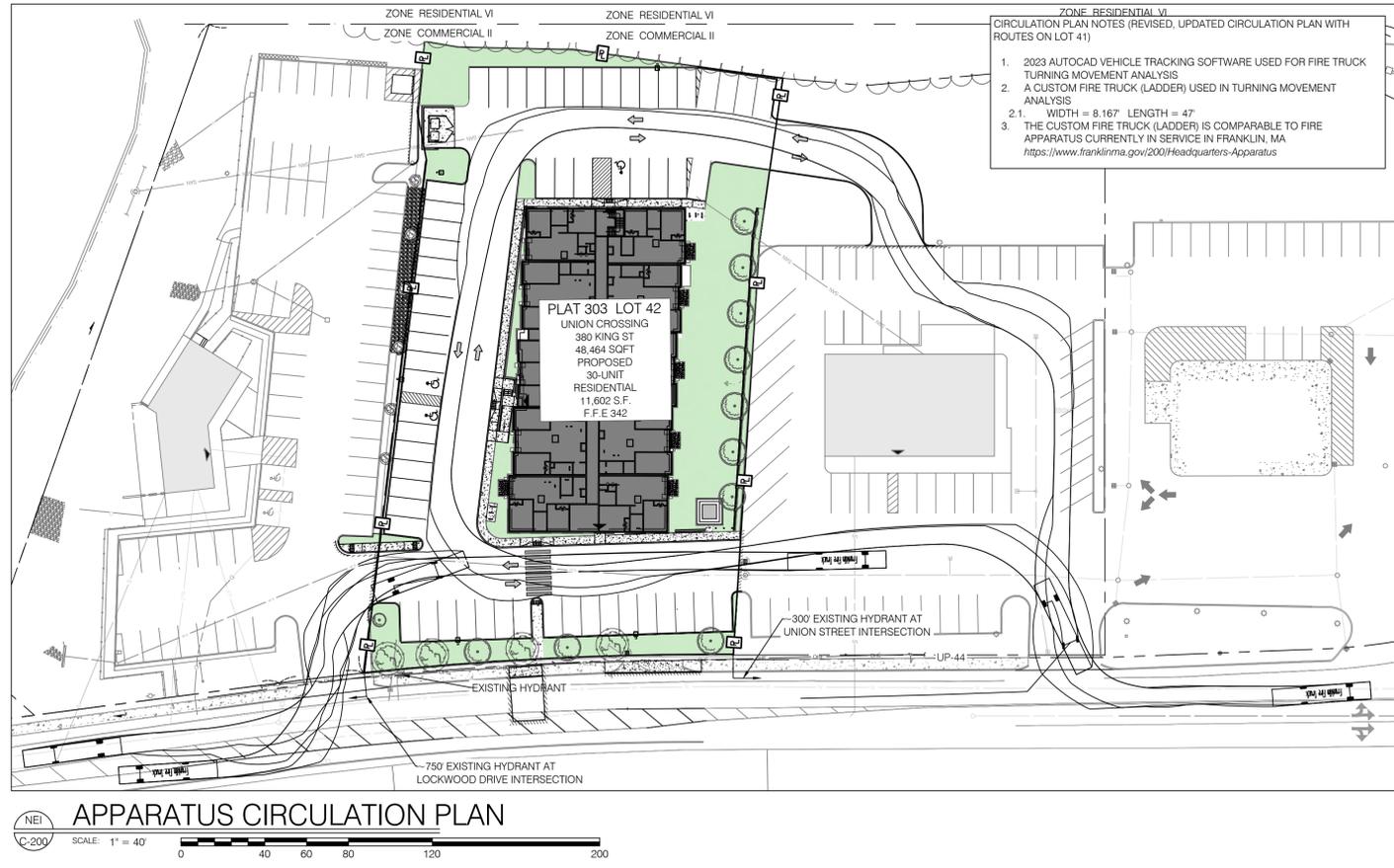
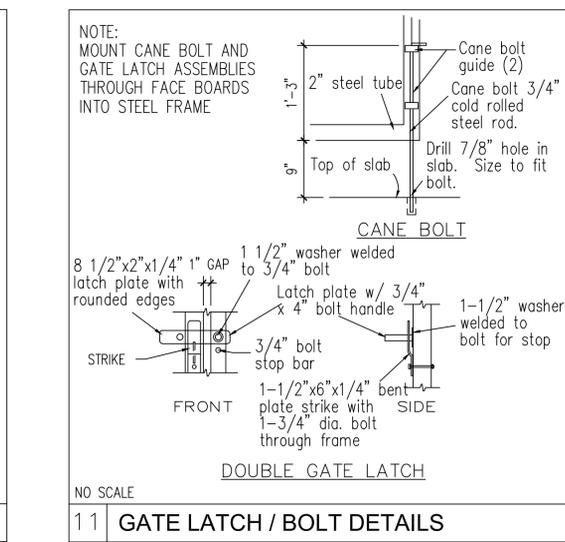
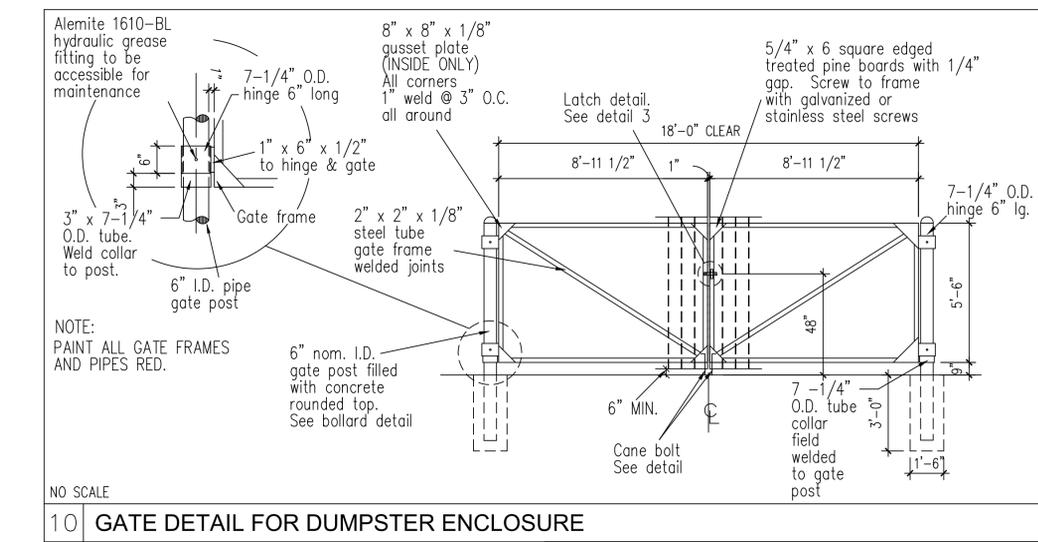
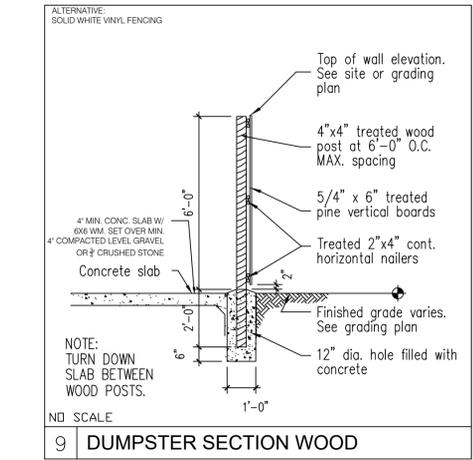
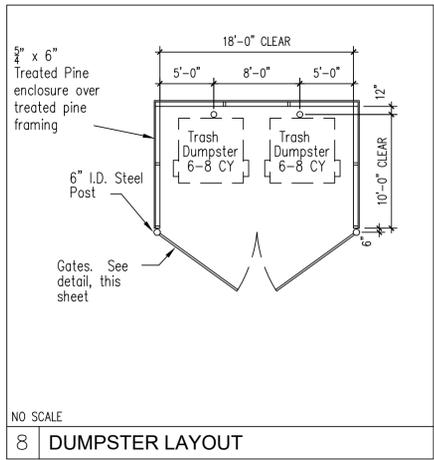
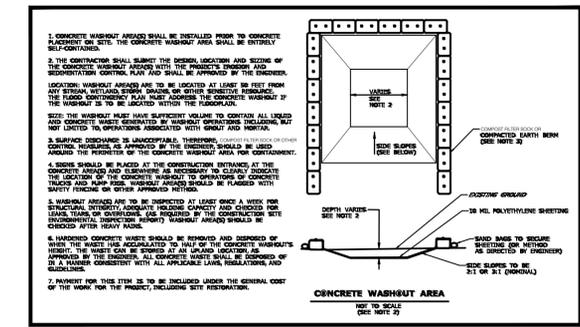
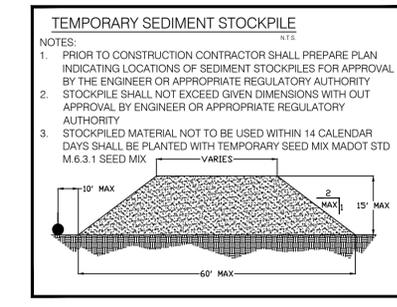
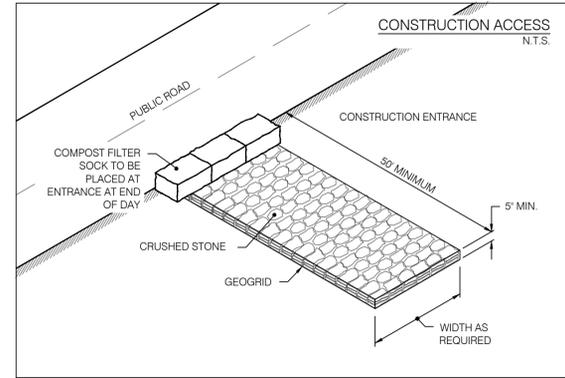
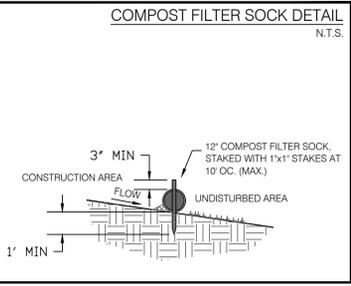


8A
C-105
DRAINAGE AND UTILITY DETAIL PLAN 1
 SCALE: 1" = 10'

N:\PROJECTS\24.0168 - MARGUERITE COMMONWEALTH (UNION CROSSING)\SITE CIVIL\24.0168 FRANKLIN CROSSING CIVIL\DWG C-105 NEI Standard.ctb 10/9/2025 Joe Malo

A. INSTALLATION. COMPOST FILTER SOCKS SHALL BE CONSTRUCTED AT THE LOCATIONS, AND IN ACCORDANCE WITH THE DETAILS INDICATED ON THE PLANS, OR AS DIRECTED BY THE ENGINEER. THE FOLLOWING STIPULATIONS ALSO APPLY:

1. COMPOST FILTER SOCKS MAY BE EITHER FABRICATED ON SITE OR DELIVERED TO THE SITE.
2. COMPOST MEDIA SHALL CONFORM TO AASHTO MP 9-06.
3. TRENCHING IS NOT REQUIRED. COMPOST FILTER SOCKS SHALL BE PLACED OVER THE TOP OF GROUND. WOODEN STAKES SHALL BE DRIVEN THROUGH THE CENTER OF THE FILTER SOCKS TO ANCHOR THEM TO THE GROUND. TO ENSURE OPTIMUM PERFORMANCE, HEAVY VEGETATION SHALL BE CUT DOWN OR REMOVED, AND EXTREMELY UNEVEN SURFACES SHALL BE GRADED TO ENSURE THAT THE COMPOST FILTER SOCK UNIFORMLY CONTACTS THE GROUND SURFACE.
4. FILTER SOCKS SHALL BE PLACED IN A CONTINUOUS LINE. WHERE ENDS INTERSECT THEY SHALL BE SLEEVED TO CREATE AN INTERLOCK WITH A TWO (2) FOOT OVERLAP. AFTER ONE SECTION IS FILLED AND THE ENDS TIED OFF, THE NEXT SECTION SHALL BE PULLED OVER THE TIED OFF END OF THE PREVIOUS SECTION, TO CREATE A 2 FOOT OVERLAP. THE OVERLAP SHALL BE STAKED. THE INTERSECTING OVERLAPS SHALL BE CONSTRUCTED TO ENSURE THAT STORMWATER DOES NOT BREAK THROUGH AT THESE INTERSECTION POINTS.
5. REMOVAL. THIS WORK, IF REQUIRED, SHALL INCLUDE THE REMOVAL OF THE COMPOST FILTER SOCK AND STAKES. UNLESS BIODEGRADABLE, THE MESH FILTER SOCK MATERIAL SHALL BE CUT OPEN AND THE MESH REMOVED. IN GENERAL, THE COMPOST FILTER MATERIAL MAY BE LEFT IN PLACE, HOWEVER THE MATERIAL WILL BE RAKED OUT, LEVELED TO SURROUNDING GRADES, THEN SEEDED. PRIOR TO SUCH REMOVAL, HOWEVER, ALL SILT, MUD AND DEBRIS ENTRAPPED OUTSIDE OF THE COMPOST FILTER SOCK SHALL BE REMOVED AND THE AREA CLEANED UP.



SHEET TITLE - CONDOMINIUMS DETAILS 1

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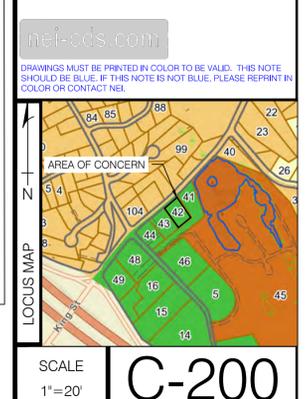
DRAWING ISSUE:
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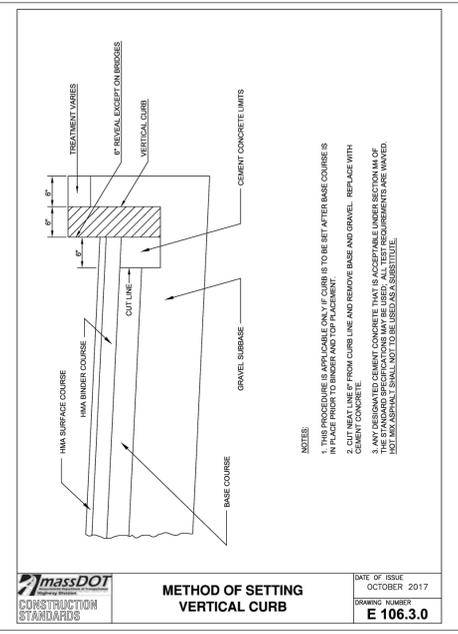
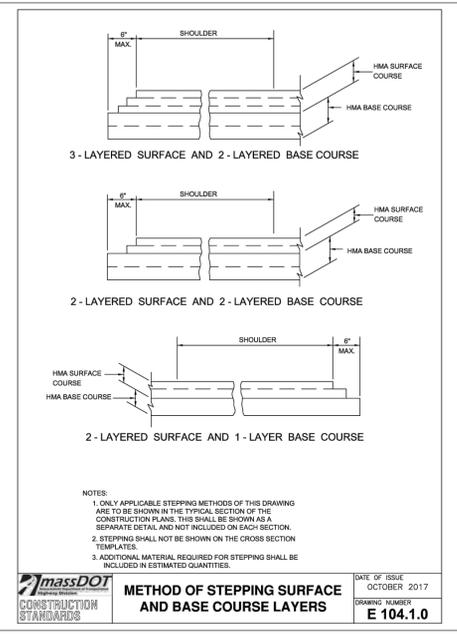
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SCALE 1"=20' **C-200**

N:\PROJECTS\24.0168 - MARGUERITE COMMONWEALTH LUNION CROSSING CIVIL\DWG C-201 - NEI Standard.cdw 10/09/2025 - Joe Malo

- GENERAL NOTES:**
- THE STATE OF MASSACHUSETTS STANDARD SPECIFICATION, 2024 EDITION, AND THE MASSACHUSETTS STANDARD DETAILS ARE MADE A PART HEREOF AS FULLY AND COMPLETELY AS IF ATTACHED HERETO. ALL WORK SHALL CONFORM TO MASSACHUSETTS STANDARD SPECIFICATION 2024 EDITION OR LATEST REVISION WITH APPLICABLE SUPPLEMENTS. THE 2024 EDITION OF THE STANDARD SPECIFICATION MAY BE OBTAINED AT THE MASSACHUSETTS DEPARTMENT OF TRANSPORTATION.
 - IT SHALL BE THE CONTRACTOR'S SOLE RESPONSIBILITY TO APPLY FOR AND OBTAIN ANY AND ALL NECESSARY PERMITS, PAY ALL FEES AND POST ALL BONDS ASSOCIATED WITH THE SAME, AND COORDINATE WITH ARCHITECT OR ENGINEER AS NECESSARY.
 - THE CONTRACTOR IS SOLELY RESPONSIBLE FOR THE SAFETY OF THE JOB SITE. THE CONTRACTOR SHALL PROVIDE TEMPORARY FENCING AND/OR BARRIERS AROUND ANY EXPOSED EXCAVATED AREAS IN ACCORDANCE WITH OSHA STANDARDS.
 - IN THE CASE THAT ANY DEVIATION / ALTERATION / OR IMPROVEMENT FROM THE APPROVED PLANS IS NECESSARY THE CONTRACTOR SHALL IMMEDIATELY CONTACT THE ENGINEER AND OWNER PRIOR TO OCCURRENCE OF DEVIATION.
 - ALL WORK SHALL BE LIMITED TO THE AREAS WITHIN THE LIMIT OF DISTURBANCE DISPLAYED ON THESE PLANS OR PROPERTY LINE IF LIMIT OF DISTURBANCE IS UNCLEAR. ANY AREA DISTURBED OUTSIDE OF THE LIMIT OF DISTURBANCE SHALL BE REPAIRED AND RESTORED TO ITS ORIGINAL CONDITION AT NO COST TO THE OWNER OR ENGINEER, AND PERFORMED TO THE ENGINEER'S SATISFACTION.
 - ALL SITE WORK SHALL MEET OR EXCEED THE SITE WORK SPECIFICATION SHOWN ON THESE PLANS AND/OR ACCOMPANYING SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING IF ANY CONFLICTS WITH EXISTING CONDITIONS OR PROPOSED CONDITIONS EXIST. IF ANY CONFLICTS ARE DISCOVERED, THE CONTRACTOR SHALL NOTIFY THE OWNER AND ENGINEER PRIOR TO INSTALLATION OF ANY PORTION OF THE SITE WORK THAT WOULD BE AFFECTED.
 - EXCAVATED ROCK SHALL BE REMOVED FROM THE SITE AND LEGALLY DISPOSED OF UNLESS OTHER ARRANGEMENTS ARE MADE WITH THE OWNER. SUITABLE ROCK MAY BE UTILIZED IN FILL AREAS WITH WRITTEN PERMISSION OF THE OWNER'S REPRESENTATIVES.
 - DEBRIS, ORGANICS AND OTHER UNSUITABLE MATERIALS UNCOVERED DURING THE COURSE OF SITE EXCAVATION SHALL BE REMOVED FROM THE SITE AND DISPOSED OF LEGALLY.
 - THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR MAINTAINING THE INTEGRITY OF ALL EXISTING UTILITIES THAT SERVICE THE SITE AND NEIGHBORING AREAS. IF ANY DAMAGE OCCURS TO EXISTING UTILITIES IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PAY ALL COSTS ASSOCIATED WITH REPAIR OF UTILITIES AS DIRECTED BY THE ENGINEER, UTILITY OWNER, OR GOVERNING AGENCY.
 - THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR QUANTITY TAKE-OFF IN COMPUTING ANY ESTIMATES.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR ESTABLISHING AND MAINTAINING ALL TEMPORARY SEDIMENTATION AND EROSION CONTROLS.
 - THE LOCATION OF EXISTING UTILITIES AS SHOWN ARE APPROXIMATE AND SHALL BE VERIFIED BY THE CONTRACTOR. "DIG SAFE" SHALL BE CONTACTED BY THE CONTRACTOR AS PART OF THIS VERIFICATION.
 - NO EXCAVATION SHALL PROCEED UNTIL UTILITY COMPANIES ARE NOTIFIED IN ADVANCE.
 - ALL TREE PROTECTION BY OTHERS UNLESS OTHERWISE NOTED.
 - CONTRACTOR TO LOAM AND SEED ALL DISTURBED AREAS WITH APPROPRIATE SEED MIXTURES.
 - ALL PLANTINGS SHALL CONFORM TO THE BEST DEVELOPMENT PRACTICES GUIDEBOOK PER THE TOWN OF FRANKLIN DEPARTMENT OF PLANNING & COMMUNITY DEVELOPMENT DATED FEBRUARY 2021.
 - CONTRACTOR SHALL COORDINATE WITH MEP FOR UTILITY CONNECTIONS TO PROPOSED STRUCTURES.



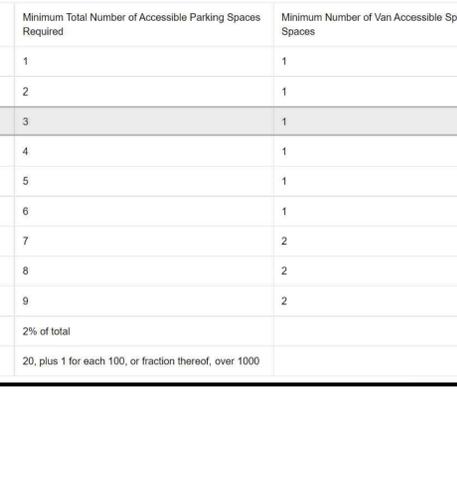
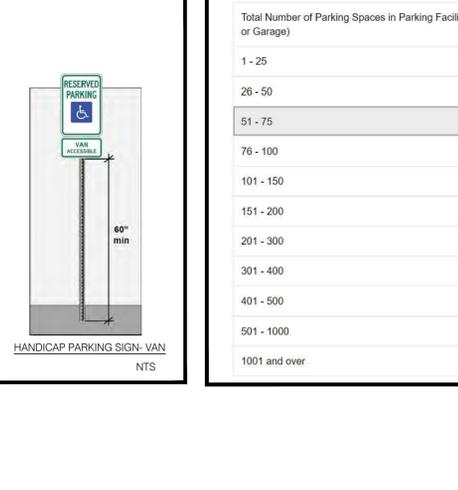
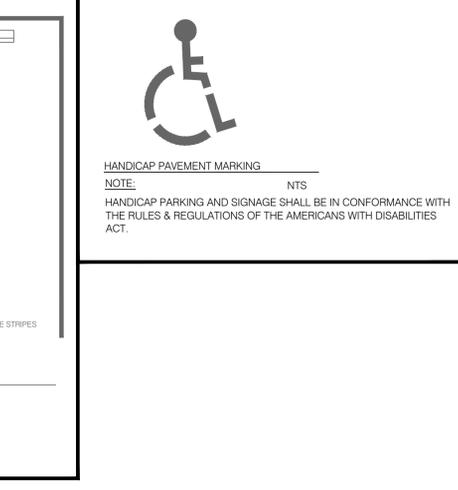
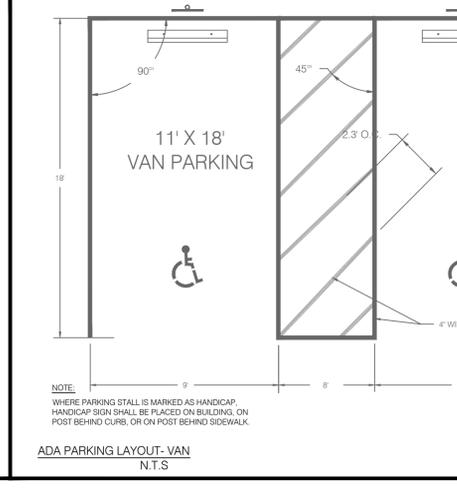
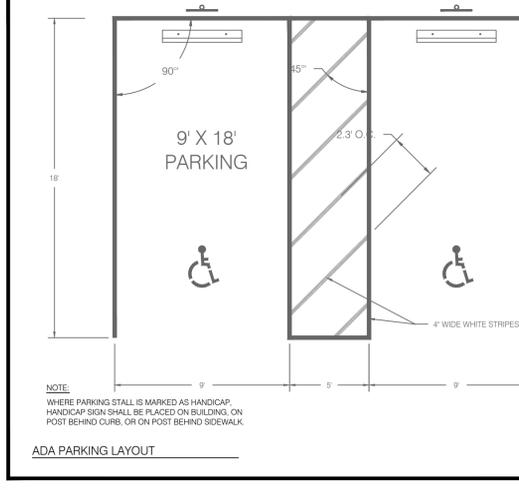
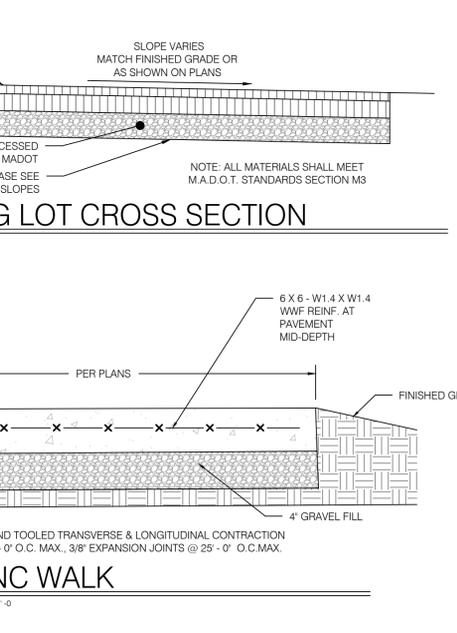
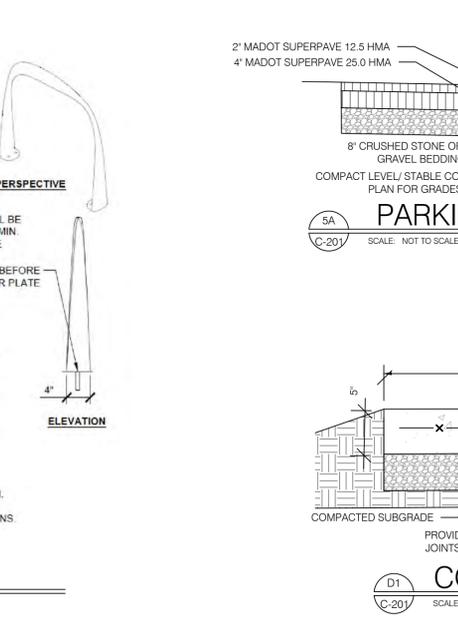
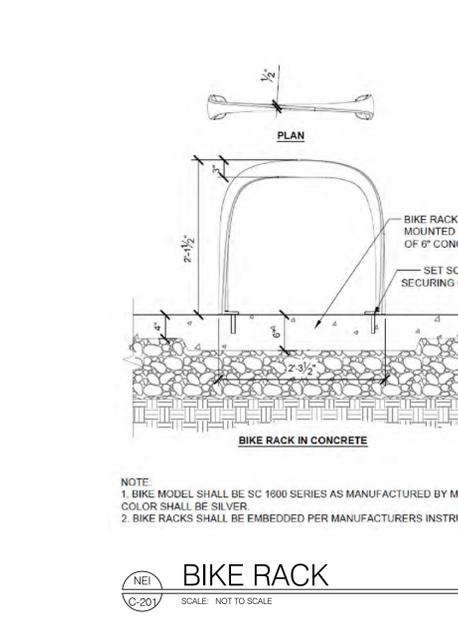
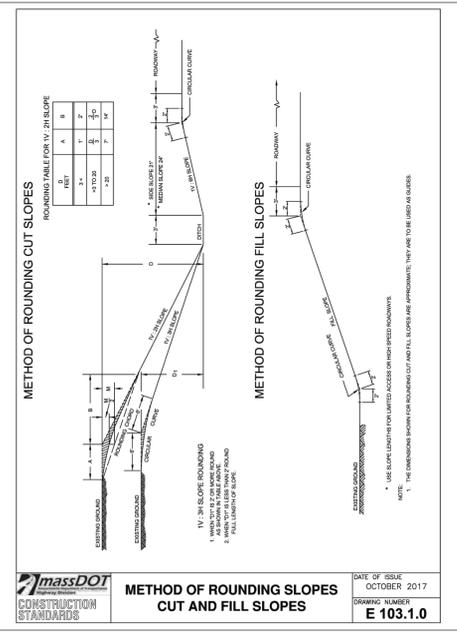
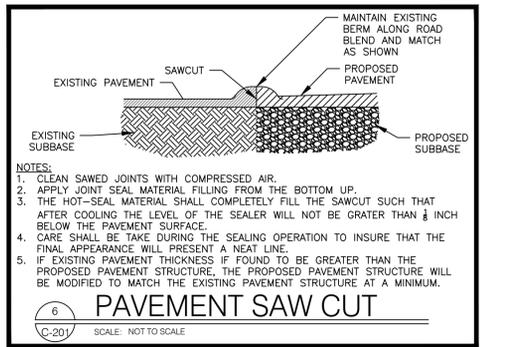
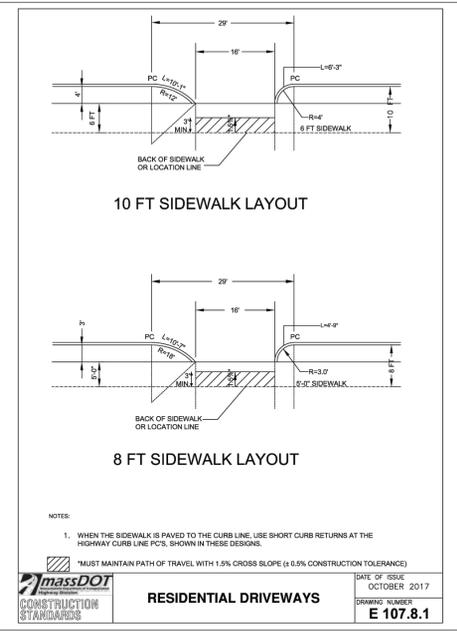
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	>0% TO 1%	7'-8"
	>1% TO 2%	9'-0"
	>2% TO 3%	11'-0"
	>3% TO 4%	14'-0"
	>4% TO 5%	15'-0" Max

*** HIGH SIDE TRANSITION LENGTH**

NOTE:
* BASED ON A DESIGN SLOPE OF 7.5% AND A REVEAL OF 6".

METHOD OF CURB TRANSITION LENGTH FOR WHEELCHAIR RAMPS

DATE OF ISSUE: OCTOBER 2017
DRAWING NUMBER: E 107.9.0



NEI
Narragansett
Engineering Inc.

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3102 East Main Road, Portsmouth RI 02871
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4	BY	EB	GENERAL AND STORMWATER PLAN REVISIONS
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6	BY	EB	STORMWATER PLAN REVISIONS
6	DATE	4.28.25	
6	BY	EB	COORDINATION MEETING REVISIONS: EASEMENT PLAN, APPARATUS CIRCULATION PLAN, WATER LINE DESIGN, LANDSCAPE REVISIONS
7	BY	EB	STORMWATER DIVERSION STRUCTURE REVISIONS
7	DATE	5.7.25	

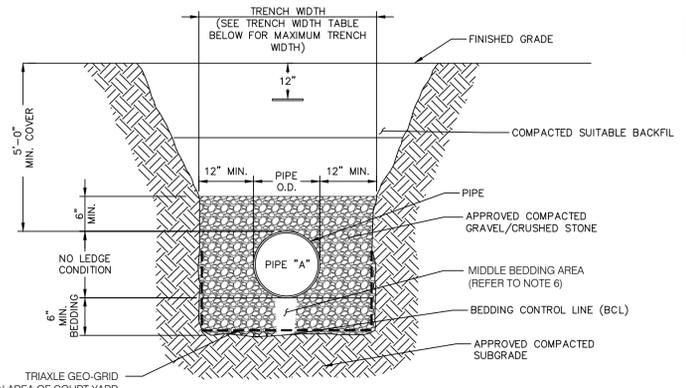
FORMAL PLAN REVISIONS

No	DATE	STAGE/DESCRIPTION	BY
1	7.11.25	EXPANDED COURTYARD ADDED RETAINING WALL CLOSED FRONT ENTRANCE ADDED BIKE RACK, UPDATED CIRCULATION PLAN	TB
2	7.23.25		
3	8.5.25	ADDED TRAFFIC CIRCULATION TO OVERVIEW	EB
4	8.14.25	REVISED TRAFFIC CIRCULATION ON OVERVIEW	JM
5	9.4.25	PEER REVIEW REVISIONS	EB
6	10.2.25	PEER REVIEW 2ND ROUND REVISIONS	EB
7	10.9.25	REVISED SEWER CONNECTION LOCATION	JM

Scale: 1" = 20'

C-201

N:\PROJECTS\24.0188 - MARGUERITE-COMMONWEALTH LUNION CROSSING\SITE CIVIL\24.0188 FRANKLIN CROSSING CIVIL.DWG C-202 NEI-Standard.ctb 10/8/2025-Joe Malo



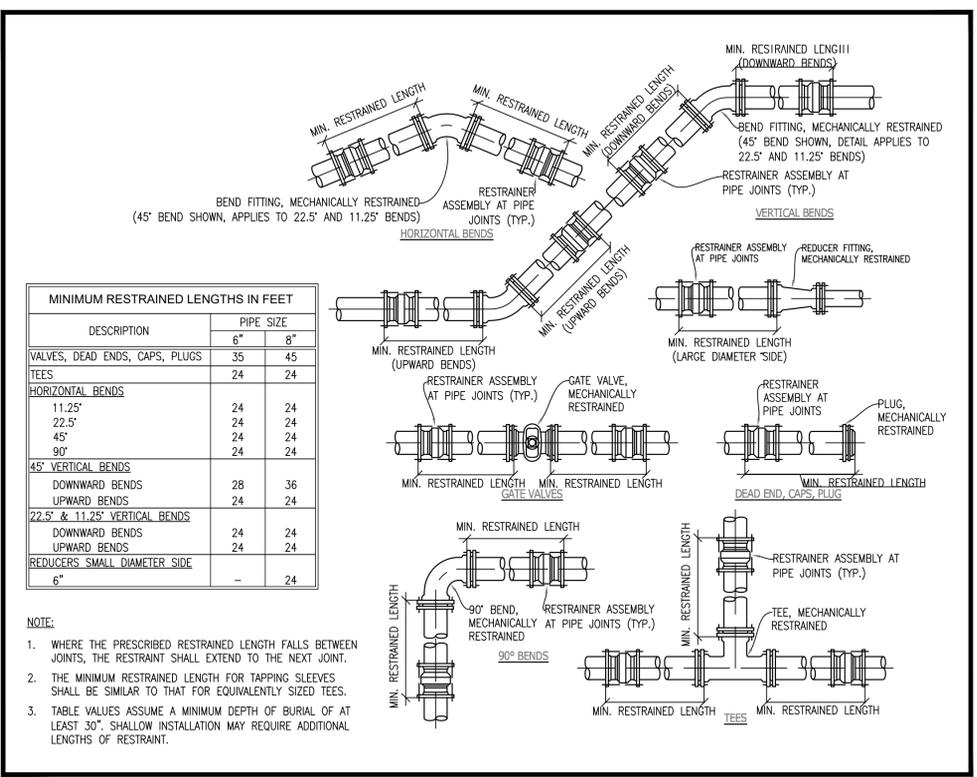
BIAXLE GEO-GRID MAY BE ACCEPTABLE SUBJECT TO CONTRACTOR SUBMISSION OF A SPECIFICATION.

- NOTES:**
1. MAINTAIN UNIFORM TRENCH WIDTH TO 6" OVER PIPE.
 2. IF SHEETING IS REQUIRED TO REMAIN, CUT OFF TWO (2) FEET BELOW FINISH GRADE.
 3. IF GROUNDWATER IS ENCOUNTERED, WRAP STONE WITH MIRAFI 140N FILTER FABRIC OR APPROVED EQUAL.
 4. SEE PAVEMENT REPAIR DETAILS FOR ROAD WORK AREAS.
 5. PROVIDE 6" MIN. BEDDING FOR AREAS OF EXCAVATION IN ROCK.
 6. LOOSELY PLACE SUITABLE BACKFILL OR CLEAN WASHED 3/4" CRUSHED STONE IN MIDDLE BEDDING AREA. DO NOT COMPACT MIDDLE BEDDING AREA.
 7. ALL TRENCHES SHALL BE SUFFICIENTLY WIDE TO ACCOMMODATE TRENCH BOX.
 8. SUBGRADE SHOULD NOT CONSIST OF UNSUITABLE SOIL. UNSUITABLE SOIL IS DEFINED AS SOILS THAT SHALL NOT PROVIDE A STABLE SUBBASE OR SIDE SLOPES DUE TO THE PROPERTIES OF SAID SOILS. SHOULD UNSUITABLE SOILS BE ENCOUNTERED THE EXISTING TRENCH SHALL BE EXCAVATED FURTHER DEPTH OR WIDTH AND REFILLED WITH APPROVED MATERIAL AT COST OF EXCAVATOR.

TRENCH WIDTHS	
PIPE SIZE	MAX (ONE PIPE) ⁽⁶⁾
15" OR LESS	4'-0"
18"	5'-0"
24"	5'-6"
30"	6'-0"
36"	6'-6"
42"	7'-0"
48"	7'-6"
MANHOLES	O.D. + 6'-0"
CATCH BASINS ⁷	O.D. + 6'-0"

⁽⁶⁾ FOR ROCK EXCAVATION SUBTRACT 1'-0"

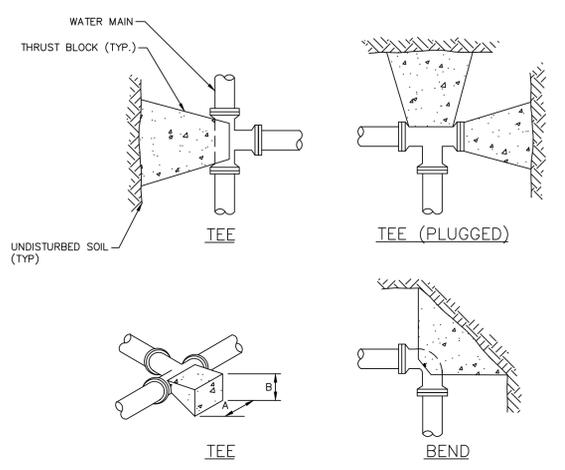
3 TYPICAL PIPE TRENCH
SCALE: N.T.S.



MINIMUM RESTRAINED LENGTHS IN FEET		
DESCRIPTION	PIPE SIZE	
	6"	8"
VALVES, DEAD ENDS, CAPS, PLUGS	35	45
TEES	24	24
HORIZONTAL BENDS		
11.25'	24	24
22.5'	24	24
45'	24	24
90'	24	24
45° VERTICAL BENDS		
DOWNWARD BENDS	28	36
UPWARD BENDS	24	24
22.5° & 11.25° VERTICAL BENDS		
DOWNWARD BENDS	24	24
UPWARD BENDS	24	24
REDUCERS SMALL DIAMETER SIDE		
6"	-	24

- NOTE:**
1. WHERE THE PRESCRIBED RESTRAINED LENGTH FALLS BETWEEN JOINTS, THE RESTRAINT SHALL EXTEND TO THE NEXT JOINT.
 2. THE MINIMUM RESTRAINED LENGTH FOR TAPPING SLEEVES SHALL BE SIMILAR TO THAT FOR EQUIVALENTLY SIZED TEES.
 3. TABLE VALUES ASSUME A MINIMUM DEPTH OF BURIAL OF AT LEAST 30". SHALLOW INSTALLATION MAY REQUIRE ADDITIONAL LENGTHS OF RESTRAINT.

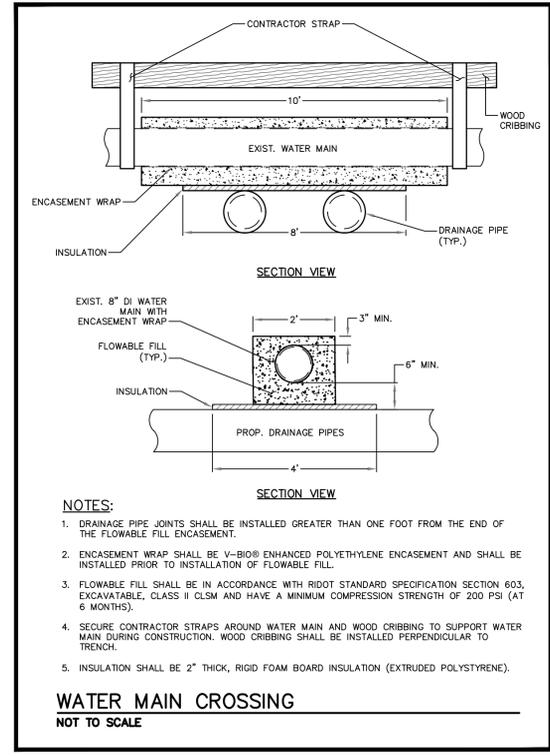
4 PIPE RESTRAINT LENGTHS
SCALE: N.T.S.



SOIL TYPE	TABLE 1: 4" THRU 10" FITTINGS				TABLE 2: 12" THRU 16" FITTINGS			
	TEES		BENDS		TEES		BENDS	
	A	B	A	B	A	B	A	B
SOFT CLAY	48"	24"	48"	24"	60"	36"	72"	36"
SAND	24"	24"	24"	24"	36"	30"	48"	30"
GRAVEL	24"	18"	24"	18"	30"	24"	40"	24"

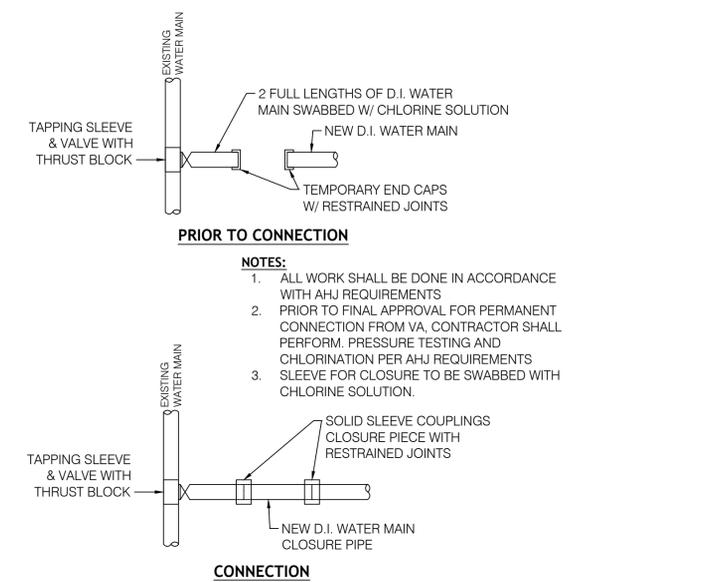
- NOTES:**
1. CONCRETE FOR ALL THRUST BLOCKS TO BE MINIMUM 3,000 PSI., 28 DAY STRENGTH, TYPE I CEMENT, 3/4" STONE.
 2. WHERE POSSIBLE, CONSTRUCT THRUST BLOCKS AGAINST UNDISTURBED SOIL. WHERE NOT POSSIBLE PLACE FILL BETWEEN THE THRUST BLOCK AND THE UNDISTURBED SOIL COMPACTED TO 90% STANDARD PROCTOR DENSITY.
 3. WRAP FITTINGS WITH POLYETHYLENE PRIOR TO CONSTRUCTING THRUST BLOCKS. NO JOINTS SHALL BE COVERED WITH CONCRETE.
 4. THRUST BLOCK DIMENSIONS ARE BASED ON A MAXIMUM WATER MAIN PRESSURE OF 150 PSI.

2 TYPICAL THRUST BLOCK DETAIL
SCALE: N.T.S.



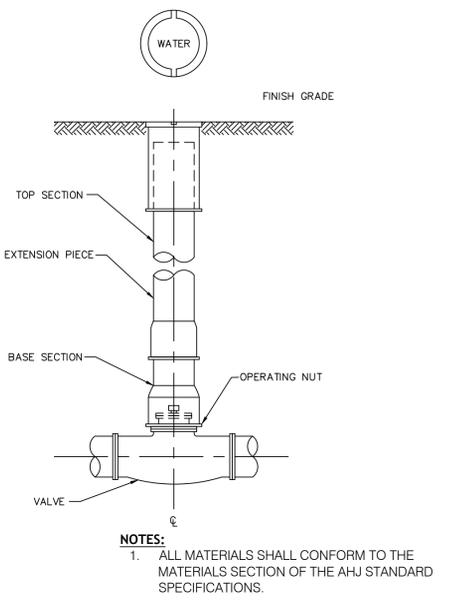
- NOTES:**
1. DRAINAGE PIPE JOINTS SHALL BE INSTALLED GREATER THAN ONE FOOT FROM THE END OF THE FLOWABLE FILL ENCASUREMENT.
 2. ENCASUREMENT WRAP SHALL BE V-BIO® ENHANCED POLYETHYLENE ENCASUREMENT AND SHALL BE INSTALLED PRIOR TO INSTALLATION OF FLOWABLE FILL.
 3. FLOWABLE FILL SHALL BE IN ACCORDANCE WITH RIDOT STANDARD SPECIFICATION SECTION 603, EXCAVATABLE, CLASS II CLSM AND HAVE A MINIMUM COMPRESSION STRENGTH OF 200 PSI (AT 6 MONTHS).
 4. SECURE CONTRACTOR STRAPS AROUND WATER MAIN AND WOOD CRIBBING TO SUPPORT WATER MAIN DURING CONSTRUCTION. WOOD CRIBBING SHALL BE INSTALLED PERPENDICULAR TO TRENCH.
 5. INSULATION SHALL BE 2" THICK, RIGID FOAM BOARD INSULATION (EXTRUDED POLYSTYRENE).

WATER MAIN CROSSING
NOT TO SCALE



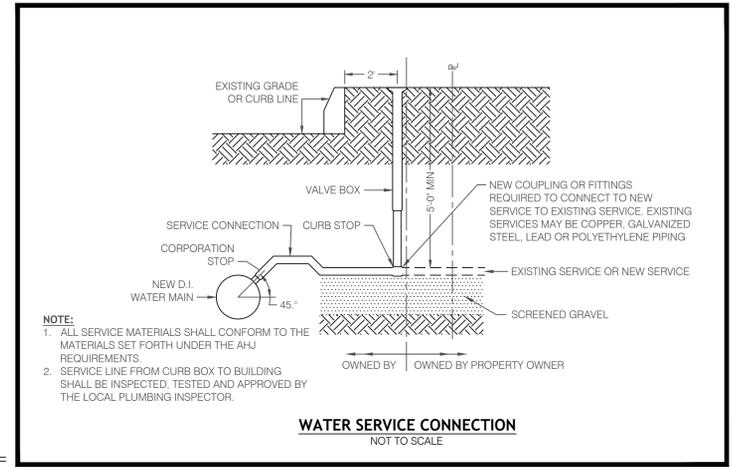
- NOTES:**
1. ALL WORK SHALL BE DONE IN ACCORDANCE WITH AHJ REQUIREMENTS.
 2. PRIOR TO FINAL APPROVAL FOR PERMANENT CONNECTION FROM VA, CONTRACTOR SHALL PERFORM PRESSURE TESTING AND CHLORINATION PER AHJ REQUIREMENTS.
 3. SLEEVE FOR CLOSURE TO BE SWABBED WITH CHLORINE SOLUTION.

4 CONN. OF PR. WATER MAIN TO EX. WATER MAIN
SCALE: N.T.S.



- NOTES:**
1. ALL MATERIALS SHALL CONFORM TO THE MATERIALS SECTION OF THE AHJ STANDARD SPECIFICATIONS.

4 GATE VALVE WITH VALVE BOX
SCALE: N.T.S.



- NOTE:**
1. ALL SERVICE MATERIALS SHALL CONFORM TO THE MATERIALS SET FORTH UNDER THE AHJ REQUIREMENTS.
 2. SERVICE LINE FROM CURB BOX TO BUILDING SHALL BE INSPECTED, TESTED AND APPROVED BY THE LOCAL PLUMBING INSPECTOR.

WATER SERVICE CONNECTION
NOT TO SCALE

NEI
Narragansett
Engineering Inc.
Civil - Survey Structural Environmental Design
3102 East Main Road, Portsmouth RI 02871
Tel. 401.683.6630 www.nei-cds.com

SHEET TITLE
380 KING ST - CONDOMINIUMS
DETAILS 3

Property Record:
Principal Parcel:
380 King St, Franklin, MA 02038
Plat: 303, Lot:42, Area: 0.919 Acres, Zone: C-II
N/F: Marguerite Margaret, TR, Marguerite Family Trust
Book/Page: 8145-260
Also Part of Subject Application:
370 King St, Franklin, MA 02038
Plat: 303, Lot:41, Area: 1.8 Acres, Zone: C-II
N/F: Marguerite Margaret, TR, Marguerite Family Trust
Book/Page: 8145-260
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Book/Page: 8145-260
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Plat: 303, Lot:44, Area: 0.928 Acres, Zone: C-II
N/F: DECOY INC.

PROJECT NO.	DATE	BY
24.0168	10.10.24	NKH

DRAWING ISSUE:

CONCEPT / DISCUSSION

PERMITTING

CONSTRUCTION

AS-BUILT

OTHER

ONLY PLANS ISSUED FOR CONSTRUCTION SHALL BE USED FOR CONSTRUCTION



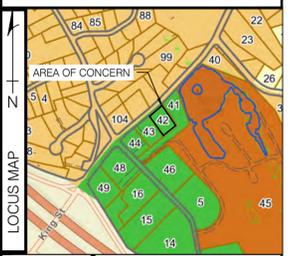
INTERNAL REVIEW

No	CHECK	CAD	DESCRIPTION/NOTES
1	BY	EB	PARKING LAYOUT
	DATE		11.26.24
2	BY	EB	STORMWATER DESIGN
	DATE		2.28.25
3	BY	EB	UTILITY REVIEW PENDING
	DATE		3.11.25
4	BY	EB	GENERAL AND STORMWATER PLAN REVISIONS
	DATE		3.19.25
5	BY	EB	STORMWATER PLAN REVISIONS
	DATE		4.28.25
6	BY	EB	COORDINATION MEETING REVISIONS: EASEMENT PLAN, APPARATUS CIRCULATION PLAN, WATER LINE DESIGN, LANDSCAPE REVISIONS
	DATE		5.1.25
7	BY	EB	STORMWATER DIVERSION STRUCTURE REVISIONS
	DATE		5.7.25

FORMAL PLAN REVISIONS

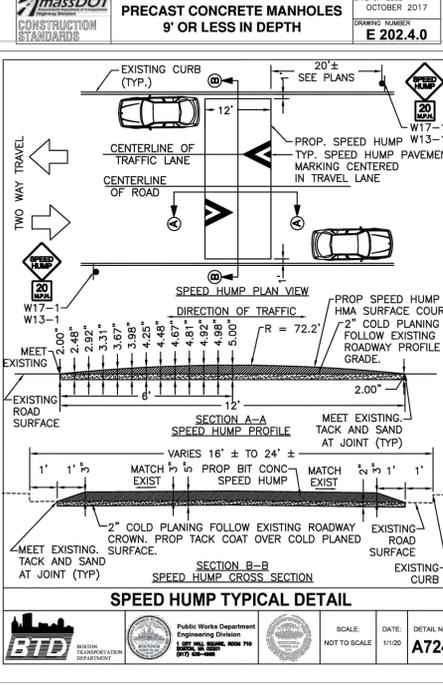
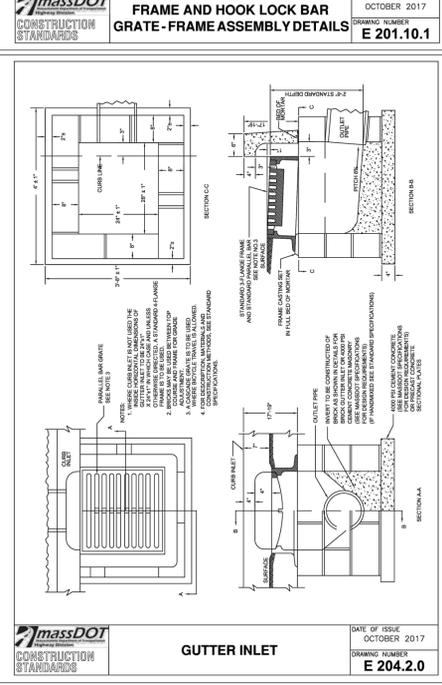
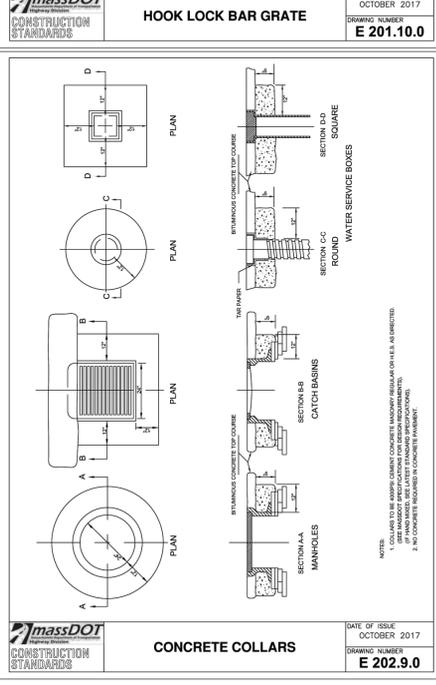
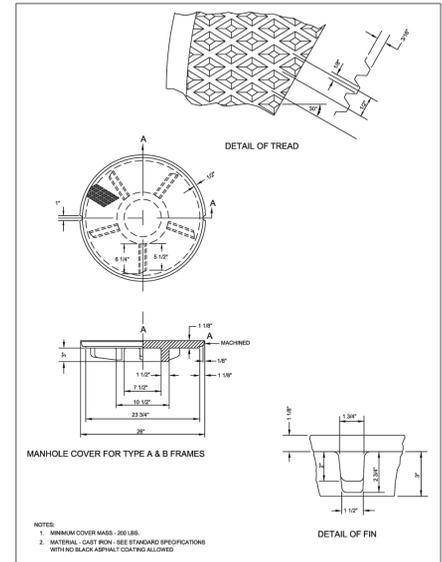
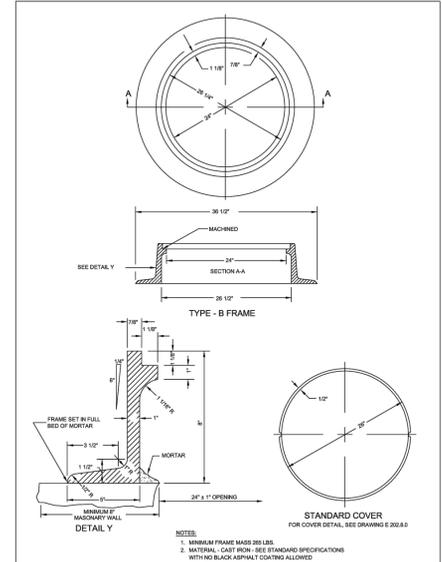
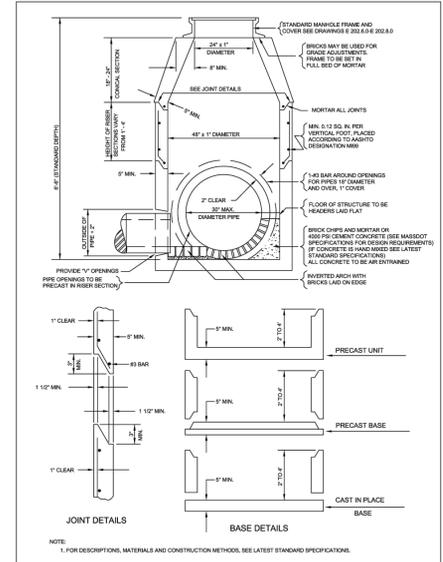
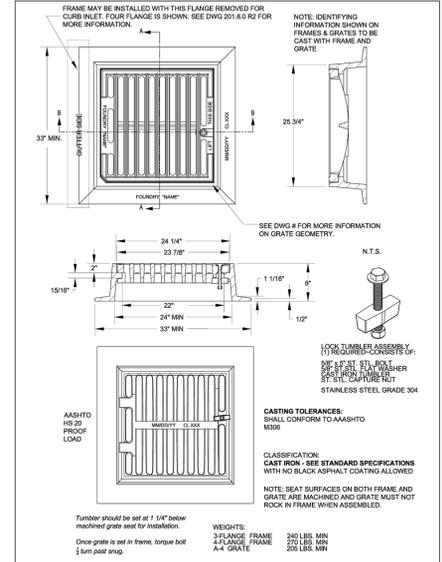
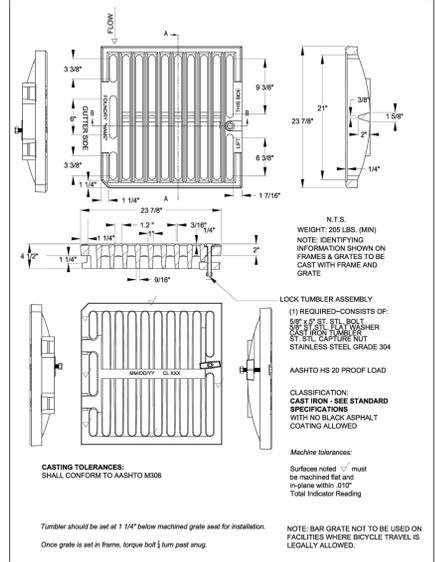
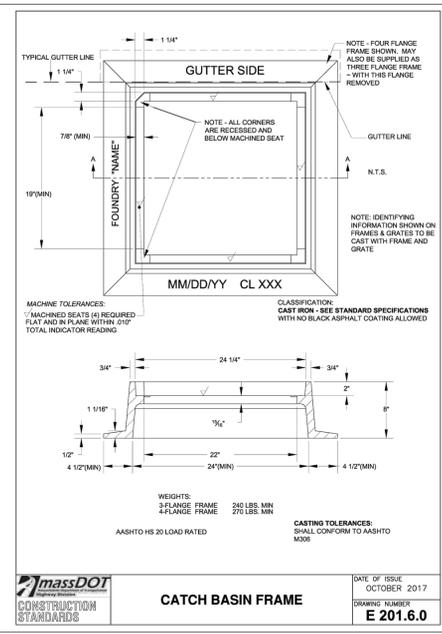
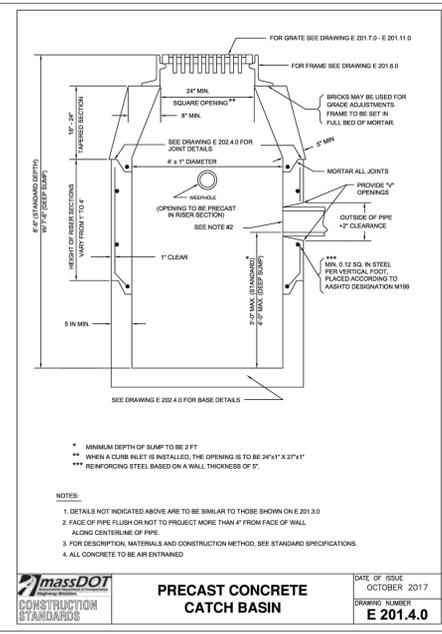
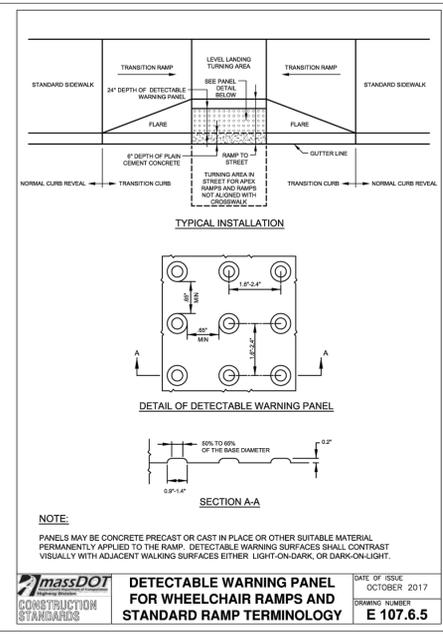
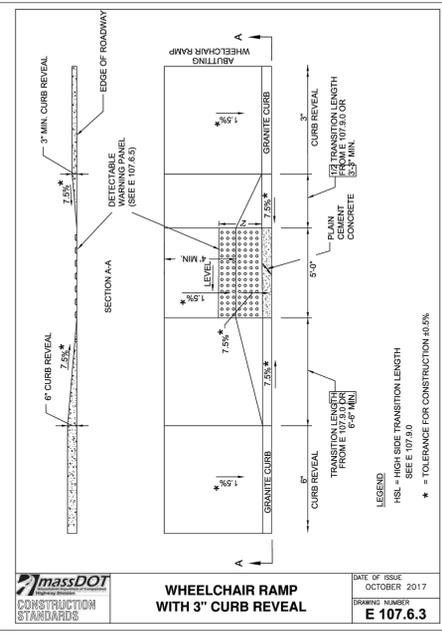
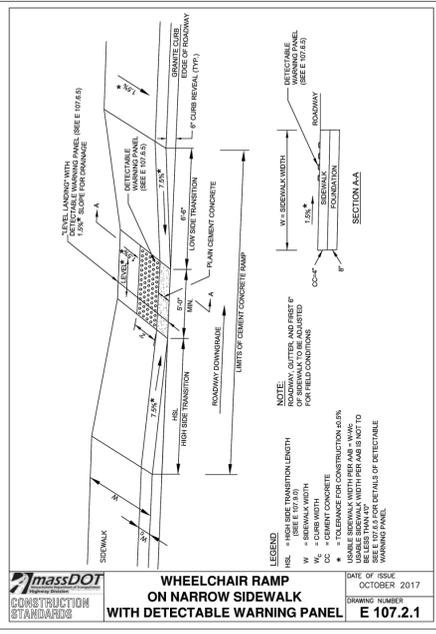
No	DATE	STAGE/DESCRIPTION	BY
1	7.11.25	EXPANDED COURTYARD	TB
		ADDED RETAINING WALL	
		CLOSED FRONT ENTRANCE	
2	7.23.25	ADDED BIKE RACK, UPDATED CIRCULATION PLAN	EB
3	8.5.25	ADDED TRAFFIC CIRCULATION TO OVERVIEW	EB
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6	10.2.25	PEER REVIEW 2ND ROUND REVISIONS	EB
7	10.9.25	REVISED SEWER CONNECTION LOCATION	JM

Drawings must be printed in color to be valid. This note should be blue. If this note is not blue, please reprint in color or contact NEI.



SCALE: 1"=20'
C-202

N:\PROJECTS\24.0168 - MARGUERITE COMMONWEALTH LUNION CROSSING\SITE CIVIL\24.0168 FRANKLIN CROSSING CIVIL.DWG C-203 NEI-Standard.cad 10/9/2025 Joe Malo



NEI
Narragansett
Engineering Inc.

Civil - Survey Structural Environmental Design
3102 East Main Road, Portsmouth RI 02871
Tel. 401.683.6630 www.nei-cds.com

SHEET TITLE - CONDOMINIUMS DETAILS 4

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PROJECT NO.	DATE	BY
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- CONSTRUCTION
- AS-BUILT
- OTHER

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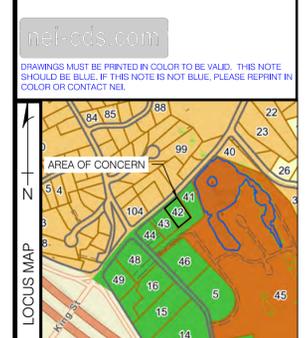


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7	DATE	5.7.25	STRUCTURE REVISIONS

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SCALE: 1"=20'

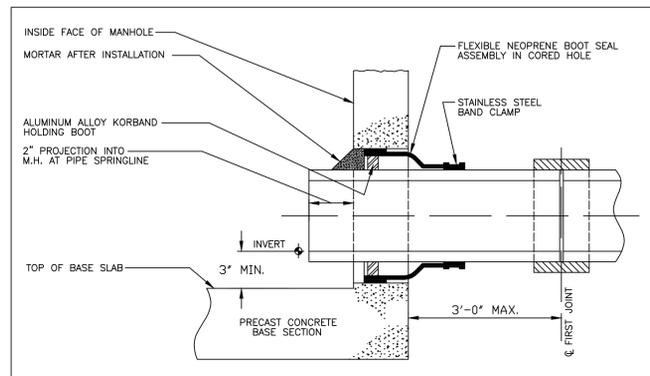
C-203

MOUNDING ANALYSIS - MASSACHUSETTS STORMWATER HANDBOOK VOLUME 3 CHAPTER 1 :

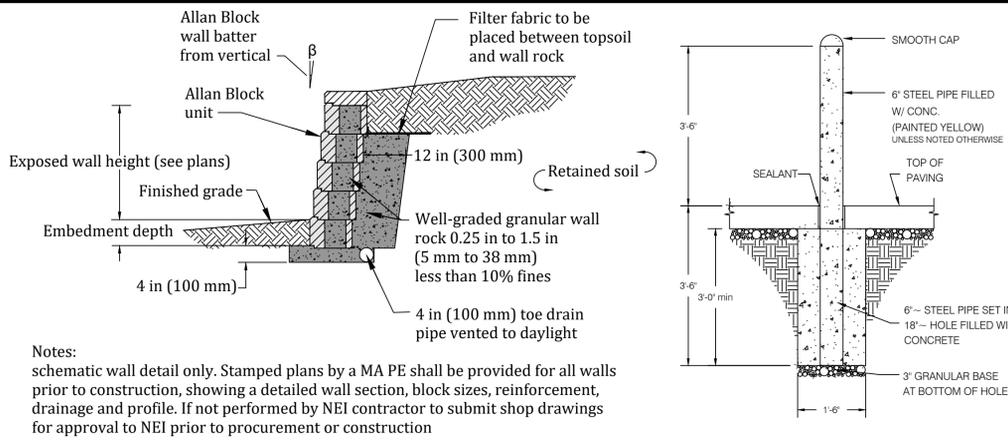
MOUNDING ANALYSIS IS REQUIRED WHEN THE VERTICAL SEPARATION FROM THE BOTTOM OF AN EXFILTRATION SYSTEM TO SEASONAL HIGH GROUNDWATER IS LESS THAN FOUR (4) FEET AND THE RECHARGE SYSTEM IS PROPOSED TO ATTENUATE THE PEAK DISCHARGE FROM A 10-YEAR OR HIGHER 24-HOUR STORM (E.G., 10-YEAR, 25-YEAR, 50-YEAR, OR 100-YEAR 24-HOUR STORM). IN SUCH CASES, THE MOUNDING ANALYSIS MUST DEMONSTRATE THAT THE REQUIRED RECHARGE VOLUME (E.G., INFILTRATION BASIN STORAGE) IS FULLY DEWATERED WITHIN 72 HOURS (SO THE NEXT STORM CAN BE STORED FOR EXFILTRATION). THE MOUNDING ANALYSIS MUST ALSO SHOW THAT THE GROUNDWATER MOUND THAT FORMS UNDER THE RECHARGE SYSTEM WILL NOT BREAK OUT ABOVE THE LAND OR WATER SURFACE OF A WETLAND (E.G., IT DOESN'T INCREASE THE WATER SHEET ELEVATION IN A BORDERING VEGETATED WETLAND, SALT MARSH, OR LAND UNDER WATER WITHIN THE 72-HOUR EVALUATION PERIOD).

THE HANTUSH OR OTHER EQUIVALENT METHOD MAY BE USED TO CONDUCT THE MOUNDING ANALYSIS. THE HANTUSH METHOD PREDICTS THE MAXIMUM HEIGHT OF THE GROUNDWATER MOUND BENEATH A RECTANGULAR OR CIRCULAR RECHARGE AREA. IT ASSUMES UNCONFINED GROUNDWATER FLOW, AND THAT A LINEAR RELATION EXISTS BETWEEN THE WATER TABLE ELEVATION AND WATER TABLE DECLINE RATE. IT RESULTS IN A WATER TABLE RECESSION HYDROGRAPH DEPICTING EXPONENTIAL DECLINE. THE HANTUSH METHOD IS AVAILABLE IN PROPRIETARY SOFTWARE AND FREE ON-LINE CALCULATORS ON THEWEB IN AUTOMATED FORMAT. IF THE ANALYSIS INDICATES THE MOUND WILL PREVENT THE INFILTRATION BMP FROM FULLY DRAINING WITHIN THE 72-HOUR PERIOD, AN ITERATIVE PROCESS MUST BE EMPLOYED TO DETERMINE AN ALTERNATIVE DESIGN THAT DRAINS WITHIN THE 72-HOUR PERIOD.

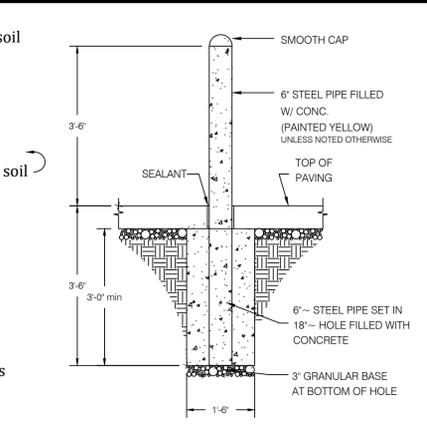
MOUNDING ANALYSIS IS ALSO NEEDED WHEN RECHARGE IS PROPOSED AT OR ADJACENT TO A SITE CLASSIFIED AS CONTAMINATED, WAS CAPPED IN PLACE, OR HAS AN ACTIVITY AND USE LIMITATION (AUL) THAT PRECLUDES INDUCING RUNOFF TO THE GROUNDWATER, PURSUANT TO MGL CHAPTER 21E AND THE MASSACHUSETTS CONTINGENCY PLAN 310 CMR 40.0000, OR IS A SOLID WASTE LANDFILL PURSUANT TO 310 CMR 19.000; OR GROUNDWATER FROM THE RECHARGE LOCATION FLOWS DIRECTLY TOWARD A SOLID WASTE LANDFILL OR 21E SITE. IN THIS CASE, THE MOUNDING ANALYSIS MUST DETERMINE WHETHER INFILTRATION OF THE REQUIRED RECHARGE VOLUME WILL CAUSE OR CONTRIBUTE TO GROUNDWATER CONTAMINATION.



1 MANHOLE WALL PIPE PENETRATION DETAIL
SCALE: NOT TO SCALE



2 TYPICAL GRAVITY RETAINING WALL SECTION
Scale: N.T.S.

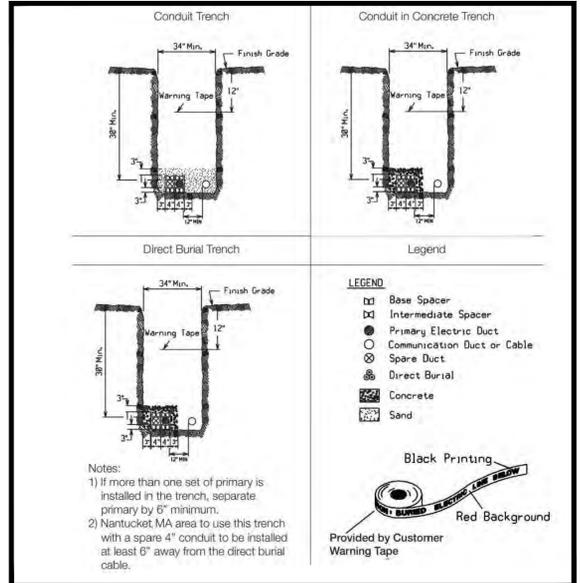


3 TYPICAL BOLLARD DETAIL
SCALE: N.T.S.

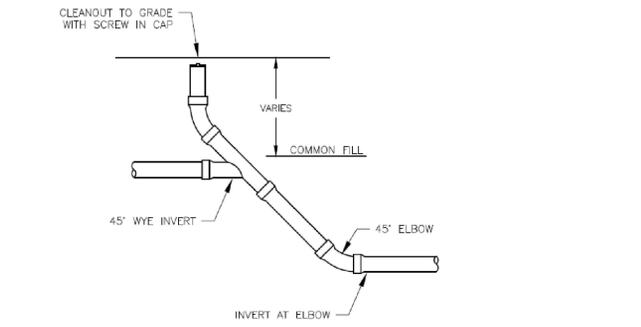
ADS DESIGN AND INSTALLATION SPECIFICATIONS:
[HTTPS://WWW.ADS-PIPE.COM/SITES/DEFAULT/FILES/RAINAGE_HANDBOOK_ADH2_STRUCTURES_%2805-19%29.PDF](https://www.ads-pipe.com/sites/default/files/drainage_handbook_adh2_structures_%2805-19%29.pdf)

ADS COVER AND TRENCH DETAILS UNDER PAVEMENT:
[HTTPS://WWW.ADS-PIPE.COM/SITES/DEFAULT/FILES/MINIMUM_AND_MAXIMUM_COVER_DEPTHS_4IN-48IN_F2648_PIPE_N-12_%28TECH_NOTE_A2.02_01-16%29.PDF](https://www.ads-pipe.com/sites/default/files/minimum_and_maximum_cover_depths_4in-48in_f2648_pipe_n-12_%28tech_note_a2.02_01-16%29.pdf)

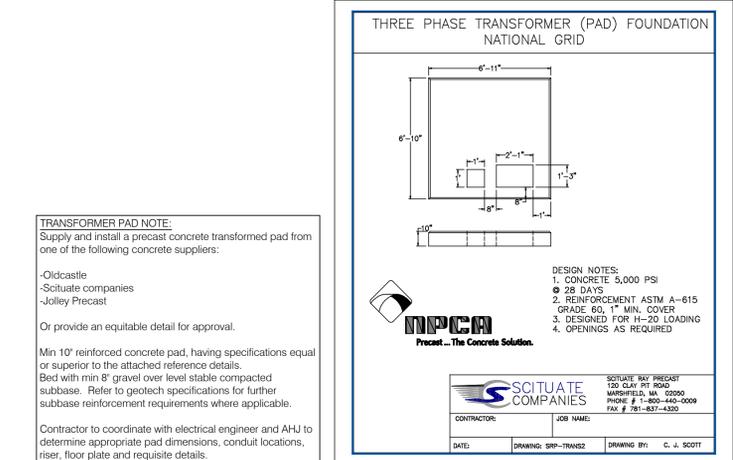
ADS CONNECTION DETAILS:
[HTTPS://WWW.ADS-PIPE.COM/SITES/DEFAULT/FILES/CONNECTION_HDPE_AND_HP_STORM_TO_MANHOLES_AND_STRUCTURES_A5.04_MARCH_2016.PDF](https://www.ads-pipe.com/sites/default/files/connection_hdpe_and_hp_storm_to_manholes_and_structures_a5.04_march_2016.pdf)



4 TYPICAL ELECTRICAL TRENCH DETAIL
SCALE: N.T.S.



5 SEWER DROP WYE
SCALE: N.T.S.



6 TRANSFORMER PAD DETAILS
SCALE: NOT TO SCALE

NEI
Narragansett Engineering Inc.
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N/F: Marguerite Margaret, TR, Marguerite Family Trust
Book/Page: 8145-260

Also Part of Subject Application:
370 King St, Franklin, MA 02038
Book/Page: 8145-260

390 King St, Franklin, MA 02038
Plat: 303, Lot:43, Area: 0.919 Acres, Zone: C-II
N/F: Marguerite Margaret, TR, Marguerite Family Trust
Book/Page: 8145-260

400 King St, Franklin, MA 02038
Plat: 303, Lot:44, Area: 0.928 Acres, Zone: C-II
N/F: DECOY INC.

PROJECT NO.	DATE	BY
24.0168	10.10.24	NKH

DRAWING ISSUE:

CONCEPT / DISCUSSION

PERMITTING

CONSTRUCTION

AS-BUILT

OTHER

ONLY PLANS ISSUED FOR CONSTRUCTION SHALL BE USED FOR CONSTRUCTION



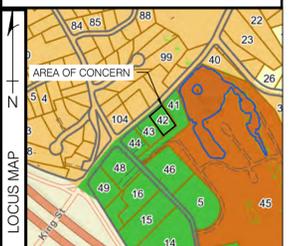
INTERNAL REVIEW

No	CHECK	CAD	DESCRIPTION/NOTES
1	BY: EB	DATE: 11.26.24	PARKING LAYOUT
2	BY: EB	DATE: 2.28.25	STORMWATER DESIGN UTILITY REVIEW PENDING
3	BY: EB	DATE: 3.11.25	GENERAL PLAN REVISIONS
4	BY: EB	DATE: 3.19.25	GENERAL AND STORMWATER PLAN REVISIONS
6	BY: EB	DATE: 4.28.25	STORMWATER PLAN REVISIONS
6	BY: EB	DATE: 5.1.25	COORDINATION MEETING REVISIONS: EASEMENT PLAN, APPARATUS CIRCULATION PLAN, WATER LINE DESIGN, LANDSCAPE REVISIONS
7	BY: EB	DATE: 5.7.25	STORMWATER DIVERSION STRUCTURE REVISIONS

FORMAL PLAN REVISIONS

No	DATE	STAGE/DESCRIPTION	BY
1	7.11.25	EXPANDED COURTYARD	TB
2	7.23.25	ADDED RETAINING WALL, CLOSED FRONT ENTRANCE, ADDED BIKE RACK, UPDATED CIRCULATION PLAN	EB
3	8.5.25	ADDED TRAFFIC CIRCULATION TO OVERVIEW	EB
4	8.14.25	REVISED TRAFFIC CIRCULATION ON OVERVIEW	JM
5	9.4.25	PEER REVIEW REVISIONS	EB
6	10.2.25	PEER REVIEW 2ND ROUND REVISIONS	EB
7	10.9.25	REVISED SEWER CONNECTION LOCATION	JM

Drawings must be printed in color to be valid. This note should be blue. If this note is not blue, please reprint in color or contact NEI.



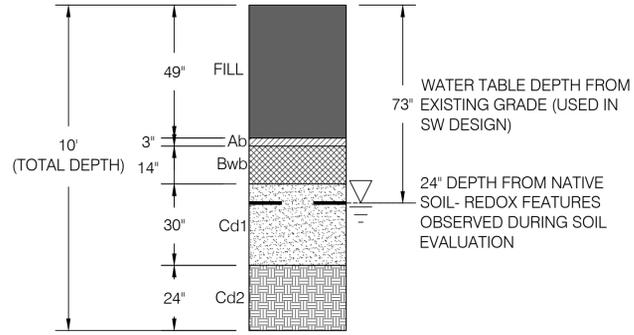
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C-204

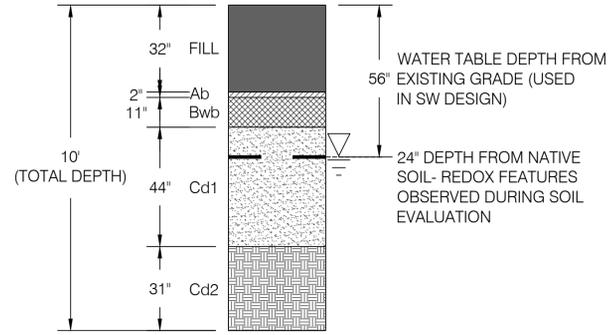
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N:\PROJECTS\24.0188 - MARGUERITE COMMONWEALTH LUNION CROSSING\SITE CIVIL\24.0188 FRANKLIN CROSSING CIVIL.DWG C-205 NEI Standard.cdw 10/9/2025 Joe Malo

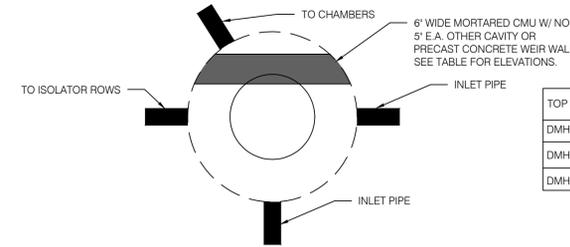
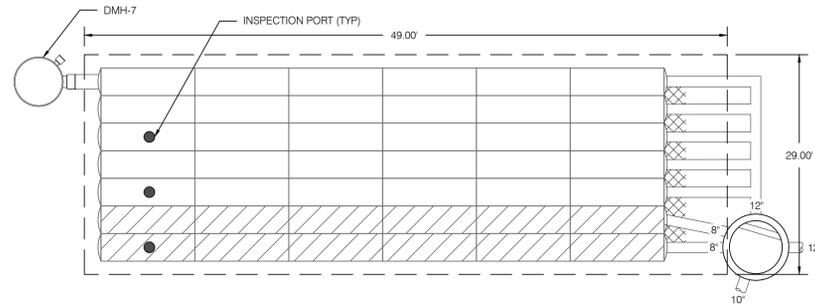
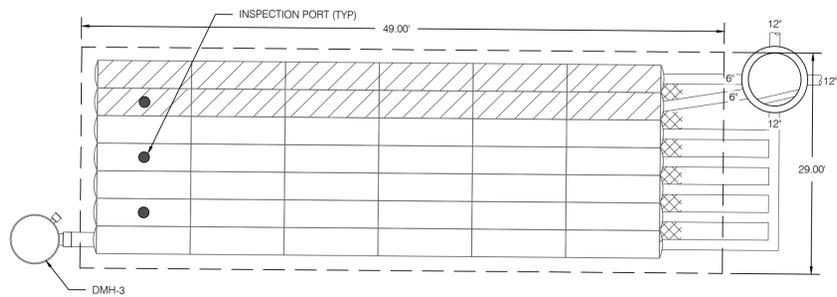
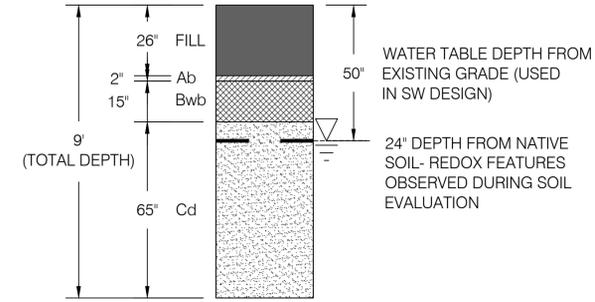
OBSERVATION HOLE #1



OBSERVATION HOLE #2



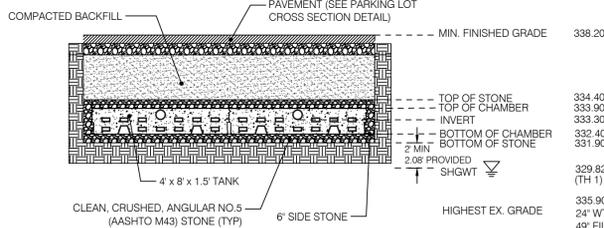
OBSERVATION HOLE #3



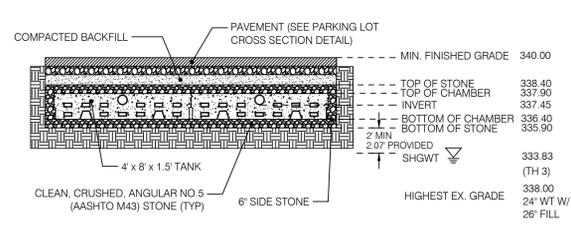
TOP OF WEIR WALL ELEVATION	
DMH-3 (DIVERSION) FOR BMP-1	EL. 333.68
DMH-6 (DIVERSION) FOR BMP-2	EL. 338.00
DMH-10 (DIVERSION) FOR BMP-3	EL. 337.50

NOTES:
1. PIPE CONFIGURATION IS REPRESENTATIVE. SEE PLANS AND DETAILS FOR PIPE CONFIGURATION.

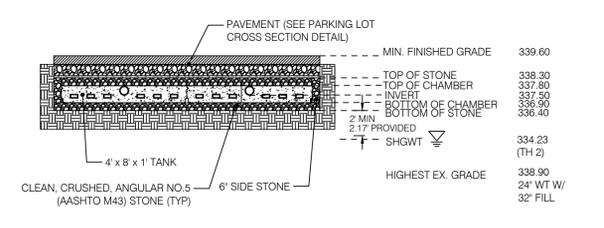
BMP 1 INFILTRATION SYSTEM PLAN VIEW



BMP 2 INFILTRATION SYSTEM PLAN VIEW



DIVERSION MANHOLE WEIR WALL



BMP 1 INFILTRATION SYSTEM CROSS SECTION



BMP 2 INFILTRATION SYSTEM CROSS SECTION

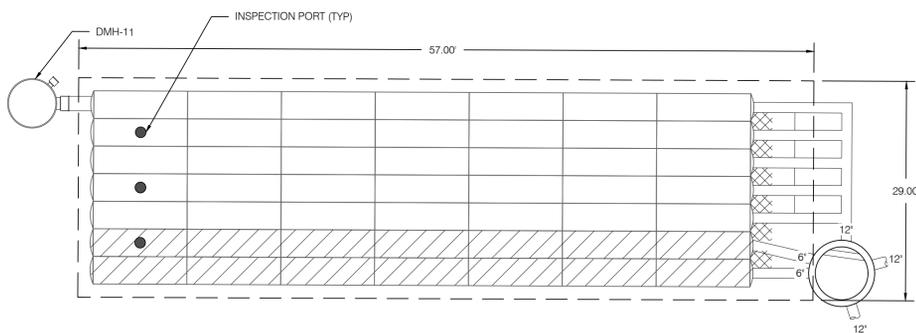


BMP 3 INFILTRATION SYSTEM CROSS SECTION



STORMWATER BMP NOTES:

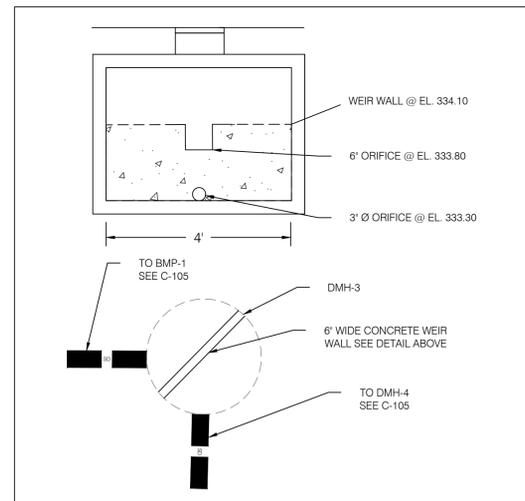
- ALL FILL BELOW AND WITHIN 6 INCHES OF THE UNDERGROUND INFILTRATION SYSTEM SIDES SHALL BE REMOVED AND REPLACED WITH C-33 SAND



BMP 3 INFILTRATION SYSTEM PLAN VIEW

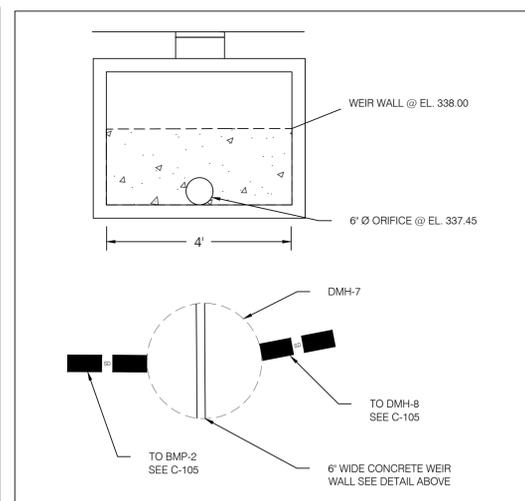
NOTES

DRAINAGE CHAMBERS SHALL BE SHEA LEACHING CHAMBERS OF PROVIDED DIMENSIONS OR APPROVAL EQUAL. ANY SUBSTITUTION OF MATERIALS FROM PROVIDED PLANS ARE SUBJECT TO WRITTEN ENGINEERING APPROVAL.
MANHOLE SIZE TO BE DETERMINED BY SITE DESIGN ENGINEER.
DUE TO THE ADAPTATION OF THIS CHAMBER SYSTEM TO SPECIFIC SITE AND DESIGN CONSTRAINTS IT MAY BE NECESSARY TO CUT AND COUPLE ADDITIONAL PIPE TO STANDARD MANHOLE COMPONENTS IN THE FIELD.
THE SITE DESIGN ENGINEER MUST REVIEW ELEVATIONS AND IF NECESSARY ADJUST GRADING TO ENSURE THE CHAMBER COVER REQUIREMENTS ARE MET.
THIS CHAMBER SYSTEM WAS DESIGNED WITHOUT SITE-SPECIFIC INFORMATION ON SOIL CONDITIONS OR BEARING CAPACITY. THE SITE DESIGN ENGINEER IS RESPONSIBLE FOR DETERMINING THE SUITABILITY OF THE SOIL AND PROVIDING THE BEARING CAPACITY OF THE INSTANT SOILS. THE BASE STONE DEPTH MAY BE INCREASED OR DECREASED ONCE THIS INFORMATION IS PROVIDED.
NOT FOR CONSTRUCTION
THIS LAYOUT IS FOR DIMENSIONAL PURPOSES ONLY TO PROVE CONCEPT & THE REQUIRED STORAGE VOLUME CAN BE ACHIEVED ON SITE.



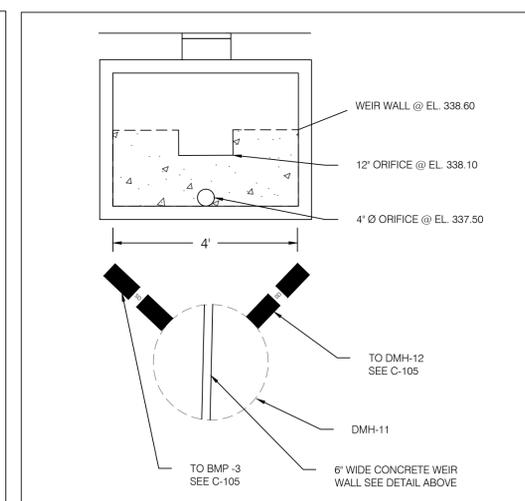
OUTLET CONTROL STRUCTURE - DMH-3

Scale: N. T. S.



OUTLET CONTROL STRUCTURE - DMH-7

Scale: N. T. S.



OUTLET CONTROL STRUCTURE - DMH-11

Scale: N. T. S.

NEI
Narragansett
Engineering Inc.
Civil - Survey Structural Environmental Design
3102 East Main Road, Portsmouth RI 02871
Tel. 401.683.6630 www.nei-cds.com

SHEET TITLE 380 KING ST - CONDOMINIUMS SOIL PROFILES & BMP DETAILS

Property Record:
Principal Parcel:
330 King St, Franklin, MA 02038
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Book/Page: 8145-260
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N/F: Marguerite Margaret, TR, Marguerite Family Trust
Book/Page 8145-260
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Plat: 303, Lot:44, Area: 0.928 Acres, Zone: C-II
N/F: DECOY INC.

PROJECT NO.	DATE	BY
24.0168	10.10.24	NKH

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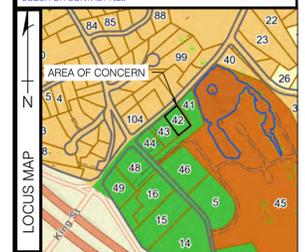
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C-205