

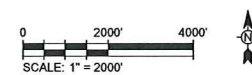
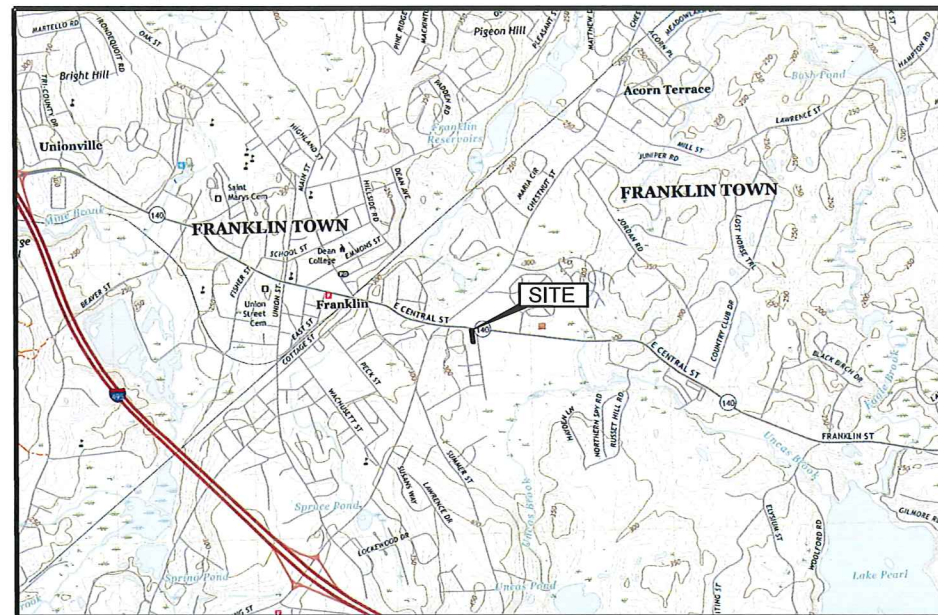
# NYLAH CROSSING LLC SITE PLAN REVIEW 240 EAST CENTRAL STREET & 9 LEWIS STREET FRANKLIN, MA

TOWN OF FRANKLIN  
TOWN CLERK  
2024 MAR 26 A 11: 01  
RECEIVED



118 TURNPIKE ROAD, SUITE 200  
SOUTHBOROUGH, MA 01772  
CONTACT@MPDCONSULTANTS.COM

LOCUS MAP



| REVISIONS |      |             |
|-----------|------|-------------|
| No.       | DATE | DESCRIPTION |
|           |      |             |
|           |      |             |
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**PROJECT**  
**NYLAH CROSSING LLC**

240 EAST CENTRAL STREET &  
9 LEWIS STREET  
FRANKLIN, MASSACHUSETTS 02038

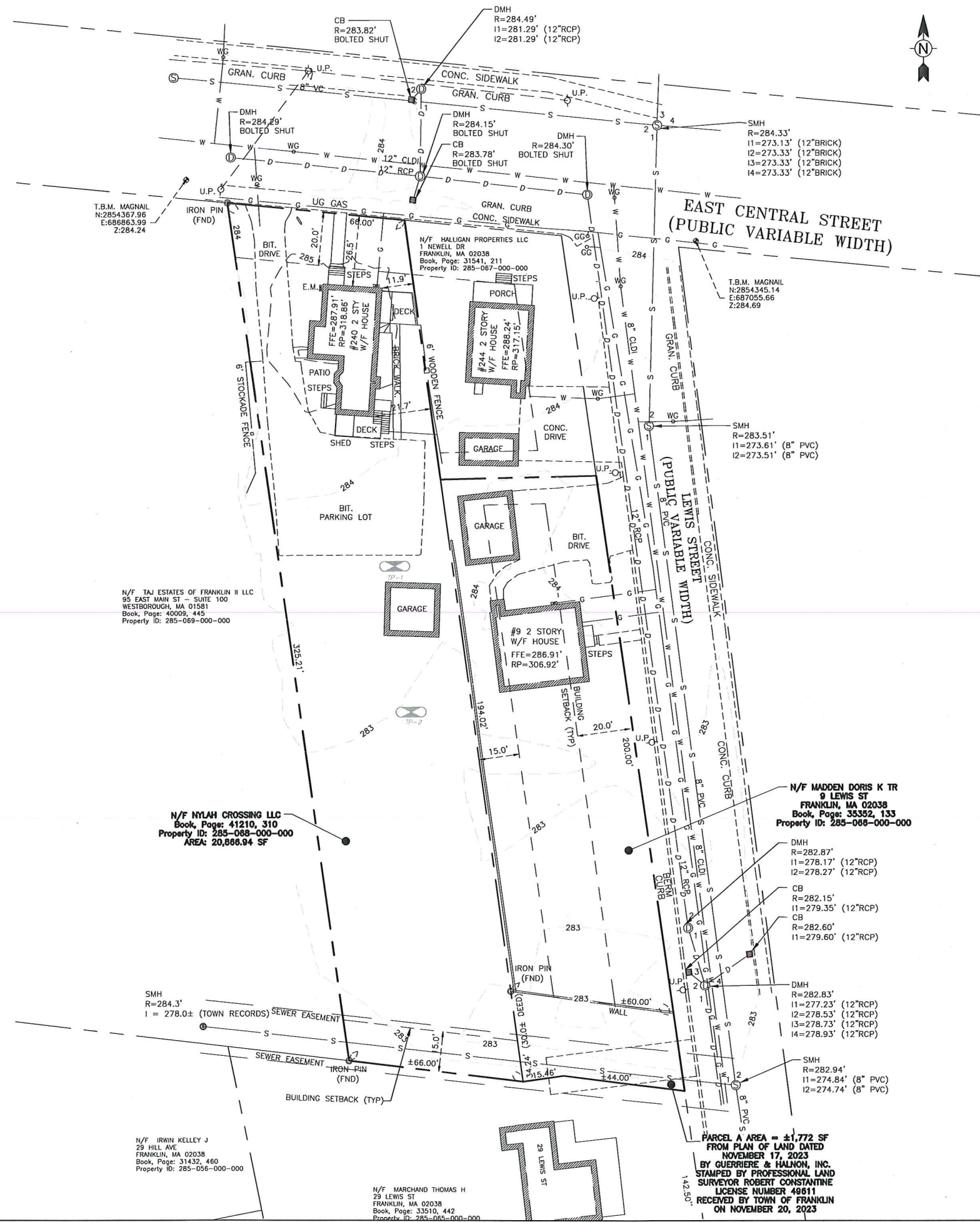
**PROPERTY OWNER:**  
NYLAH CROSSING LLC  
95 EAST MAIN STREET, SUITE 100  
WESTBOROUGH, MASSACHUSETTS 01581

| SHEET NO.      | SHEET TITLE                                   |
|----------------|---|
| --             | COVER SHEET                                   |
| V-101          | EXISTING CONDITIONS                           |
| C-101          | DEMOLITION, EROSION AND SEDIMENT CONTROL PLAN |
| C-102          | LAYOUT AND MATERIALS PLAN                     |
| C-103          | GRADING PLAN                                  |
| C-104          | UTILITIES PLAN                                |
| C-105          | LANDSCAPING PLAN                              |
| C-106          | EROSION CONTROL DETAILS                       |
| C-107 to C-111 | CONSTRUCTION DETAILS                          |

SHEET TITLE  
**COVER PAGE**

|             |                |             |     |
|-------------|----------------|-------------|-----|
| DATE:       | 03/13/2024     | DRAWING NO. | --- |
| DRAWN BY:   | CMS            |             |     |
| CHECKED BY: | JG             |             |     |
| FILE:       | NYLAHCROSS.dwg |             |     |





| LEGEND                 |     |
|------------------------|-----|
| SUBJECT PARCEL         | --- |
| PROPERTY LINE          | --- |
| EXISTING MINOR CONTOUR | --- |
| EXISTING MAJOR CONTOUR | --- |

- GENERAL NOTES**
- EXISTING CONDITIONS SURVEY COMPLETED BY TODD CHAPIN PROFESSIONAL LICENSE 37558 AND SUPPLEMENTED FROM PLAN OF LAND DATED NOVEMBER 17, 2023 BY GUERRIERE & HALNON, INC. STAMPED BY PROFESSIONAL LAND SURVEYOR ROBERT CONSTANTINE LICENSE NUMBER 49611 RECEIVED BY TOWN OF FRANKLIN ON NOVEMBER 20, 2023.
  - THIS LAND IS ZONED COMMERCIAL I (C1).
  - THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL UTILITIES IN THE FIELD. DISCREPANCIES SHALL BE IMMEDIATELY REPORTED TO THE DESIGN ENGINEER UPON DISCOVERY.
  - REFER TO FRANKLIN ASSESSORS PARCEL NUMBER 265-068-000.
  - THE SITE IS LOCATED IN THE TOWN OF FRANKLIN WATER RESOURCE DISTRICT.
  - REFERENCED DEED BOOK 41210 PAGE 310 PLAN NUMBER 2488 OF 1908 RECORDED AT NORFOLK COUNTY REGISTRY OF DEEDS, PROPERTY OWNED BY NYLAH CROSSING LLC.

| ZONING SUMMARY                                       |                 |                    |
|--|-----------------|--------------------|
| ZONING DISTRICT: COMMERCIAL I                        |                 |                    |
| WATER RESOURCE DISTRICT                              |                 |                    |
| MIXED USE DEVELOPMENT PROJECT                        |                 |                    |
| CRITERIA   | REQUIRED        | EXISTING           |
| MIN. LOT AREA  | 5,000 SF        | ±34,567 SF         |
| MIN. LOT FRONTAGE                                    | 50 FT           | 66 FT              |
| MIN. LOT DEPTH                                       | 50 FT           | >50 FT             |
| MIN. LOT WIDTH                                       | 45 FT           | 64.37 FT           |
| MIN. FRONT YARD <sup>(1)</sup>                       | 20 FT           | 13 FT              |
| MIN. SIDE YARD <sup>(14)</sup>                       | 10 FT           | 4.6 FT             |
| MIN. REAR YARD                                       | 15 FT           | >15 FT             |
| MAX. BUILDING HEIGHT <sup>(15)</sup>                 | 3 STORIES/40 FT | 2 STORIES/33.86 FT |
| MAX. % OF LOT UPLAND COVERED BY STRUCTURES           | 80%             | 8.68%              |
| MAX. % OF LOT UPLAND COVERED BY STRUCTURES**         | 90%             | -                  |
| MAX. % IMPERVIOUS COVER WATER RESOURCE DISTRICT AREA | 80%             | 13.24%             |

- BUT NO NEW STRUCTURE SHALL BE REQUIRED TO PROVIDE A DEEPER YARD THAN THAT EXISTING ON THAT PARCEL UPON ADOPTION OF THIS AMENDMENT.
- PERMITTED RESIDENTIAL USES MUST OBSERVE REQUIREMENTS OF THE GENERAL RESIDENTIAL V DISTRICT FOR RESIDENTIAL USE BUILDING ONLY. MIXED-USE BUILDINGS ARE EXEMPT FROM THIS REQUIREMENT.
- THE 10-FOOT SIDE SETBACK IS ONLY REQUIRED ON ONE SIDE OF LOT; IF LOT ABUTS A RESIDENTIAL DISTRICT, A 20-FOOT SETBACK IS REQUIRED ON THE ABUTTING SIDE.
- BUILDINGS UP TO 50 FEET IN HEIGHT, REGARDLESS OF THE NUMBER OF STORIES, MAY BE PERMITTED BY A SPECIAL PERMIT FROM THE PLANNING BOARD.
- NON-RESIDENTIAL IMPERVIOUS COVER IN THE TOWN OF FRANKLIN WATER DISTRICT SHALL NOT EXCEED 80%.

**FIELD RESULTS:**  
BY CHRIS STANTON

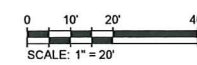
**TEST PITS**

TP-1  
0'-4" LOAMY SAND, 10YR 3/3, HSG A (TOPSOIL)  
4'-18" LOAMY SAND, 7.5Y 5/4, HSG A (SUBSOIL)  
18"-96" FINE SAND 10% COBBLES AND STONES, 10YR 5/1, HSG A (SUBSTRATUM)  
NO WEEDING  
NO REDOX  
NO REFUSAL

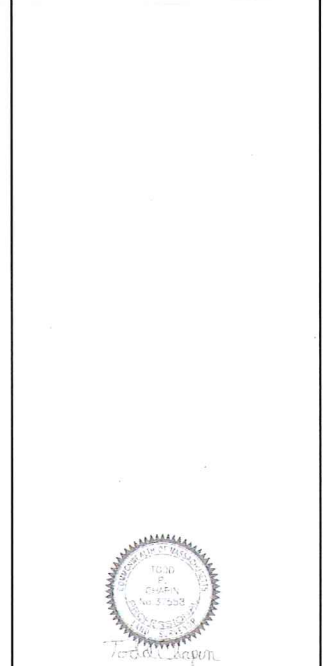
TP-2  
0'-4" LOAMY SAND, 10YR 3/3, HSG A (TOPSOIL)  
4'-36" LOAMY SAND, 7.5Y 5/4, HSG A (SUBSOIL)  
36"-96" FINE SAND 10% COBBLES AND STONES, 10YR 5/1, HSG A (SUBSTRATUM)  
NO WEEDING  
NO REDOX  
NO REFUSAL

I CERTIFY THAT I AM CURRENTLY APPROVED BY THE DEPARTMENT OF ENVIRONMENTAL PROTECTION PURSUANT 310 CMR 15.017 TO CONDUCT SOIL EVALUATIONS AND THAT THE ABOVE ANALYSIS HAS BEEN PERFORMED BY ME CONSISTENT WITH THE REQUIRED TRAINING, EXPERTISE, AND EXPERIENCE DESCRIBED IN 310 CMR 15.017. I FURTHER CERTIFY THAT THE RESULTS OF MY SOIL EVALUATION, AS INDICATED ON THE ATTACHED SOIL EVALUATION FORM, ARE ACCURATE AND IN ACCORDANCE WITH 310 CMR 15.100 THROUGH 15.107.

CHRIS STANTON  
SOIL EVALUATOR# SE14267



118 TURNPIKE ROAD, SUITE 200  
SOUTHBOROUGH, MA 01772  
CONTACT@MPDCONSULTANTS.COM



| REVISIONS |      |             |
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**PROJECT**  
NYLAH CROSSING LLC

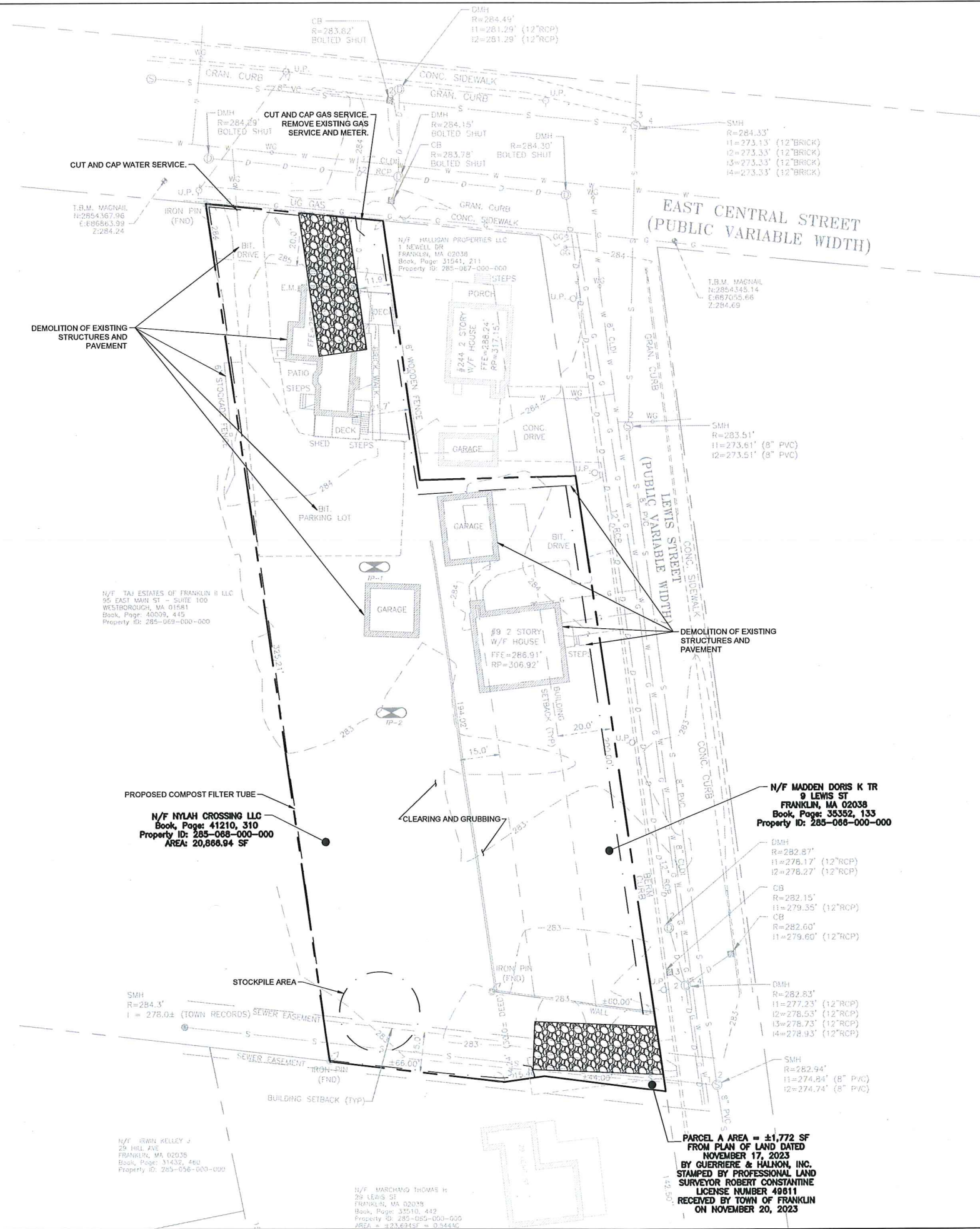
240 EAST CENTRAL STREET &  
9 LEWIS STREET  
FRANKLIN, MASSACHUSETTS 02038

**PROPERTY OWNER:**  
NYLAH CROSSING LLC  
95 EAST MAIN STREET, SUITE 100  
WESTBOROUGH, MASSACHUSETTS 01581

**SHEET TITLE**  
EXISTING CONDITIONS

|           |            |             |                |
|-----------|------------|-------------|----------------|
| SCALE:    | 1" = 20'   | DRAWING NO. | V-101          |
| DATE:     | 12/12/2023 | CHECKED BY: | TC             |
| DRAWN BY: | MT         | FILE:       | NYLAHCROSS.dwg |





| LEGEND                       |     |
|------------------------------|-----|
| SUBJECT PARCEL               | --- |
| PROPERTY LINE                | --- |
| EXISTING MINOR CONTOUR       | --- |
| EXISTING MAJOR CONTOUR       | --- |
| LINEAR SEDIMENTATION CONTROL | --- |

- GENERAL NOTES**
- ALL SLOPES, UNLESS OTHERWISE SPECIFIED, SHALL BE LOAMED AND SEEDED FOR STABILIZATION AS SOON AS POSSIBLE TO PREVENT EROSION INTO WETLAND RESOURCE AREAS, ADJUTING PROPERTIES, OR PUBLIC WAYS. EROSION CONTROL BLANKETS ARE REQUIRED FOR ALL 2H:1V SLOPES. SLOPES MAY NOT EXCEED 2H:1V.
  - DIG SAFE NOTE: IN ACCORDANCE WITH MGL CH. 82, SEC. 40 INCLUDING AMENDMENTS, ALL CONTRACTORS SHALL NOTIFY UTILITY COMPANIES AND GOVERNMENT AGENCIES, IN WRITING, OF THE INTENT TO EXCAVATE, BLAST, DEMOLISH, BORE, OR PERFORM OTHER EARTH MOVING OPERATIONS NO LESS THAN 72 HOURS AND NO MORE THAN 30 DAYS PRIOR TO THE COMMENCEMENT OF SUCH WORK (EXCLUSIVE OF SATURDAYS, SUNDAYS, AND LEGAL HOLIDAYS) OR CALL "DIG SAFE" AT 1-888-DIG-SAFE.
  - LOCATION OF UNDERGROUND UTILITIES ON THIS PLAN ARE APPROXIMATE ONLY AND ARE BASED ON A COMBINATION OF ACTUAL FIELD LOCATIONS OF VISIBLE STRUCTURES AND RECORD PLAN COMPILATIONS. EXISTING UTILITY LOCATIONS AND ELEVATIONS SHOWN SHALL BE VERIFIED BY CONTRACTOR PRIOR TO CONSTRUCTION. TEST PITS SHALL BE PERFORMED BY CONTRACTOR PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL INFORM THE DESIGN ENGINEER OF ANY DISCREPANCIES PRIOR TO THE START OF ANY CONSTRUCTION.



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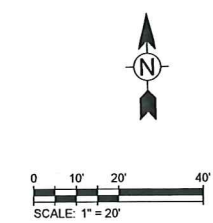


| REVISIONS |      |             |
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**PROJECT**  
**NYLAH CROSSING LLC**  
 240 EAST CENTRAL STREET &  
 9 LEWIS STREET  
 FRANKLIN, MASSACHUSETTS 02038  
**PROPERTY OWNER:**  
 NYLAH CROSSING LLC  
 95 EAST MAIN STREET, SUITE 100  
 WESTBOROUGH, MASSACHUSETTS 01581

**SHEET TITLE**  
**DEMOLITION, EROSION  
 AND SEDIMENT  
 CONTROL PLAN**

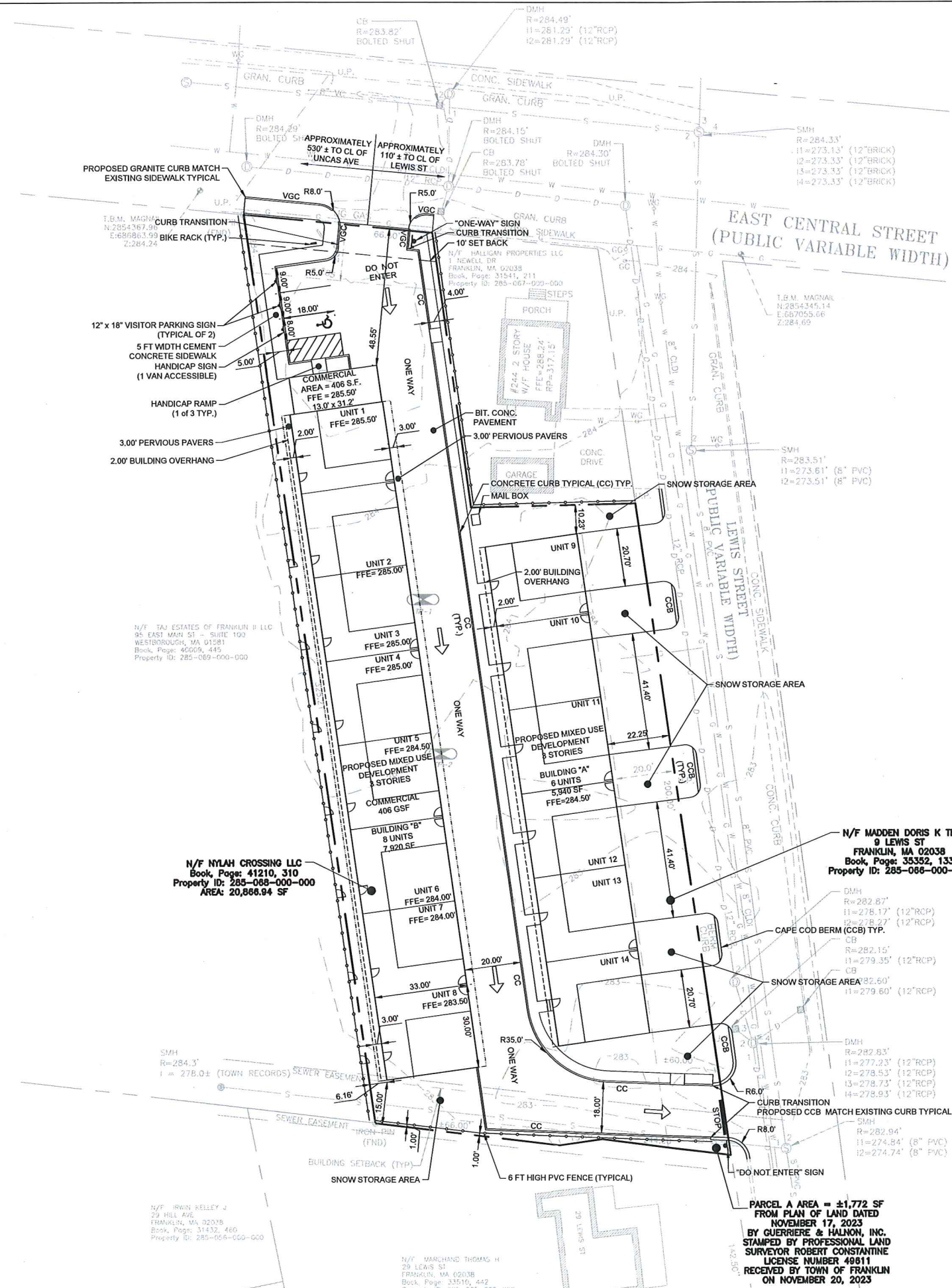
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| DATE:       | 03/13/2024     |             |       |
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| CHECKED BY: | JG             |             |       |
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**CONSTRUCTION NOTES**

- AT THE END OF CONSTRUCTION, ALL DRAINAGE STRUCTURES ARE TO BE CLEANED OF SILT, STONES AND OTHER DEBRIS.
- CONTRACTOR SHALL REPORT SIGNIFICANT CONFLICTS TO THE OWNER AND DESIGN ENGINEER.
- THE CONTRACTOR SHALL VERIFY EXISTING GRADES IN THE FIELD AND REPORT ANY DISCREPANCIES IMMEDIATELY TO THE OWNER AND DESIGN ENGINEER.
- EXCAVATION REQUIRED WITHIN THE PROXIMITY OF EXISTING UTILITY LINES SHALL BE DONE BY HAND. CONTRACTOR SHALL REPAIR ANY DAMAGE TO EXISTING UTILITY LINES OR STRUCTURES INCURRED DURING CONSTRUCTION OPERATIONS.
- PITCH EVENLY BETWEEN SPOT GRADES. GRADE ALL AREAS TO DRAIN. ALL PAVED AREAS MUST PITCH TO DRAIN AT A MINIMUM OF 1/8" PER FOOT UNLESS OTHERWISE SPECIFIED. ANY DISCREPANCIES NOT ALLOWING THIS MINIMUM PITCH SHALL BE REPORTED TO THE OWNER OR HIS REPRESENTATIVE PRIOR TO CONTINUING WORK.
- ALL SITE WORK SHALL CONFORM TO THE CONTRACT DOCUMENTS AND SHALL COMPLY WITH APPLICABLE CODES AND REGULATIONS.
- DURING THE PROGRESS OF THE WORK, THE CONTRACTOR MAY BE REQUIRED TO EXCAVATE ADDITIONAL TEST PITS FOR THE PURPOSE OF LOCATING UNDERGROUND UTILITIES OR STRUCTURES AS AN AID IN ESTABLISHING THE PRECISE LOCATION OF NEW WORK. THIS WORK IS TO BE PERFORMED AT NO ADDITIONAL COST TO THE OWNER. TEST PITS SHALL BE BACKFILLED, AS SOON AS THE DESIRED INFORMATION HAS BEEN OBTAINED.
- PROTECT STRUCTURES, UTILITIES, SIDEWALKS, PAVEMENTS AND OTHER FACILITIES FROM DAMAGE CAUSED BY SETTLEMENT, LATERAL MOVEMENT, UNDERMINING, WASHOUT AND OTHER HAZARDS CREATED BY CONTRACTOR OPERATIONS.
- PAVING, CONCRETE WORK AND BASE COURSE PREPARATION SHALL BE DONE ONLY AFTER EXCAVATION AND CONSTRUCTION WORK WHICH MIGHT INJURE THEM HAS BEEN COMPLETED. DAMAGE CAUSED DURING CONSTRUCTION SHALL BE REPAIRED BEFORE ACCEPTANCE.
- PAVEMENT OR BASE MATERIALS SHALL NOT BE PLACED ON A MUDDY OR FROZEN SUBGRADE.
- ESTABLISHMENT OF GRADES, GRADE CONTROL, AND CONFORMANCE TO REQUIRED GRADE TOLERANCES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- PROTECT GRADED, FINISHED OR PAVED AREAS FROM DAMAGE AND KEEP THEM FREE OF TRASH AND DEBRIS RESULTING FROM CONSTRUCTION OPERATIONS. REPAIR AND RE-ESTABLISH GRADES IN SETTLED, ERODED AND RUTTED AREAS.
- PAVEMENT EXCAVATED DURING UTILITY CONSTRUCTION, WHETHER ON THE SITE OR ADJACENT PROPERTIES, SHALL BE RESTORED AND MATCHED WITH EXACTLY THE SAME MATERIALS AND TOLERANCES AS PRIOR TO DISRUPTION, AT NO ADDITIONAL COST TO THE OWNER, OR ADJACENT PROPERTY OWNERS.
- STONE USED FOR MACHINE PLACED RIP-RAP SHALL BE REASONABLY WELL GRADED, HARD, DURABLE, ANGULAR IN SHAPE, RESISTANT TO WEATHERING AND FREE FROM ORGANIC MATERIAL. ROUNDED STONES OR BOULDERS ARE NOT ACCEPTABLE. THE MINIMUM WEIGHT OF THE STONE SHALL BE 155 POUNDS PER CUBIC FOOT. STONE SHALL BE PLACED IN CONFORMANCE WITH THE LINES, GRADES AND THICKNESS SHOWN ON THE DRAWINGS.
- AT ALL LOCATIONS WHERE EXISTING CURBING OR PAVEMENT ABUTS NEW CONSTRUCTION, THE EDGE OF THE EXISTING CURB OR PAVEMENT SHALL BE SAW CUT TO A CLEAN, SMOOTH EDGE. BLEND NEW PAVEMENT, CURBS AND EARTHWORK SMOOTHLY INTO EXISTING BY MATCHING LINES, GRADES AND JOINTS.
- ALL RIP RAP STONE SHALL BE HAND CHINKED AND SHALL CONFORM TO MASSACHUSETTS HIGHWAY DEPARTMENT STANDARDS.
- PROTECT PROPOSED INFILTRATION BASINS FROM SEDIMENTATION THROUGHOUT CONSTRUCTION OPERATIONS. INFILTRATION BASINS ARE NOT TO BE USED UNTIL DRAINAGE SYSTEM IS INSTALLED AND FUNCTIONAL.



**LEGEND**

|                        |          |
|------------------------|----------|
| SUBJECT PARCEL         | ---      |
| PROPERTY LINE          | ---      |
| EXISTING MINOR CONTOUR | ---      |
| EXISTING MAJOR CONTOUR | ---      |
| MAJOR CONTOUR          | ---      |
| SPOT GRADE             | x 193.75 |

- GENERAL NOTES**
- EXISTING CONDITIONS SURVEY COMPLETED BY TODD CHAPIN PROFESSIONAL LICENSE 37559 AND SUPPLEMENTED FROM PLAN OF LAND DATED NOVEMBER 17, 2023 BY GUERRIERE & HALNON, INC. STAMPED BY PROFESSIONAL LAND SURVEYOR ROBERT CONSTANTINE LICENSE NUMBER 49611 RECEIVED BY TOWN OF FRANKLIN ON NOVEMBER 20, 2023.
  - THIS LAND IS ZONED COMMERCIAL I (CI).
  - ALL LAND WILL BE COMBINED PRIOR TO LAND ENDORSEMENT
  - THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL UTILITIES IN THE FIELD. DISCREPANCIES SHALL BE IMMEDIATELY REPORTED TO THE DESIGN ENGINEER UPON DISCOVERY.
  - REFER TO FRANKLIN ASSESSORS PARCEL NUMBER 265-068-000.
  - THE SITE IS LOCATED IN THE TOWN OF FRANKLIN WATER RESOURCE DISTRICT.
  - REFERENCED DEED BOOK 41210 PAGE 310 PLAN NUMBER 2488 OF 1908 RECORDED AT NORFOLK COUNTY REGISTRY OF DEEDS. PROPERTY OWNED BY NYLAH CROSSING LLC.

**ZONING SUMMARY**

ZONING DISTRICT: COMMERCIAL I

WATER RESOURCE DISTRICT

MIXED USE DEVELOPMENT PROJECT

| CRITERIA   | REQUIRED        | EXISTING           | PROPOSED        |
|--|-----------------|--------------------|-----------------|
| MIN. LOT AREA  | 5,000 SF        | ±34,567 SF         | ±34,567 SF      |
| MIN. LOT FRONTAGE                                    | 50 FT           | 66 FT              | 66 FT           |
| MIN. LOT DEPTH                                       | 50 FT           | >50 FT             | >50 FT          |
| MIN. LOT WIDTH                                       | 45 FT           | 64.4 FT            | 64.4 FT         |
| MIN. FRONT YARD <sup>(1)</sup>                       | 20 FT           | 13 FT              | 20 FT           |
| MIN. SIDE YARD <sup>(14)</sup>                       | 10 FT           | 4.6 FT             | 6.16 FT         |
| MIN. REAR YARD                                       | 15 FT           | >15 FT             | 15 FT           |
| MAX. BUILDING HEIGHT <sup>(15)</sup>                 | 3 STORIES/40 FT | 2 STORIES/33.66 FT | 3 STORIES/40 FT |
| MAX. % OF LOT UPLAND COVERED BY STRUCTURES           | 80%             | 8.68%              | -               |
| MAX. % OF LOT UPLAND COVERED BY "STRUCTURES"         | 90%             | -                  | 40.8%           |
| MAX. % IMPERVIOUS COVER WATER RESOURCE DISTRICT AREA | 80%             | 13.24%             | 78.6%           |

- BUT NO NEW STRUCTURE SHALL BE REQUIRED TO PROVIDE A DEEPER YARD THAN THAT EXISTING ON THAT PARCEL UPON ADOPTION OF THIS AMENDMENT.
- PERMITTED RESIDENTIAL USES MUST OBSERVE REQUIREMENTS OF THE GENERAL RESIDENTIAL V DISTRICT FOR RESIDENTIAL USE BUILDING ONLY. MIXED-USE BUILDINGS ARE EXEMPT FROM THIS REQUIREMENT.
- THE 10-FOOT SIDE SETBACK IS ONLY REQUIRED ON ONE SIDE OF LOT, IF LOT ABUTS A RESIDENTIAL DISTRICT, A 20-FOOT SETBACK IS REQUIRED ON THE ABUTTING SIDE.
- BUILDINGS UP TO 50 FEET IN HEIGHT, REGARDLESS OF THE NUMBER OF STORIES, MAY BE PERMITTED BY A SPECIAL PERMIT FROM THE PLANNING BOARD.
- NON-RESIDENTIAL IMPERVIOUS COVER IN THE TOWN OF FRANKLIN WATER DISTRICT SHALL NOT EXCEED 80%.

**COMMERCIAL I**

185 ATTACHMENT 7-(3) NO MORE THAN ONE DWELLING PER 2,250 SF OF LOT AREA MAY BE PERMITTED.

34,567 SF LOT / 2,250 SF = 15 MAX DWELLINGS.

PROPOSED - (14) THREE BEDROOM UNITS.

**PARKING REQUIREMENTS**

| CRITERIA  | REQUIRED | PROPOSED |
|---|----------|----------|
| RESIDENTIAL UNIT PARKING 1.5 PER UNIT                   | 21       | 28       |
| COMMERCIAL AREA PARKING 1 PER 500 S.F. TOTAL = 40± S.F. | 1        | 3        |
| TOTAL SPACES  | 22       | 31       |

**DESIGNATED PARKING**

|                                   |    |
|-----------------------------------|----|
| VISITOR PARKING                   | 2  |
| DELIVERY PARKING                  | 1  |
| HANDICAP PARKING (VAN ACCESSIBLE) | 1  |
| 2 SPACES PER UNIT (14 UNITS)      | 28 |



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SOUTH-BOROUGH, MA 01772

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**REVISIONS**

| No. | DATE | DESCRIPTION |
|-----|------|-------------|
|     |      |             |
|     |      |             |
|     |      |             |

**PROJECT**

**NYLAH CROSSING LLC**

240 EAST CENTRAL STREET &  
9 LEWIS STREET  
FRANKLIN, MASSACHUSETTS 02038

**PROPERTY OWNER:**

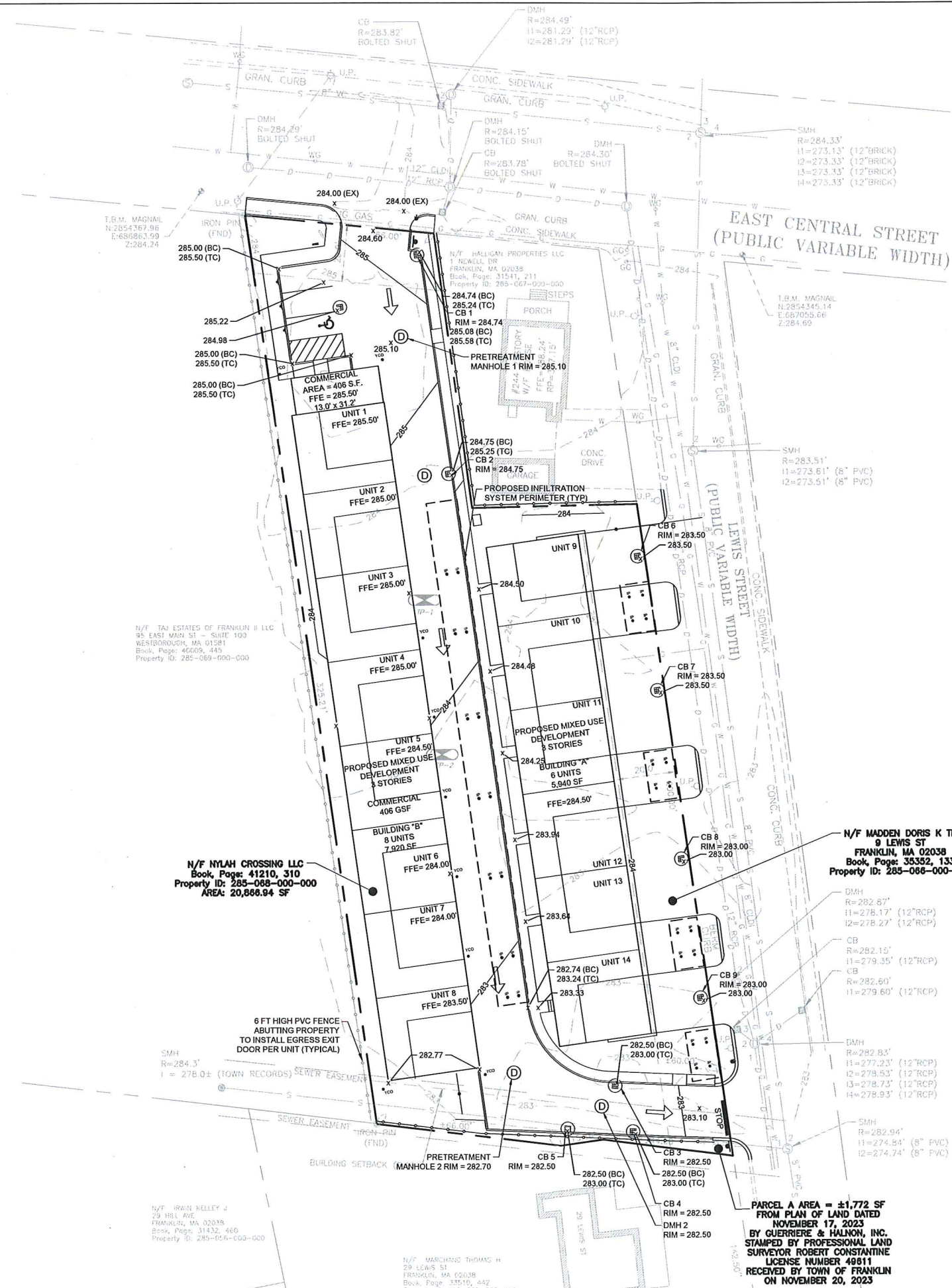
NYLAH CROSSING LLC  
95 EAST MAIN STREET, SUITE 100  
WESTBOROUGH, MASSACHUSETTS 01581

**SHEET TITLE**

**LAYOUT AND MATERIALS PLAN**

|             |                |             |       |
|-------------|----------------|-------------|-------|
| SCALE:      | 1" = 20'       | DRAWING NO. | C-102 |
| DATE:       | 03/13/2024     |             |       |
| DRAWN BY:   | CMS            |             |       |
| CHECKED BY: | JG             |             |       |
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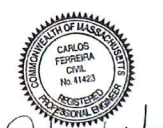
| LEGEND                 |          |
|------------------------|----------|
| SUBJECT PARCEL         | ---      |
| PROPERTY LINE          | ---      |
| EXISTING MINOR CONTOUR | ---      |
| EXISTING MAJOR CONTOUR | ---      |
| MAJOR CONTOUR          | ---      |
| SPOT GRADE             | x 193.75 |

**CONSTRUCTION NOTES**

- EXISTING CONDITIONS SURVEY COMPLETED BY TODD CHAPIN PROFESSIONAL LICENSE 37558 AND SUPPLEMENTED FROM PLAN OF LAND DATED NOVEMBER 17, 2023 BY GUERRIERE & HALNOR, INC. STAMPED BY PROFESSIONAL LAND SURVEYOR ROBERT CONSTANTINE LICENSE NUMBER 49611 RECEIVED BY TOWN OF FRANKLIN ON NOVEMBER 20, 2023.
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- CONTRACTOR SHALL REPORT SIGNIFICANT CONFLICTS TO THE OWNER AND DESIGN ENGINEER.
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- ALL SITE WORK SHALL CONFORM TO THE CONTRACT DOCUMENTS AND SHALL COMPLY WITH APPLICABLE CODES AND REGULATIONS.
- DURING THE PROGRESS OF THE WORK, THE CONTRACTOR MAY BE REQUIRED TO EXCAVATE ADDITIONAL TEST PITS FOR THE PURPOSE OF LOCATING UNDERGROUND UTILITIES OR STRUCTURES AS AN AID IN ESTABLISHING THE PRECISE LOCATION OF NEW WORK. THIS WORK IS TO BE PERFORMED AT NO ADDITIONAL COST TO THE OWNER. TEST PITS SHALL BE BACKFILLED, AS SOON AS THE DESIRED INFORMATION HAS BEEN OBTAINED.
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- STONE USED FOR MACHINE PLACED RIP-RAP SHALL BE REASONABLY WELL GRADED, HARD, DURABLE, ANGULAR IN SHAPE, RESISTANT TO WEATHERING AND FREE FROM ORGANIC MATERIAL. ROUNDED STONES OR BOULDERS ARE NOT ACCEPTABLE. THE MINIMUM WEIGHT OF THE STONE SHALL BE 155 POUNDS PER CUBIC FOOT. STONE SHALL BE PLACED IN CONFORMANCE WITH THE LINES, GRADES AND THICKNESS SHOWN ON THE DRAWINGS.
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- ALL RIP RAP STONE SHALL BE HAND CHINKED AND SHALL CONFORM TO MASSACHUSETTS HIGHWAY DEPARTMENT STANDARDS.
- PROTECT PROPOSED INFILTRATION CHAMBERS FROM SEDIMENTATION THROUGHOUT CONSTRUCTION OPERATIONS. INFILTRATION CHAMBERS ARE NOT TO BE USED UNTIL DRAINAGE SYSTEM IS INSTALLED AND FUNCTIONAL.



118 TURNPIKE ROAD, SUITE 200  
SOUTHBOROUGH, MA 01772  
CONTACT@MPDCONSULTANTS.COM



| REVISIONS |      |             |
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| No.       | DATE | DESCRIPTION |
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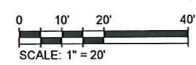
**PROJECT**  
NYLAH CROSSING LLC

240 EAST CENTRAL STREET &  
9 LEWIS STREET  
FRANKLIN, MASSACHUSETTS 02038

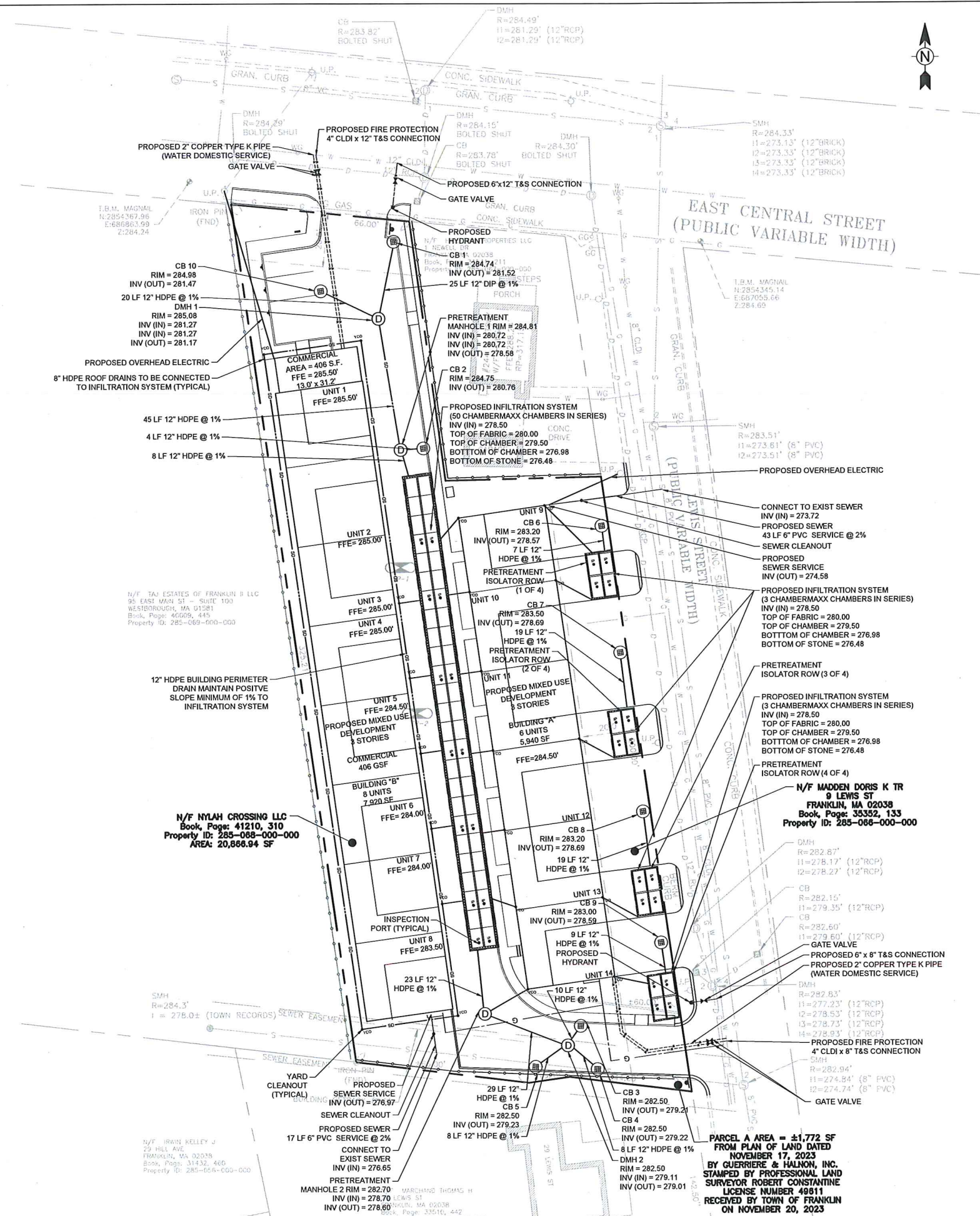
**PROPERTY OWNER:**  
NYLAH CROSSING LLC  
95 EAST MAIN STREET, SUITE 100  
WESTBOROUGH, MASSACHUSETTS 01581

**SHEET TITLE**  
GRADING PLAN

|             |                |             |       |
|-------------|----------------|-------------|-------|
| SCALE:      | 1" = 20'       | DRAWING NO. | C-103 |
| DATE:       | 03/13/2024     |             |       |
| DRAWN BY:   | CMS            |             |       |
| CHECKED BY: | JG             |             |       |
| FILE:       | NYLAHCROSS.dwg |             |       |







**LEGEND**

|                        |     |
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| SUBJECT PARCEL         | --- |
| PROPERTY LINE          | --- |
| EXISTING MINOR CONTOUR | --- |
| EXISTING MAJOR CONTOUR | --- |

- CONSTRUCTION NOTES**
- EXISTING CONDITIONS SURVEY COMPLETED BY TODD CHAPIN PROFESSIONAL LICENSE 37558 AND SUPPLEMENTED FROM PLAN OF LAND DATED NOVEMBER 17, 2023 BY GUERRIERE & HALKON, INC. STAMPED BY PROFESSIONAL LAND SURVEYOR ROBERT CONSTANTINE LICENSE NUMBER 49611 RECEIVED BY TOWN OF FRANKLIN ON NOVEMBER 20, 2023.
  - AT THE END OF CONSTRUCTION, ALL DRAINAGE STRUCTURES ARE TO BE CLEANED OF SILT, STONES AND OTHER DEBRIS.
  - CONTRACTOR SHALL REPORT SIGNIFICANT CONFLICTS TO THE OWNER AND DESIGN ENGINEER.
  - THE CONTRACTOR SHALL VERIFY EXISTING GRADES IN THE FIELD AND REPORT ANY DISCREPANCIES IMMEDIATELY TO THE OWNER AND DESIGN ENGINEER.
  - EXCAVATION REQUIRED WITHIN THE PROXIMITY OF EXISTING UTILITY LINES SHALL BE DONE BY HAND. CONTRACTOR SHALL REPAIR ANY DAMAGE TO EXISTING UTILITY LINES OR STRUCTURES INCURRED DURING CONSTRUCTION OPERATIONS.
  - PITCH EVENLY BETWEEN SPOT GRADES. GRADE ALL AREAS TO DRAIN. ALL PAVED AREAS MUST PITCH TO DRAIN AT A MINIMUM OF 1/8" PER FOOT UNLESS OTHERWISE SPECIFIED. ANY DISCREPANCIES NOT ALLOWING THIS MINIMUM PITCH SHALL BE REPORTED TO THE OWNER OR HIS REPRESENTATIVE PRIOR TO CONTINUING WORK.
  - ALL SITE WORK SHALL CONFORM TO THE CONTRACT DOCUMENTS AND SHALL COMPLY WITH APPLICABLE CODES AND REGULATIONS.
  - DURING THE PROGRESS OF THE WORK, THE CONTRACTOR MAY BE REQUIRED TO EXCAVATE ADDITIONAL TEST PITS FOR THE PURPOSE OF LOCATING UNDERGROUND UTILITIES OR STRUCTURES AS AN AID IN ESTABLISHING THE PRECISE LOCATION OF NEW WORK. THIS WORK IS TO BE PERFORMED AT NO ADDITIONAL COST TO THE OWNER. TEST PITS SHALL BE BACKFILLED, AS SOON AS THE DESIRED INFORMATION HAS BEEN OBTAINED.
  - PROTECT STRUCTURES, UTILITIES, SIDEWALKS, PAVEMENTS AND OTHER FACILITIES FROM DAMAGE CAUSED BY SETTLEMENT, LATERAL MOVEMENT, UNDERMINING, WASHOUT AND OTHER HAZARDS CREATED BY CONTRACTOR OPERATIONS.
  - PAVING, CONCRETE WORK AND BASE COURSE PREPARATION SHALL BE DONE ONLY AFTER EXCAVATION AND CONSTRUCTION WORK WHICH MIGHT INJURE THEM HAS BEEN COMPLETED. DAMAGE CAUSED DURING CONSTRUCTION SHALL BE REPAIRED BEFORE ACCEPTANCE.
  - PAVEMENT OR BASE MATERIALS SHALL NOT BE PLACED ON A MUDDY OR FROZEN SUBGRADE.
  - ESTABLISHMENT OF GRADES, GRADE CONTROL, AND CONFORMANCE TO REQUIRED GRADE TOLERANCES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
  - PROTECT GRADED, FINISHED OR PAVED AREAS FROM DAMAGE AND KEEP THEM FREE OF TRASH AND DEBRIS RESULTING FROM CONSTRUCTION OPERATIONS. REPAIR AND RE-ESTABLISH GRADES IN SETTLED, ERODED AND RUTTED AREAS.
  - PAVEMENT EXCAVATED DURING UTILITY CONSTRUCTION, WHETHER ON THE SITE OR ADJACENT PROPERTIES, SHALL BE RESTORED AND MATCHED WITH EXACTLY THE SAME MATERIALS AND TOLERANCES AS PRIOR TO DISRUPTION. AT NO ADDITIONAL COST TO THE OWNER, OR ADJACENT PROPERTY OWNERS.
  - STONE USED FOR MACHINE PLACED RIP-RAP SHALL BE REASONABLY WELL GRADED, HARD, DURABLE, ANGULAR IN SHAPE, RESISTANT TO WEATHERING AND FREE FROM ORGANIC MATERIAL. ROUNDED STONES OR BOULDERS ARE NOT ACCEPTABLE. THE MINIMUM WEIGHT OF THE STONE SHALL BE 155 POUNDS PER CUBIC FOOT. STONE SHALL BE PLACED IN CONFORMANCE WITH THE LINES, GRADES AND THICKNESS SHOWN ON THE DRAWINGS.
  - AT ALL LOCATIONS WHERE EXISTING CURBING OR PAVEMENT ABUTS NEW CONSTRUCTION, THE EDGE OF THE EXISTING CURB OR PAVEMENT SHALL BE SAW CUT TO A CLEAN, SMOOTH EDGE. BLEND NEW PAVEMENT, CURBS AND EARTHWORK SMOOTHLY INTO EXISTING BY MATCHING LINES, GRADES AND JOINTS.
  - ALL RIP RAP STONE SHALL BE HAND CHINKED AND SHALL CONFORM TO MASSACHUSETTS HIGHWAY DEPARTMENT STANDARDS.
  - PROTECT PROPOSED INFILTRATION CHAMBERS FROM SEDIMENTATION THROUGHOUT CONSTRUCTION OPERATIONS. INFILTRATION CHAMBERS ARE NOT TO BE USED UNTIL DRAINAGE SYSTEM IS INSTALLED AND FUNCTIONAL.



118 TURNPIKE ROAD, SUITE 200  
SOUTHBOROUGH, MA 01772  
CONTACT@MPDCONSULTANTS.COM



**REVISIONS**

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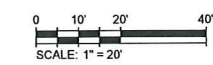
**PROJECT**  
NYLAH CROSSING LLC

240 EAST CENTRAL STREET &  
9 LEWIS STREET  
FRANKLIN, MASSACHUSETTS 02038

**PROPERTY OWNER:**  
NYLAH CROSSING LLC  
95 EAST MAIN STREET, SUITE 100  
WESTBOROUGH, MASSACHUSETTS 01581

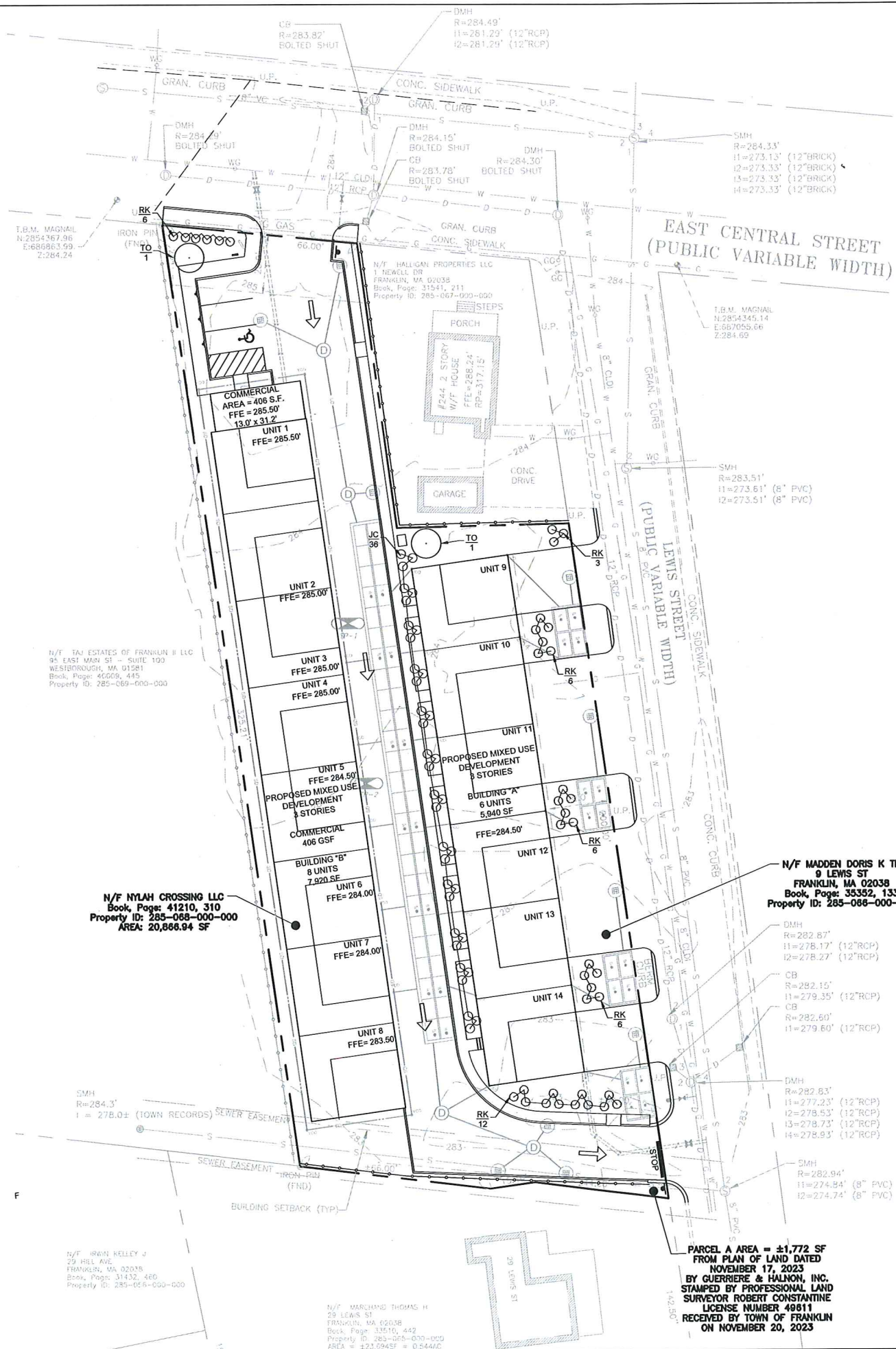
**SHEET TITLE**  
UTILITIES PLAN

|           |            |             |                |
|-----------|------------|-------------|----------------|
| SCALE:    | 1" = 20'   | DRAWING NO. | C-104          |
| DATE:     | 03/13/2024 | CHECKED BY: | JG             |
| DRAWN BY: | CMS        | FILE:       | NYLAHCROSS.dwg |



PARCEL A AREA = ±1,772 SF  
FROM PLAN OF LAND DATED  
NOVEMBER 17, 2023  
BY GUERRIERE & HALKON, INC.  
STAMPED BY PROFESSIONAL LAND  
SURVEYOR ROBERT CONSTANTINE  
LICENSE NUMBER 49611  
RECEIVED BY TOWN OF FRANKLIN  
ON NOVEMBER 20, 2023





| LEGEND                 |       |
|------------------------|-------|
| SUBJECT PARCEL         | _____ |
| PROPERTY LINE          | _____ |
| EXISTING MINOR CONTOUR | _____ |
| EXISTING MAJOR CONTOUR | _____ |
| MAJOR CONTOUR          | _____ |

| PLANT SCHEDULE |     |  |                       |
|----------------|-----|--|-----------------------|
| QUANTITY       | KEY | BOTANICAL NAME                         | COMMON NAME           |
| <b>SHRUBS</b>  |     |  |                       |
| 36             | JC  | JUNIPERUS CHINENSIS "PLUMOSA COMPACTA" | CHINESE JUNIPER       |
| 39             | RK  | RHODODENDRON KEISKEI                   | KEISKE'S RHODODENDRON |
| <b>TREES</b>   |     |  |                       |
| 2              | TO  | THUJA OCCIDENTALIS "PYRAMIDALIS"       | PYRAMIDAL ARBORVITAE  |



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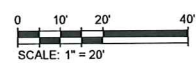
*Carlos Figueira*

| REVISIONS |      |             |
|-----------|------|-------------|
| No.       | DATE | DESCRIPTION |
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**PROJECT**  
**NYLAH CROSSING LLC**  
 240 EAST CENTRAL STREET &  
 9 LEWIS STREET  
 FRANKLIN, MASSACHUSETTS 02038  
**PROPERTY OWNER:**  
 NYLAH CROSSING LLC  
 95 EAST MAIN STREET, SUITE 100  
 WESTBOROUGH, MASSACHUSETTS 01581

**SHEET TITLE**  
**LANDSCAPING PLAN**

|             |                |              |
|-------------|----------------|--------------|
| SCALE:      | 1" = 20'       | DRAWING NO.  |
| DATE:       | 03/12/2023     |              |
| DRAWN BY:   | CMS            | <b>C-105</b> |
| CHECKED BY: | JG             |              |
| FILE:       | NYLAHCROSS.dwg |              |



PARCEL A AREA = ±1,772 SF  
 FROM PLAN OF LAND DATED  
 NOVEMBER 17, 2023  
 BY GUERRIERE & HALKON, INC.  
 STAMPED BY PROFESSIONAL LAND  
 SURVEYOR ROBERT CONSTANTINE  
 LICENSE NUMBER 40611  
 RECEIVED BY TOWN OF FRANKLIN  
 ON NOVEMBER 20, 2023

N/F NYLAH CROSSING LLC  
 Book, Page: 41210, 310  
 Property ID: 285-068-000-000  
 AREA: 20,866.94 SF

N/F MADDEN DORIS K TR  
 9 LEWIS ST  
 FRANKLIN, MA 02038  
 Book, Pages: 35352, 133  
 Property ID: 285-068-000-000

N/F TAJ ESTATES OF FRANKLIN II LLC  
 95 EAST MAIN ST - SUITE 100  
 WESTBOROUGH, MA 01581  
 Book, Page: 46608, 445  
 Property ID: 285-069-000-000

T.B.M. MAGNALL  
 N:2854367.96  
 E:668683.99  
 Z:284.24

T.B.M. MAGNALL  
 N:2854345.14  
 E:667055.66  
 Z:284.69

SMH  
 R=284.3'  
 I1=278.0'± (TOWN RECORDS) SEWER EASEMENT

DMH  
 R=282.87'  
 I1=278.17' (12"RCP)  
 I2=278.27' (12"RCP)  
 CB  
 R=282.15'  
 I1=279.35' (12"RCP)  
 CB  
 R=282.50'  
 I1=279.60' (12"RCP)

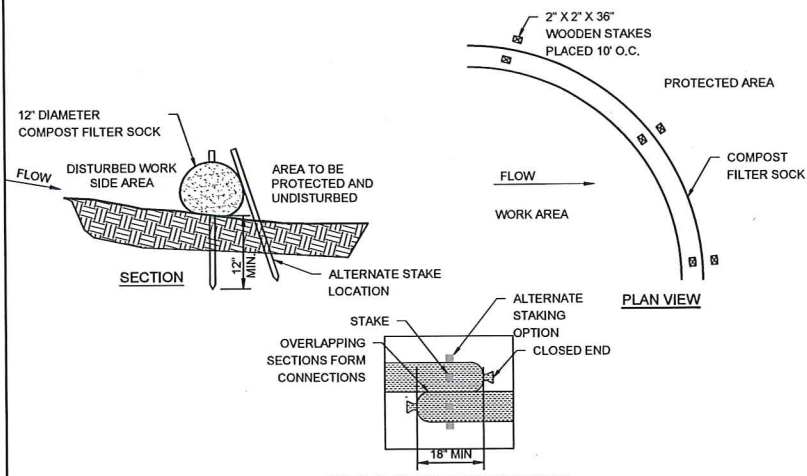
DMH  
 R=282.83'  
 I1=277.23' (12"RCP)  
 I2=278.53' (12"RCP)  
 I3=278.73' (12"RCP)  
 I4=278.93' (12"RCP)

SMH  
 R=282.94'  
 I1=274.84' (8" PVC)  
 I2=274.74' (8" PVC)

N/F IRVIN KELLEY J  
 29 HBL AVE  
 FRANKLIN, MA 02038  
 Book, Page: 31432, 460  
 Property ID: 285-066-000-000

N/F MARCHAND THOMAS H  
 29 LEWIS ST  
 FRANKLIN, MA 02038  
 Book, Page: 33510, 442  
 Property ID: 285-065-000-000  
 AREA = ±21,045 SF = 0.544AC





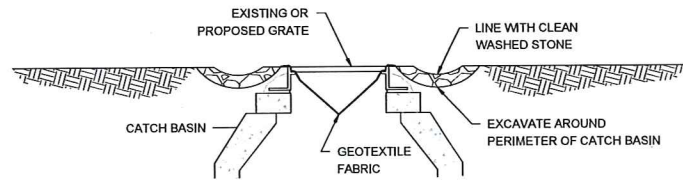
CONNECTION/ATTACHMENT DETAIL

NOTES:

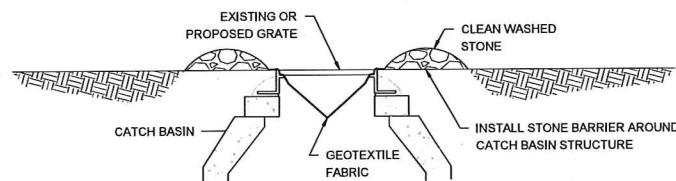
1. LINEAR SEDIMENTATION CONTROL SHALL BE PREFABRICATED COMPOST SOCK (FILTREXX SOXX) OR APPROVED EQUAL.
2. MATERIAL FOR SOCKS SHALL CONSIST OF SANITIZED MATURE COMPOST, FREE OF VIABLE WEED SEEDS AND FOREIGN DEBRIS SUCH AS GLASS AND PLASTIC. COMPOST SHALL BE IN SHREDDED OR GRANULAR FORM AND FREE FROM HARD LUMPS. IN ADDITION, NO KILN-DRIED WOOD OR CONSTRUCTION DEBRIS SHALL BE ALLOWED. CONTRACTOR SHALL REFER TO MASSDOT SPECIFICATIONS M1.06.0 FOR MATERIAL SPECIFICATIONS.
3. SOCK SHALL CONSIST OF JUTE MESH OR OTHER APPROVED BIODEGRADABLE MATERIAL.
4. COMPOST SOCKS SHALL BE INSTALLED PRIOR TO THE BEGINNING OF CONSTRUCTION AND REMAIN IN PLACE UNTIL ALL DISTURBED AREAS HAVE BEEN STABILIZED WITH SUFFICIENT VEGETATION TO PREVENT EROSION.
5. COMPOST SOCKS SHALL BE INSPECTED WEEKLY AT A MINIMUM AS WELL AS AFTER EACH STORM EVENT.
6. ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HEIGHT HAS REACHED A MAXIMUM OF 50% OF THE HEIGHT OF THE COMPOST SOCK.

LINEAR SEDIMENTATION CONTROL

NOT TO SCALE

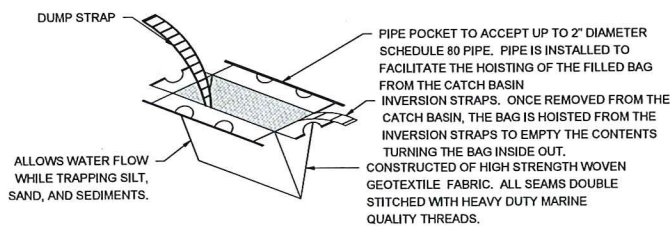


EXCAVATION



GRAVELSTONE BARRIER

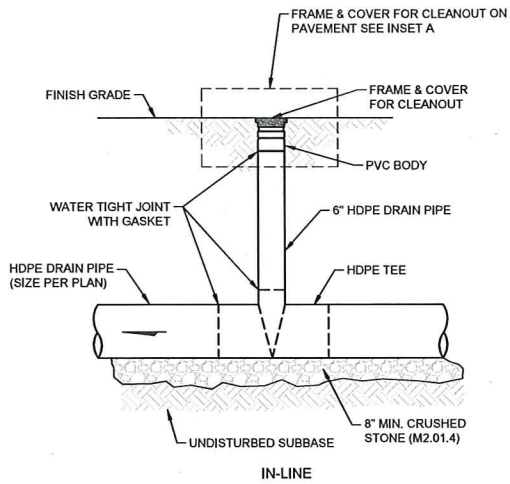
COMBINATION INLET PROTECTION FOR USE WHERE CONTRIBUTING AREAS ARE UN-STABILIZED



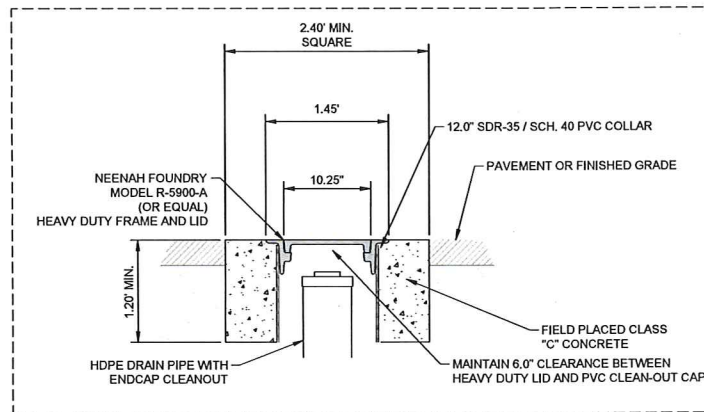
FOR USE WHERE CONTRIBUTING AREAS ARE PAVED

GEOTEXTILE CATCH BASIN INLET PROTECTION

NOT TO SCALE



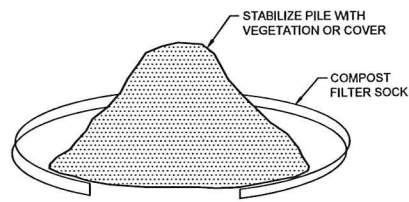
IN-LINE



INSET A

YARD CLEANOUT

NOT TO SCALE



NOTE: STOCKPILES MUST BE PHYSICALLY SEPARATED FROM OTHER STORMWATER CONTROLS.

NOTE: STOCKPILES MUST BE PHYSICALLY SEPARATED FROM OTHER STORMWATER CONTROLS.

SOIL STOCKPILES: FOR ANY STOCKPILED OR LAND CLEARING DEBRIS COMPOSED, IN WHOLE OR IN PART, OF SEDIMENT OR SOIL, THE FOLLOWING MEASURES MUST BE FOLLOWED:

- LOCATE THE PILES OUTSIDE OF ANY NATURAL BUFFERS AND PHYSICALLY SEPARATED FROM OTHER STORMWATER CONTROLS.
- PROTECT FROM CONTACT WITH STORMWATER (INCLUDING RUN-ON) USING A TEMPORARY PERIMETER SEDIMENT BARRIER.
- PROVIDE COVER OR APPROPRIATE TEMPORARY STABILIZATION TO AVOID DIRECT CONTACT WITH PRECIPITATION OR TO MINIMIZE SEDIMENT DISCHARGE.
- DO NOT HOSE DOWN OR SWEEP SOIL OR SEDIMENT ACCUMULATED ON PAVEMENT OR OTHER IMPERVIOUS SURFACES INTO ANY STORMWATER CONVEYANCE (UNLESS CONNECTED TO A SEDIMENT BASIN, SEDIMENT TRAP, OR SIMILARLY EFFECTIVE CONTROL), STORM DRAIN INLET, OR SURFACE WATER.

SOIL STOCKPILING CONTROL

NOT TO SCALE

CONSTRUCTION SEQUENCE RECOMMENDED:

1. INSTALLATION OF STABILIZED CONSTRUCTION ENTRANCE/EXIT (AS SHOWN).
2. INSTALLATION OF EROSION CONTROL BARRIER
3. INSTALLATION OF INLET PROTECTION IN STREET
4. EARTHWORK AND EXCAVATION/FILLING AS NECESSARY
5. CONSTRUCTION OF UTILITIES BEGINNING WITH STORMWATER MEASURES
6. CONSTRUCTION OF BUILDINGS
7. PLACE 6" TOPSOIL ON SLOPES AFTER FINAL GRADING COMPLETED. FERTILIZE, SEED, AND MULCH SEED MIXTURE TO BE INSTALLED AS REQUIRED.
8. REMOVE EROSION CONTROLS AS DISTURBED AREAS BECOME STABILIZED TO 70% STABILIZATION OR GREATER.

PRACTICE DEWATERING: THE CONTRACTOR IS PROHIBITED FROM DISCHARGING GROUNDWATER OR ACCUMULATED STORMWATER THAT IS REMOVED FROM EXCAVATIONS, TRENCHES, FOUNDATIONS, VAULTS, OR OTHER POINTS OF ACCUMULATION ASSOCIATED WITH A CONSTRUCTION ACTIVITY, UNLESS SUCH WATERS ARE FIRST TREATED BY AN APPROPRIATE CONTROL FOR SEDIMENT. APPROPRIATE CONTROLS INCLUDE, BUT ARE NOT LIMITED TO, SEDIMENT BASINS OR TRAPS, DEWATERING TANKS, WEIR TANKS, OR FILTRATION SYSTEMS (E.G., BAG OR SAND FILTERS) THAT ARE DESIGNED TO REMOVE SEDIMENT. UNCONTAMINATED, NON-TURBID DEWATERING WASTEWATER, SUCH AS WELL-POINT GROUND WATER, CAN BE DISCHARGED WITHOUT BEING ROUTED TO A CONTROL PROVIDED THE DEWATERING FLOW COMPLIES WITH THE VELOCITY DISSIPATION REQUIREMENTS OF PART 2.1.4.1 OF THE CONSTRUCTION GENERAL PERMIT.

\*\*\*NOTE: THE C.G.P. ALLOWS FOR THE DISCHARGE OF UNCONTAMINATED WATER ONLY.

THE FOLLOWING DISCHARGE REQUIREMENTS FOR DEWATERING ACTIVITIES MUST BE MET:

- I. DO NOT DISCHARGE FLOATING SOLIDS OR VISIBLE FOAM.
- II. USE AN OIL-WATER SEPARATOR OR SUITABLE FILTRATION DEVICE (SUCH AS A CARTRIDGE FILTER) THAT IS DESIGNED TO REMOVE OIL, GREASE, OR OTHER PRODUCTS IF DEWATERING WASTEWATER IS FOUND TO CONTAIN THESE MATERIALS.
- III. UTILIZE VEGETATED AREAS OF THE SITE TO INFILTRATE WASTEWATER FROM DEWATERING ACTIVITIES, UNLESS INFEASIBLE.
- IV. PROVIDE ENERGY DISSIPATION AT ALL POINTS WHERE DEWATERING WASTEWATER IS DISCHARGED. DEWATERING DISCHARGES MUST NOT CAUSE EROSION AT THE DISCHARGE POINT OR SCOURING OF THE BANKS OF THE WATER OF THE U.S.;
- V. WITH SEDIMENT THAT HAS BEEN REMOVED DURING THE MAINTENANCE OF A DEWATERING DEVICE, YOU MUST MANAGE SUCH SEDIMENT IN ACCORDANCE WITH AII, ABOVE.
- VI. WITH BACKWASH WATER, EITHER HULL AWAY FOR DISPOSAL OR RETURN IT TO THE BEGINNING OF THE TREATMENT PROCESS FOR ANOTHER PASS THROUGH THE SERIES OF DEWATERING DEVICES; AND
- VII. REPLACE AND CLEAN THE FILTER MEDIA USED IN DEWATERING DEVICES WHEN THE PRESSURE DIFFERENTIAL EQUALS OR EXCEEDS THE MANUFACTURER'S SPECIFICATIONS.

THE OWNER IS NOT APPLYING FOR A CONSTRUCTION DEWATERING PERMIT, AND NO DIRECT DISCHARGE OF WATER GENERATED FROM CONSTRUCTION DEWATERING ACTIVITIES TO THE CITY STORMWATER/SEWER SYSTEM WILL BE ALLOWED. DEWATERING EFFLUENT GENERATED BY THE CONSTRUCTION SHALL BE MANAGED ON SITE, INCLUDING TEMPORARY STORAGE/CONTAINERIZATION, IF NECESSARY, AND IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

EROSION PREVENTION AND SEDIMENT CONTROL NOTES

1. THE BEST MANAGEMENT PRACTICES PROVIDED HEREIN REPRESENT THE MINIMUM MEASURES TO BE EMPLOYED DURING EACH PHASE OF CONSTRUCTION. SPECIFIC SEQUENCING PLANS WILL BE DEVELOPED FOR EACH PHASE OF CONSTRUCTION AS DESIGN IS ADVANCED.
2. THE CONTRACTOR MUST PERFORM ACTIVITIES IN ACCORDANCE WITH THE REQUIREMENTS OF THE GENERAL PERMIT FOR DISCHARGES FROM CONSTRUCTION ACTIVITIES.
3. THESE EROSION AND SEDIMENTATION PLANS ARE PART OF THE STORMWATER POLLUTION PREVENTION PLAN.
4. NO LAND CLEARING, GRUBBING OR GRADING ACTIVITIES, EXCEPT THAT REQUIRED FOR INSTALLATION OF THE CONSTRUCTION EXIT, SHALL BEGIN UNTIL PERIMETER SEDIMENT CONTROL MEASURES AND STORMWATER MANAGEMENT PRACTICES ARE CONSTRUCTED AND STABILIZED.
5. TEMPORARY STABILIZATION PRACTICES MUST BE INITIATED AS SOON AS PRACTICABLE ON PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 14 DAYS AFTER CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.
6. SEDIMENT MUST BE REMOVED FROM TRAPS OR PONDS WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY 50%.
7. SEDIMENT MUST BE REMOVED FROM A SILT FENCE, COMPOST FILTER SOCK OR STRAW WATTLE WHEN IT REACHES 50% OF THE ABOVE-GROUND HEIGHT.
8. ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES WILL BE INSTALLED BY THE CONTRACTOR IF DEEMED NECESSARY BY ON-SITE INSPECTION.
9. IF ADDITIONAL EROSION OR SEDIMENT CONTROL MEASURES ARE NECESSARY, THEY MUST BE IMPLEMENTED BEFORE THE NEXT STORM EVENT WHENEVER PRACTICABLE. IF NOT PRACTICABLE BEFORE THE NEXT STORM EVENT, THEY MUST BE IMPLEMENTED AS SOON AS POSSIBLE.
10. OFF SITE SEDIMENT TRACKING MUST BE MINIMIZED. SEDIMENT TRACKED ONTO ADJACENT PUBLIC WAYS MUST BE SWEEP BY THE END OF THE SAME WORK DAY.
11. STORMWATER RUNOFF FROM EXPOSED SURFACES MUST BE DIVERTED, RETAINED/DETAINED TO MINIMIZE THE DISCHARGE OF POLLUTANTS.
12. VELOCITY DISSIPATION DEVICES MUST BE INSTALLED AT DISCHARGE LOCATIONS OF ANY OUTFALL TO PROVIDE NON-EROSIVE FLOW VELOCITY.
13. PREVENT LITTER, CONSTRUCTION DEBRIS AND CONSTRUCTION CHEMICALS THAT COULD BE EXPOSED TO STORMWATER FROM BECOMING A POLLUTANT SOURCE IN STORMWATER DISCHARGES.

118 TURNPIKE ROAD, SUITE 200  
SOUTHBOROUGH, MA 01772

CONTACT@MPDCONSULTANTS.COM



REVISIONS

| No. | DATE | DESCRIPTION |
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|     |      |             |

PROJECT

**NYLAH CROSSING LLC**

240 EAST CENTRAL STREET &  
9 LEVINS STREET  
FRANKLIN, MASSACHUSETTS 02038

PROPERTY OWNER:

NYLAH CROSSING LLC  
95 EAST MAIN STREET, SUITE 100  
WESTBOROUGH, MASSACHUSETTS 01581

SHEET TITLE

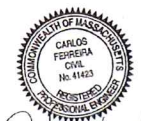
**EROSION CONTROL  
DETAILS**

DATE: 03/13/2024  
DRAWN BY: CMS  
CHECKED BY: JG  
FILE: NYLAHCROSS.dwg

DRAWING NO.

**C-106**





*Carlos Federspa*

| REVISIONS |      |             |
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| No.       | DATE | DESCRIPTION |
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|           |      |             |
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PROJECT  
**NYLAH CROSSING LLC**

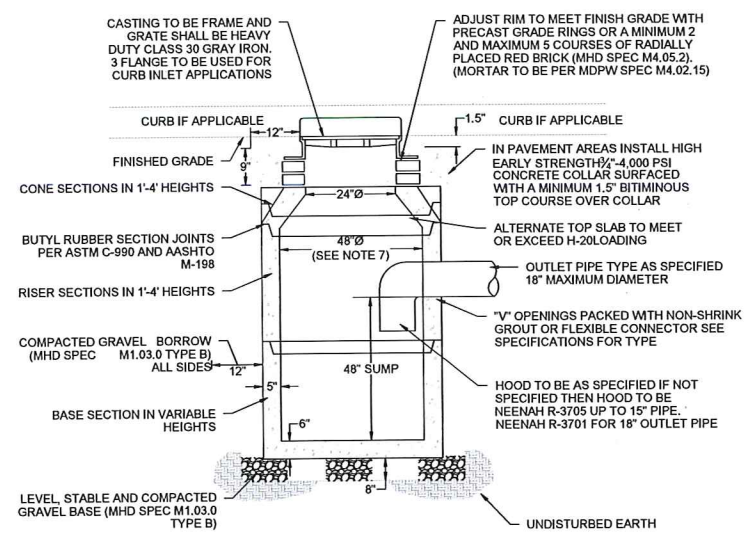
240 EAST CENTRAL STREET &  
9 LEVINS STREET  
FRANKLIN, MASSACHUSETTS 02038

PROPERTY OWNER:

NYLAH CROSSING LLC  
95 EAST MAIN STREET, SUITE 100  
WESTBOROUGH, MASSACHUSETTS 01581

SHEET TITLE  
**CONSTRUCTION  
DETAILS**

|             |                |             |              |
|-------------|----------------|-------------|--------------|
| DATE:       | 03/13/2024     | DRAWING NO. | <b>C-107</b> |
| DRAWN BY:   | CMS            |             |              |
| CHECKED BY: | JG             |             |              |
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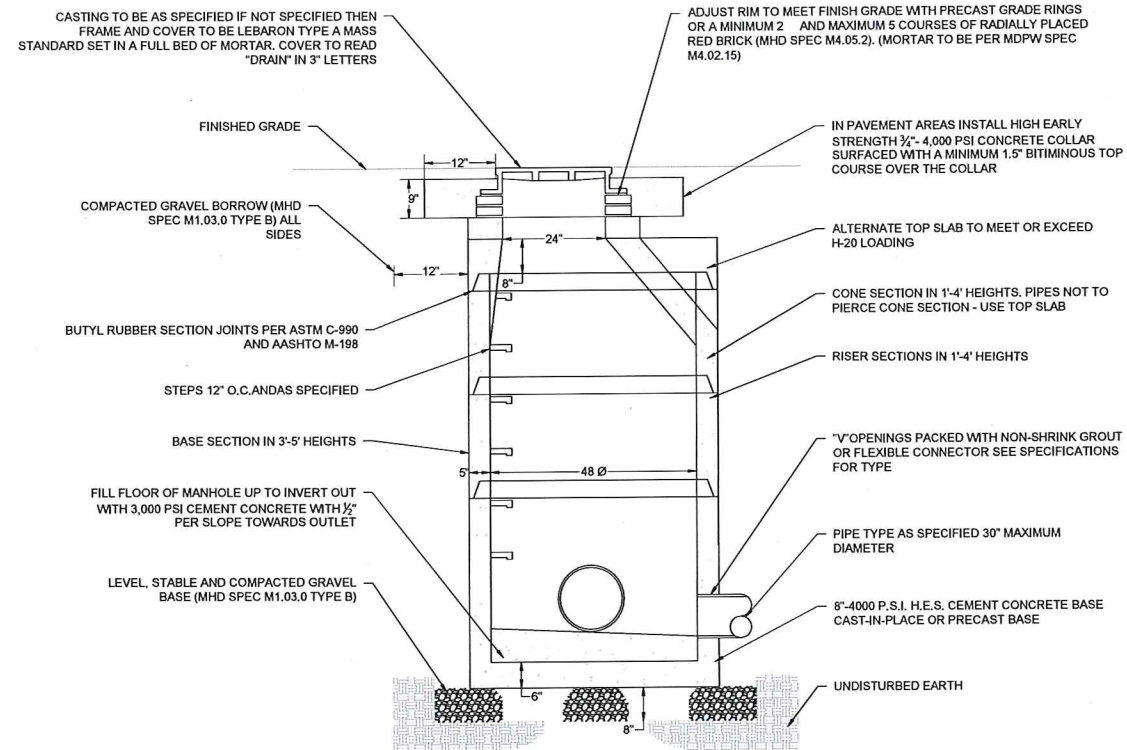


NOTES:

- EXCAVATION TO ALLOW FOR FREE TRAVEL OF COMPACTION EQUIPMENT.
- ALL COMPACTION TO A MINIMUM 95 PERCENT DRY DENSITY DETERMINED BY ASTM D1557 SEE SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
- ALL PRECAST TO MEET OR EXCEED ASTM C-478 AND AASHTO M 199 SPECIFICATIONS.
- REINFORCED STEEL TO MEET OR EXCEED ASTM A185 AND H-20 LOADING REQUIREMENTS.
- ALL PRECAST CONCRETE TO BE 4,000 P.S.I. MINIMUM AND MEET ASTM C-478 (6.1).
- FILL ALL INTERNAL AND EXTERNAL HOLES WITH NON-SHRINK GROUT.
- WHERE DOUBLE CATCH BASIN (I.D. 60") IS SPECIFIED ON PLAN, PROVIDE MINIMUM WALL DIMENSION OF 6", MINIMUM BASE DIMENSION OF 8" AND A 24"x48" DOUBLE FRAME AND GRATE.

**CATCH BASIN (DEEP SUMP)**

NOT TO SCALE



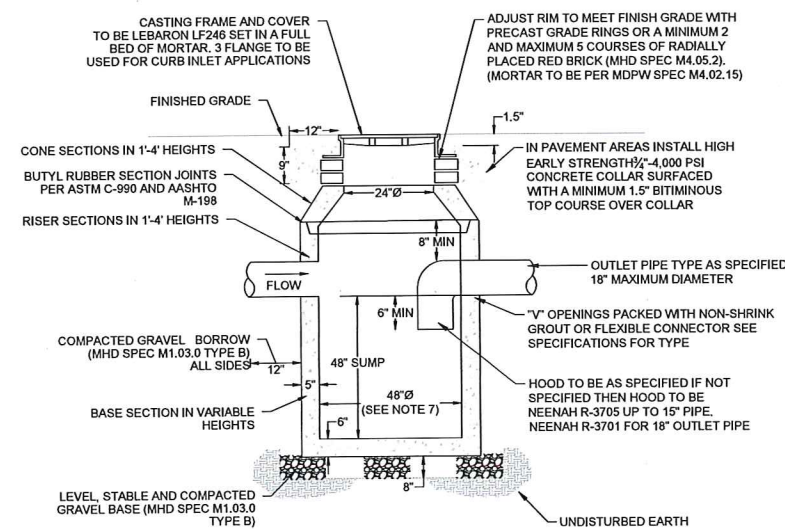
NOTES:

- MANHOLES SHALL BE 48" INSIDE DIAMETER (I.D.) UNLESS OTHERWISE INDICATED ON PLANS. FOR I.D. SIZES GREATER THAN 48" REFER TO TABLE BELOW FOR THICKNESS REQUIREMENTS.
- EXCAVATION TO ALLOW FOR FREE TRAVEL OF COMPACTION EQUIPMENT.
- ALL COMPACTION TO A MINIMUM 95 PERCENT DRY DENSITY DETERMINED BY ASTM D1557. SEE SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
- ALL PRECAST TO MEET OR EXCEED ASTM C-478 AND AASHTO M 199 SPECIFICATIONS.
- REINFORCED STEEL TO MEET OR EXCEED ASTM A185 AND H-20 LOADING REQUIREMENTS.
- ALL PRECAST CONCRETE TO BE 4,000 PSI MINIMUM AND MEET ASTM C-478 (6.1).
- IF NO STEPS ARE SPECIFIED THAN AS THE LOCAL MUNICIPALITY REQUIRES OR IF NO MUNICIPALITY REQUIREMENTS THEN COPOLYMER POLYPROPYLENE COATED REINFORCED PER ASTM C-478 AND OSHA (STD 1-1.9).
- CONTRACTOR TO CONFIRM WITH CITY OR TOWN DPW THAT BRICK INVERTS ARE NOT A REQUIREMENT.
- FILL ALL INTERNAL AND EXTERNAL HOLES WITH NON-SHRINK GROUT.

| I.D. | BASE THICKNESS (MIN.) | WALL THICKNESS (MIN.) |
|------|-----------------------|-----------------------|
| 5'   | 8"                    | 6"                    |
| 6'   | 8"                    | 7"                    |
| 8'   | 10"                   | 9"                    |
| 10'  | 10"                   | 10"                   |

**PRECAST CONCRETE DRAIN MANHOLE**

NOT TO SCALE

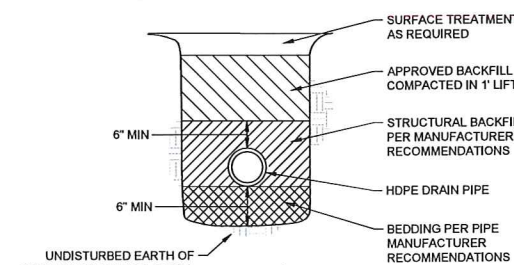


NOTES:

- EXCAVATION TO ALLOW FOR FREE TRAVEL OF COMPACTION EQUIPMENT.
- ALL COMPACTION TO A MINIMUM 95 PERCENT DRY DENSITY DETERMINED BY ASTM D1557 SEE SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
- ALL PRECAST TO MEET OR EXCEED ASTM C-478 AND AASHTO M 199 SPECIFICATIONS.
- REINFORCED STEEL TO MEET OR EXCEED ASTM A185 AND H-20 LOADING REQUIREMENTS.
- ALL PRECAST CONCRETE TO BE 4,000 P.S.I. MINIMUM AND MEET ASTM C-478 (6.1).
- FILL ALL INTERNAL AND EXTERNAL HOLES WITH NON-SHRINK GROUT.
- WHERE DOUBLE CATCH BASIN (I.D. 60") IS SPECIFIED ON PLAN, PROVIDE MINIMUM WALL DIMENSION OF 6", MINIMUM BASE DIMENSION OF 8" AND A 24"x48" DOUBLE FRAME AND GRATE.

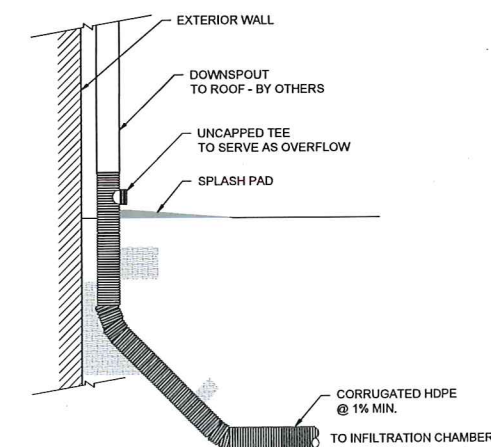
**PRE-TREATMENT DRAINAGE MANHOLE (DEEP SUMP)**

NOT TO SCALE



**DRAIN PIPE BEDDING**

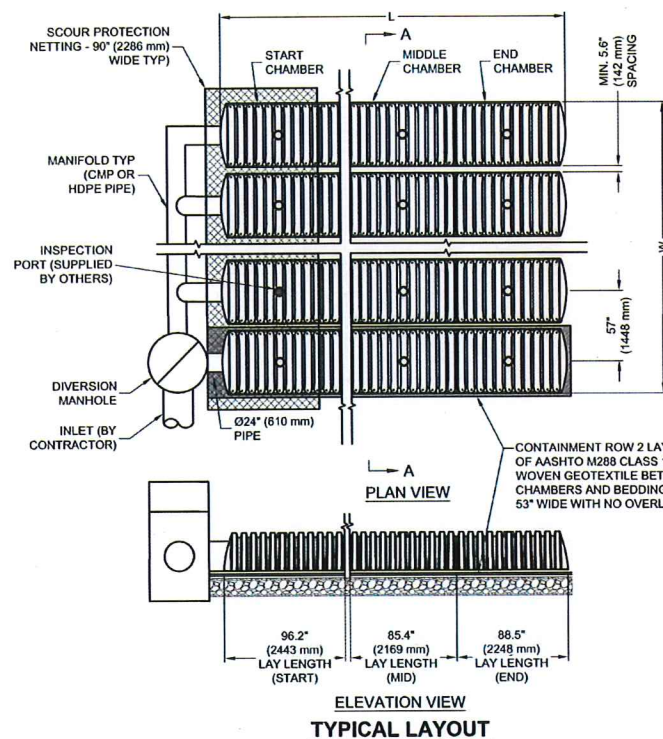
NOT TO SCALE



**DOWNSPOUT CONNECTION**

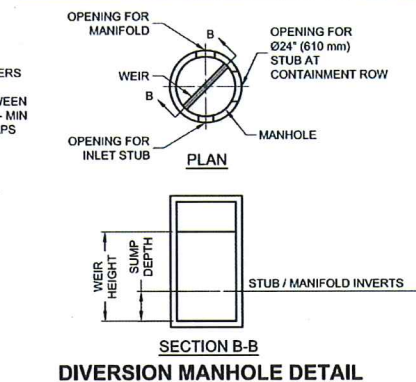
NOT TO SCALE





| CHAMBERMAXX DESIGN DETAILS                               |               |                |              |
|--|---------------|----------------|--------------|
| FEATURE  | START CHAMBER | MIDDLE CHAMBER | END CHAMBER  |
| OVERALL CHAMBER HEIGHT - IN (mm)                         | 30.3 (770)    | 30.3 (770)     | 30.3 (770)   |
| OVERALL CHAMBER WIDTH - IN (mm)                          | 51.4 (1306)   | 51.4 (1306)    | 51.4 (1306)  |
| ACTUAL LENGTH - IN (mm)                                  | 98.4 (2500)   | 91.0 (2311)    | 92.0 (2337)  |
| INSTALLED LAY LENGTHS - IN (mm)                          | 96.2 (2443)   | 85.4 (2169)    | 88.5 (2248)  |
| CHAMBER STORAGE VOLUME - CF (m³)                         | 50.2 (1.421)  | 47.2 (1.338)   | 46.2 (1.307) |
| CHAMBER STORAGE PER LINEAR FOOT - CF/LF (m³/m)           | 6.3 (0.582)   | 6.6 (0.616)    | 6.3 (0.582)  |
| *INSTALLED CHAMBER VOLUME - CF (m³)                      | 78.1 (2.211)  | 75.1 (2.127)   | 74.1 (2.098) |
| *INSTALLED CHAMBER VOLUME PER LINEAR FOOT - CF/LF (m³/m) | 9.7 (0.905)   | 10.6 (0.981)   | 10.0 (0.934) |
| CHAMBER WEIGHT - LB (kg)                                 | 83 (37.65)    | 73 (33.11)     | 76 (34.47)   |

| * SITE SPECIFIC DATA REQUIREMENTS   |  |
|---|--|
| FOR DETAILED DESIGN ASSISTANCE REFERENCE CHAMBERMAXX DYODS (DESIGN YOUR OWN DETENTION SYSTEM) SOFTWARE AND CHAMBERMAXX STAGE STORAGE CALCULATOR @ WWW.CONTECHSTORMWATER.COM |  |
| TOTAL REQUIRED STORAGE VOLUME (CF OR m³)  |  |
| DEPTH TO INVERT BELOW ASPHALT (FT OR m)   |  |
| LIMITING WIDTH (FT OR m)  |  |
| LIMITING LENGTH (FT OR m)   |  |
| POROUS STONE ABOVE CHAMBER (IN OR mm)   |  |
| POROUS STONE BELOW CHAMBER (IN OR mm)   |  |
| STONE POROSITY (0 TO 40%)   |  |
| MANIFOLD SYSTEM DIAMETER (IN OR mm)   |  |
| * PER ENGINEER OF RECORD  |  |



**GENERAL NOTES**

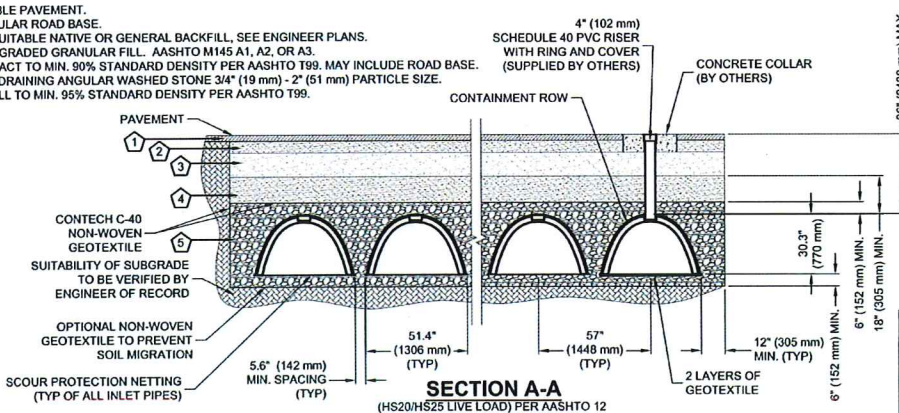
- ALL ELEVATIONS, DIMENSIONS AND LOCATIONS OF RISERS AND INLETS SHALL BE VERIFIED BY THE ENGINEER OF RECORD.
- PRIOR TO INSTALLATION OF THE CHAMBERMAXX SYSTEM A PRE-CONSTRUCTION MEETING SHALL BE CONDUCTED. THOSE REQUIRED TO ATTEND ARE THE SUPPLIER OF THE SYSTEM, THE GENERAL CONTRACTOR, SUB-CONTRACTORS AND THE ENGINEER.
- CHAMBERMAXX CHAMBERS ARE MANUFACTURED FROM POLYPROPYLENE PLASTIC.
- CHAMBERMAXX SYSTEM TO MEET AASHTO HS20HS25 LIVE LOADING, PER AASHTO LRFD SECTION 12.
- ACCESS COVERS TO MEET AASHTO HS20HS25 LIVE LOADING.
- MINIMUM COVER IS 18-INCHES (457 mm) AND MAXIMUM COVER IS 96-INCHES (2438 mm) TO BOTTOM OF FLEXIBLE PAVEMENT OR TO TOP OF RIGID PAVEMENT. FOR COVER HEIGHTS GREATER THAN 96-INCHES (2438 mm) CONTACT YOUR LOCAL REPRESENTATIVE.
- ALL PARTS PROVIDED BY CONTECH UNLESS OTHERWISE NOTED.
- FOR INFORMATION ON PRE-TREATMENT SYSTEMS, REFERENCE CONTECH PRE-TREATMENT SYSTEM STANDARD DETAILS OR CONTACT YOUR LOCAL REPRESENTATIVE.
- CHAMBERMAXX BY CONTECH ENGINEERED SOLUTIONS, LLC

**INSTALLATION NOTES**

- CHAMBERMAXX INSTALLATION GUIDE TO BE REVIEWED BY CONTRACTOR PRIOR TO INSTALLATION.
- PRIOR TO PLACING BEDDING, THE FOUNDATION MUST BE CONSTRUCTED TO A UNIFORM AND STABLE GRADE. IN THE EVENT THAT UNSUITABLE FOUNDATION MATERIALS ARE ENCOUNTERED DURING EXCAVATION, A GEOGRID SHALL BE UTILIZED OR UNSUITABLE MATERIAL SHALL BE REMOVED AND BROUGHT BACK TO GRADE WITH FILL MATERIAL AS APPROVED BY THE ENGINEER OF RECORD. ONCE THE FOUNDATION PREPARATION IS COMPLETE, THE BEDDING MATERIAL CAN BE PLACED.
- THE SCOUR PROTECTION NETTING TO EXTEND 1'-0" (305 mm) BEYOND OUTSIDE EDGE OF INLET CHAMBERS.
- COVER ANY OPEN VOID SPACES GREATER THAN 3/4" (19 mm) ON CHAMBERS WITH A NON-WOVEN GEOTEXTILE TO PREVENT INFILTRATION OF BACKFILL MATERIAL.
- STONE EMBEDMENT MATERIAL SHALL BE INSTALLED TO 95% STANDARD PROCTOR DENSITY AND PLACED IN 6-INCH (152 mm) TO 8-INCH (203 mm) LIFTS SUCH THAT THERE IS NO MORE THAN A TWO LIFT DIFFERENTIAL BETWEEN ANY OF THE CHAMBERS AT ANY TIME. GRANULAR BACKFILL MATERIAL SHALL BE COMPACTED TO 90% SPD. BACKFILLING SHALL BE ADVANCED ALONG THE LENGTH OF THE CHAMBER ROWS AT THE SAME RATE TO AVOID DIFFERENTIAL LOADING AND DISPLACEMENT OF THE CHAMBERS. THE MINIMUM CHAMBER SPACING MUST BE MAINTAINED.
- REFER TO CHAMBERMAXX INSTALLATION GUIDE FOR TEMPORARY CONSTRUCTION LOADING GUIDELINES.
- IT IS ALWAYS THE CONTRACTOR'S RESPONSIBILITY TO FOLLOW OSHA GUIDELINES FOR SAFE PRACTICES.
- GENERAL INSTALLATION METHODS AND MATERIALS TO BE IN ACCORDANCE WITH ASTM D2321.

**KEY**

- FLEXIBLE PAVEMENT.
- GRANULAR ROAD BASE.
- ANY SUITABLE NATIVE OR GENERAL BACKFILL, SEE ENGINEER PLANS.
- WELL GRADED GRANULAR FILL - AASHTO M145 A1, A2, OR A3. COMPACT TO MIN. 90% STANDARD DENSITY PER AASHTO T99. MAY INCLUDE ROAD BASE.
- FREE DRAINING ANGULAR WASHED STONE 3/4" (19 mm) - 2" (51 mm) PARTICLE SIZE. INSTALL TO MIN. 95% STANDARD DENSITY PER AASHTO T99.



**CONTECH**  
ENGINEERED SOLUTIONS LLC  
www.ContechES.com  
9025 Centre Pointe Dr., Suite 400, West Chester, OH 45069  
800-338-1122 513-645-7000 513-645-7993 FAX

**CHAMBERMAXX STORMWATER RETENTION STANDARD DETAIL CONTAINMENT ROW OPTION**

CHAMBERMaxx®  
PATENT PENDING



| REVISIONS |      |             |
|-----------|------|-------------|
| No.       | DATE | DESCRIPTION |
|           |      |             |
|           |      |             |
|           |      |             |

**PROJECT**  
NYLAH CROSSING LLC

240 EAST CENTRAL STREET &  
9 LEVMS STREET  
FRANKLIN, MASSACHUSETTS 02038

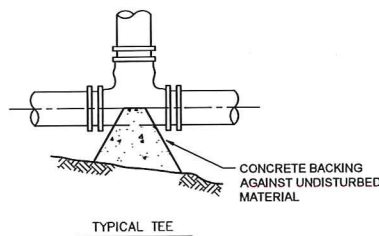
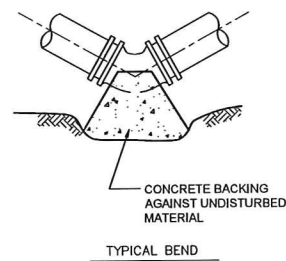
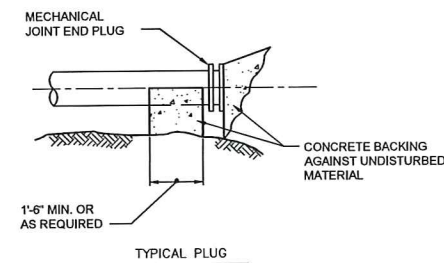
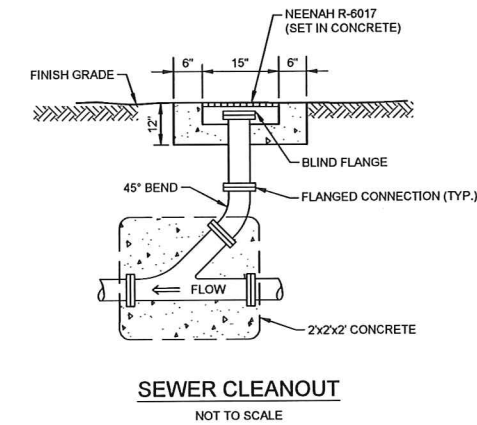
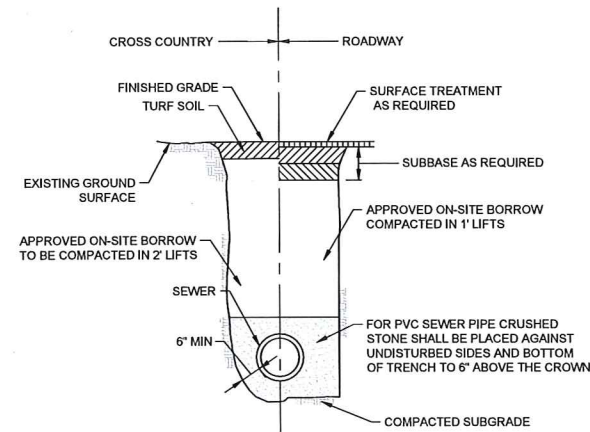
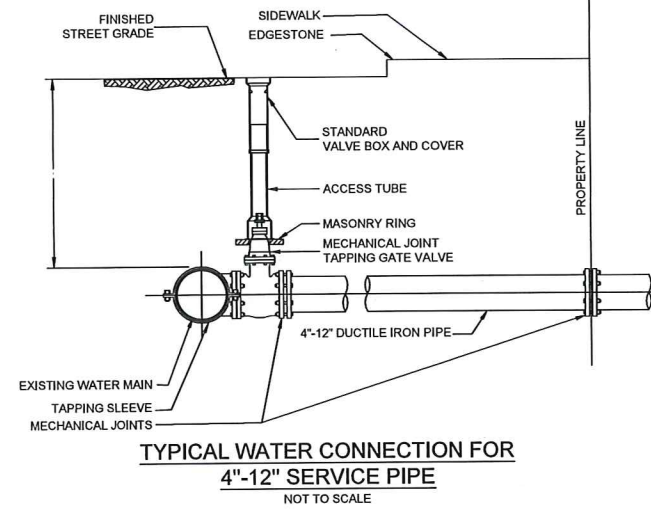
**PROPERTY OWNER:**  
NYLAH CROSSING LLC  
95 EAST MAIN STREET, SUITE 100  
WESTBOROUGH, MASSACHUSETTS 01581

**SHEET TITLE**  
CONSTRUCTION  
DETAILS

|             |                |                             |
|-------------|----------------|-----------------------------|
| DATE:       | 03/13/2024     | DRAWING NO.<br><b>C-108</b> |
| DRAWN BY:   | CMS            |                             |
| CHECKED BY: | JG             |                             |
| FILE:       | NYLAHCROSS.dwg |                             |

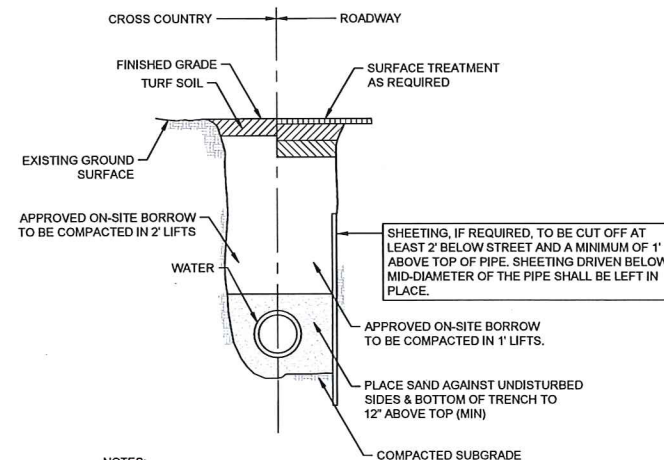


- NOTES:**
1. CONCRETE THRUST BLOCK TO BE USED ONLY WHERE IT WILL BEAR ON UNDISTURBED EARTH
  2. USE RESTRAINED JOINT FITTINGS OR TIE RODS WHERE CONCRETE THRUST BLOCK IS UNACCEPTABLE
  3. SIZE OF BLOCK OR MEGALUG TO BE DESIGNED FOR SPECIFIC CONDITIONS

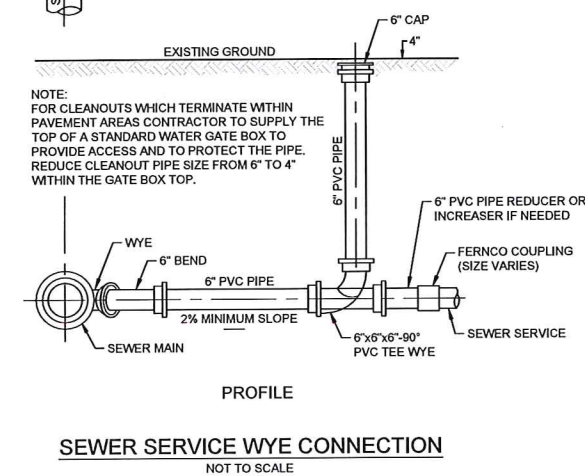
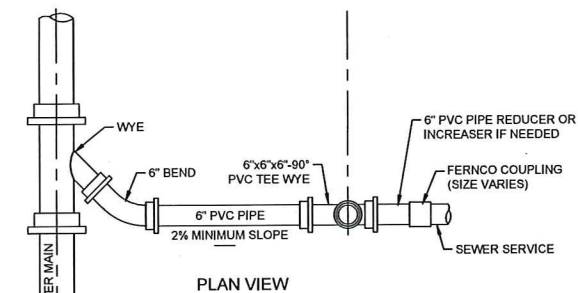


| SIZE OF MAIN (IN.) | 45° BEND | TEES & PLUGS | 22 1/2° BEND |
|--------------------|----------|--------------|--------------|
| 8" & LESS          | 4        | 5            | 2            |
| 10"                | 6        | 8            | 3            |
| 12"                | 9        | 12           | 5            |
| 16"                | 12       | 16           | 7            |

**CONCRETE THRUST BLOCKS**  
NOT TO SCALE



- NOTES:**
1. 5'-0" MIN COVER (TYP)
  2. CONNECTION AND INSTALLATION OF WATER SERVICE TO BE COORDINATED WITH TOWN OF BROCKTON WATER DEPARTMENT.



**SEWER SERVICE WYE CONNECTION**  
NOT TO SCALE



| No. | DATE | DESCRIPTION |
|-----|------|-------------|
|     |      |             |
|     |      |             |
|     |      |             |

**PROJECT**  
**NYLAH CROSSING LLC**

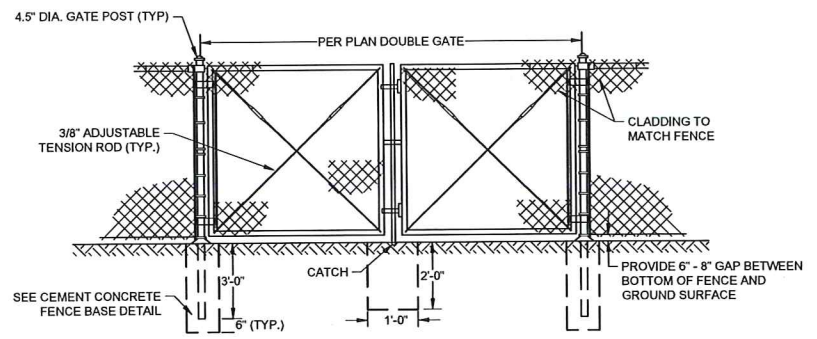
240 EAST CENTRAL STREET &  
9 LEVAS STREET  
FRANKLIN, MASSACHUSETTS 02038

**PROPERTY OWNER:**  
NYLAH CROSSING LLC  
95 EAST MAIN STREET, SUITE 100  
WESTBOROUGH, MASSACHUSETTS 01581

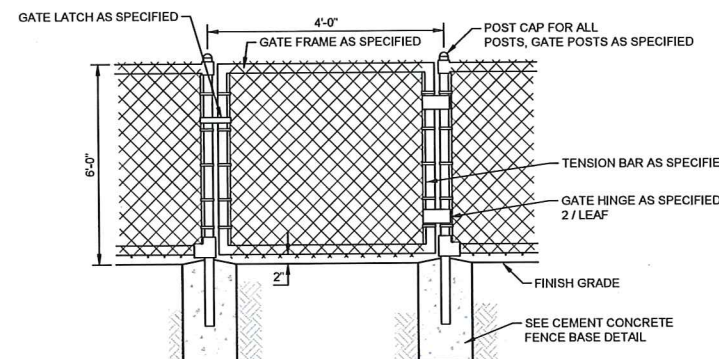
**SHEET TITLE**  
**CONSTRUCTION DETAILS**

|             |                |             |              |
|-------------|----------------|-------------|--------------|
| DATE:       | 03/13/2024     | DRAWING NO. | <b>C-109</b> |
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| CHECKED BY: | JG             |             |              |
| FILE:       | NYLAHCROSS.dwg |             |              |

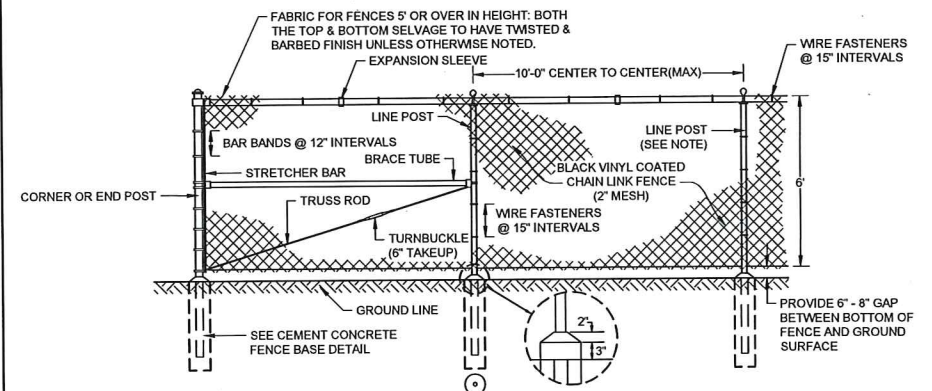




**CHAIN LINK GATE**  
NOT TO SCALE



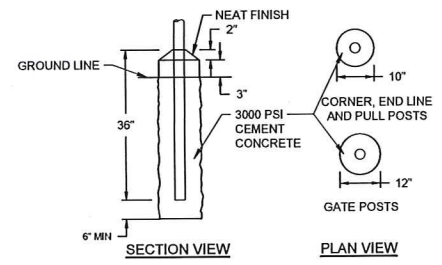
**CHAIN LINK FENCE GATE**  
NOT TO SCALE



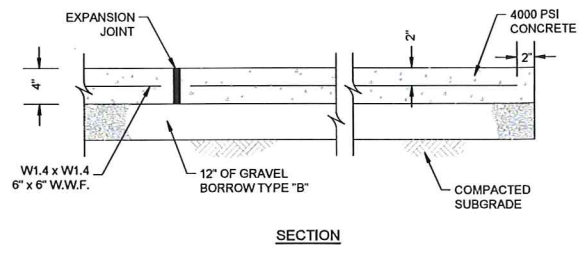
- NOTES:**
- THIS DETAIL IS FOR REFERENCE ONLY. CONTRACTOR SHALL COORDINATE WITH MANUFACTURER FOR DESIGN OF GATES AND FENCE.
  - CHAIN LINK FENCE, POSTS, RAILS, FABRIC, GATES, AND ACCESSORIES SHALL BE BLACK VINYL COATED.
  - GRADE OF FENCE SHALL BE PARALLEL WITH THE FINISHED GROUND SURFACE.
  - LINE POSTS TO BE SPACED 10'-0" CENTER TO CENTER MAXIMUM EXCEPT ON CURVES WHERE THEY SHALL BE SPACED AS FOLLOWS:  

| RADIUS OF CURVE | MAXIMUM SPACING        |
|-----------------|------------------------|
| 100' OR LESS    | 5'-0" CENTER TO CENTER |
| 100' TO 200'    | 6'-0" CENTER TO CENTER |
| 200' TO 500'    | 8'-0" CENTER TO CENTER |
  - END, CORNER AND INTERMEDIATE BRACE POSTS SHALL BE SET IN CONCRETE BASES. INTERMEDIATE BRACE POSTS SHALL BE SPACED AT 300-FOOT MAXIMUM INTERVALS. CHANGES IN LINE OF 30 DEGREES OR MORE SHALL BE CONSIDERED AS CORNERS.
  - ALL LINE POSTS, EXCEPT THOSE WHICH ARE UNSTABLE DUE TO SOIL CONDITIONS SHALL HAVE DRIVE ANCHOR ASSEMBLIES. LINE POSTS IN UNSTABLE SOIL CONDITIONS SHALL BE PLACED IN A CONCRETE BASE.

**CHAIN LINK FENCE**  
NOT TO SCALE

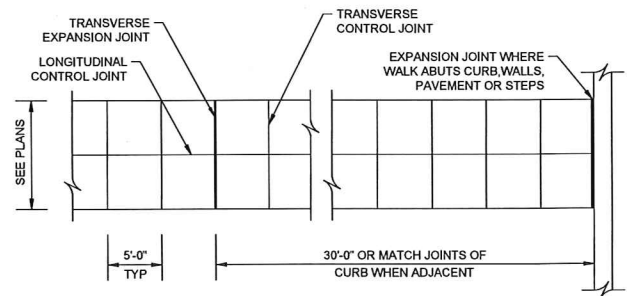


**CEMENT CONCRETE FENCE BASE**  
NOT TO SCALE



**SECTION**

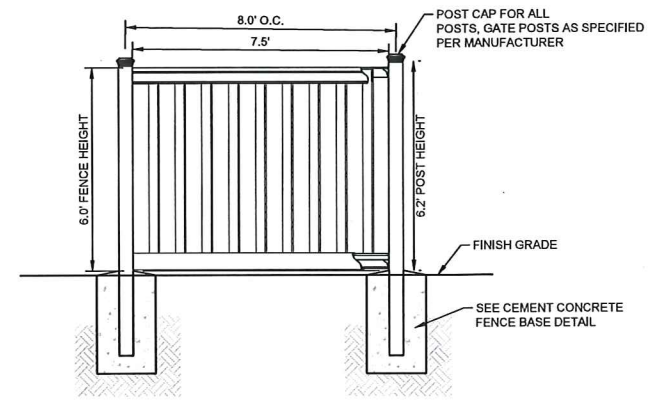
- NOTES:**
- SUBGRADE PREPARATION MUST BE IN ACCORDANCE WITH GEOTECHNICAL ENGINEER'S RECOMMENDATIONS



**PLAN**

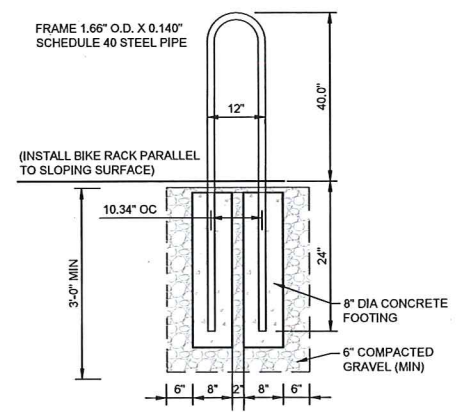
| WALK WIDTH   | LONGITUDINAL CONTROL JOINT     |
|--------------|--------------------------------|
| 0 - 7 FEET   | NONE                           |
| 8 - 10 FEET  | ONE ALONG WALK AT CENTERLINE   |
| 11 - 15 FEET | TWO ALONG WALK AT THIRD POINTS |

**CEMENT CONCRETE WALKWAY**  
NOT TO SCALE

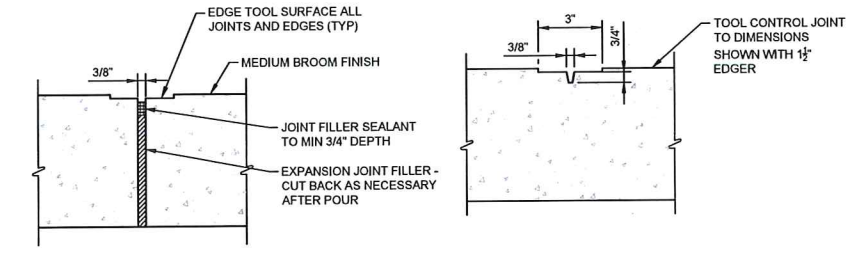


- NOTES:**
- THIS DETAIL IS FOR REFERENCE ONLY. CONTRACTOR SHALL COORDINATE WITH MANUFACTURER FOR DESIGN OF GATES AND FENCE.

**PVC FENCE**  
NOT TO SCALE

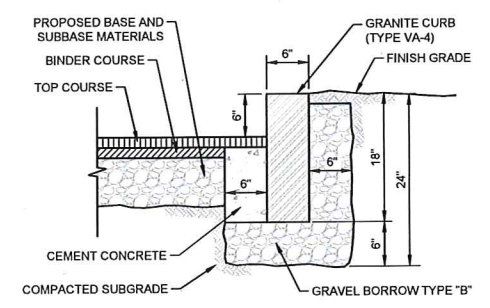


**BIKE RACK**  
NOT TO SCALE

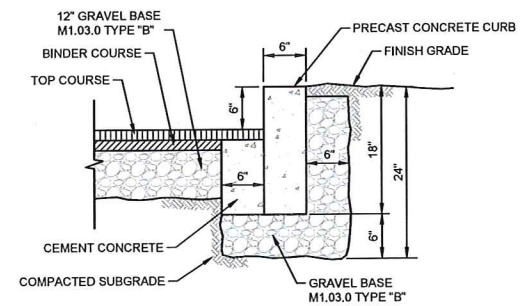


**CONCRETE WALK EXPANSION JOINT**  
NOT TO SCALE

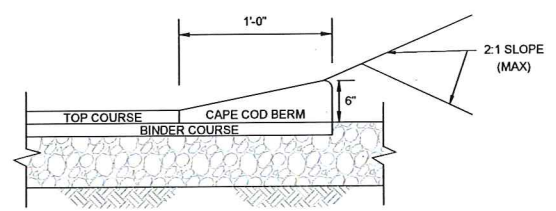
**CONCRETE WALK CONTROL JOINT**  
NOT TO SCALE



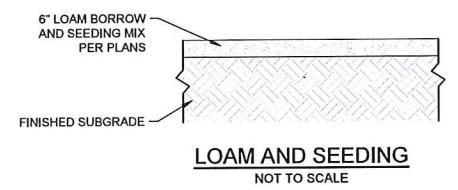
**TYPE VA-4 GRANITE CURB (VGC)**  
NOT TO SCALE



**PRECAST CONCRETE CURB (CC)**  
NOT TO SCALE



**CAPE COD BERM (CCB)**  
NOT TO SCALE



**LOAM AND SEEDING**  
NOT TO SCALE



**REVISIONS**

| No. | DATE | DESCRIPTION |
|-----|------|-------------|
|     |      |             |
|     |      |             |
|     |      |             |

**PROJECT**  
NYLAH CROSSING LLC

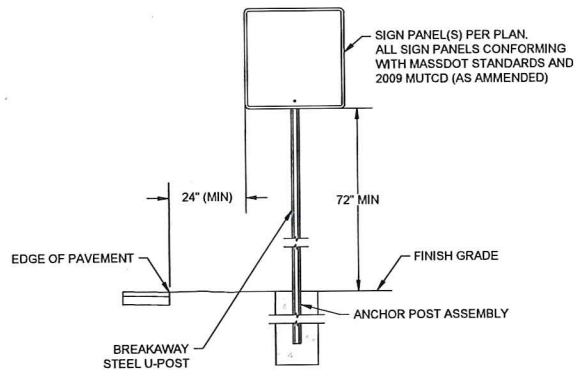
240 EAST CENTRAL STREET &  
9 LEWIS STREET  
FRANKLIN, MASSACHUSETTS 02038

**PROPERTY OWNER:**  
NYLAH CROSSING LLC  
95 EAST MAIN STREET, SUITE 100  
WESTBOROUGH, MASSACHUSETTS 01581

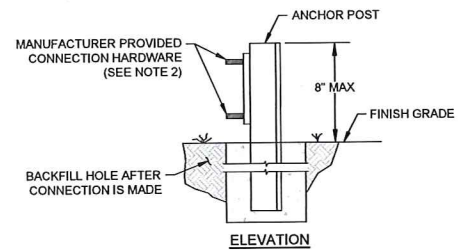
**SHEET TITLE**  
CONSTRUCTION  
DETAILS

|             |                |             |       |
|-------------|----------------|-------------|-------|
| DATE:       | 03/13/2024     | DRAWING NO. | C-110 |
| DRAWN BY:   | CMS            |             |       |
| CHECKED BY: | JG             |             |       |
| FILE:       | NYLAHCROSS.dwg |             |       |

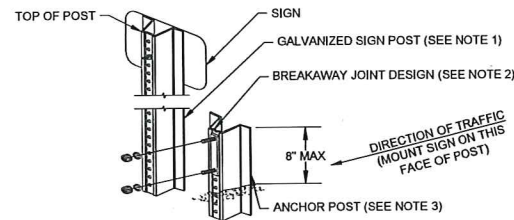




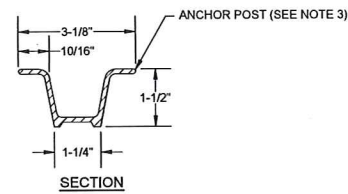
**ROADSIDE SIGN**



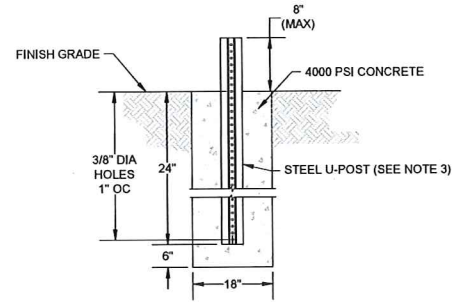
**ELEVATION**



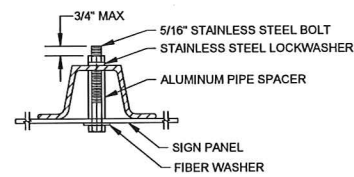
**SIGN POST - ISOMETRIC**



**SECTION**



**ANCHOR POST ASSEMBLY**

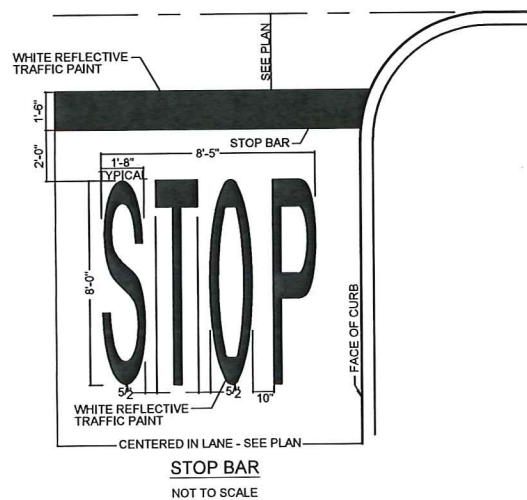


**SIGN PANEL MOUNTING**

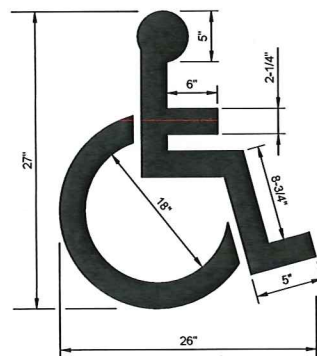
**NOTES:**

- SIGN POSTS SHALL BE U-SHAPED, HOT-DIP GALVANIZED STEEL WEIGHING AT LEAST 2.0 LBS. POSTS SHALL HAVE PRE-PUNCHED HOLES THAT ALIGN WITH HOLES IN SIGN PANELS. ALL POSTS SHALL HAVE ADEQUATE LENGTH TO MEET THE REQUIRED SIGN PANEL MOUNTING HEIGHTS. NO SPLICES WILL BE ALLOWED IN SIGN POSTS.
- ALL SIGN POSTS SHALL INCORPORATE A BREAKAWAY DESIGN. ALL FASTENERS, SPACERS, PLATES, OR FITTINGS REQUIRED FOR THE BREAKAWAY JOINT SHALL BE FURNISHED BY THE SIGN POST MANUFACTURER.
- ANCHOR POSTS SHALL BE U-SHAPED, HOT-DIP GALVANIZED STEEL WEIGHING AT LEAST 2.0 LBS. ANCHOR POSTS SHALL HAVE PRE-PUNCHED HOLES THAT ALIGN WITH HOLES IN THE SIGN POST ABOVE. SOIL ANCHOR POST SHALL BE SECURED TO POST AS RECOMMENDED BY MANUFACTURER.

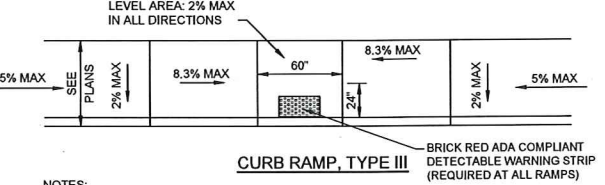
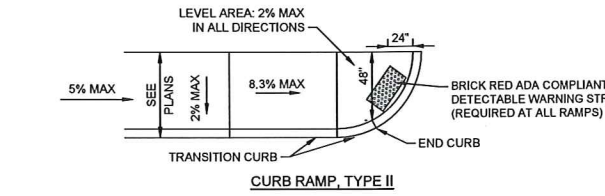
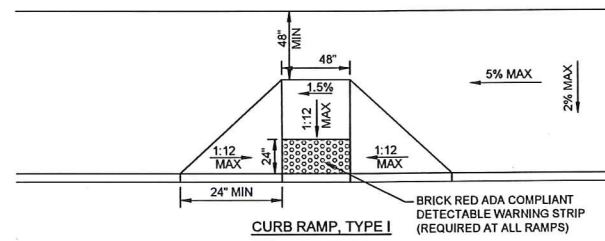
**SIGN POST ASSEMBLY**  
NOT TO SCALE



**STOP BAR**  
NOT TO SCALE



**ACCESSIBLE PARKING PAVEMENT MARKING**  
NOT TO SCALE



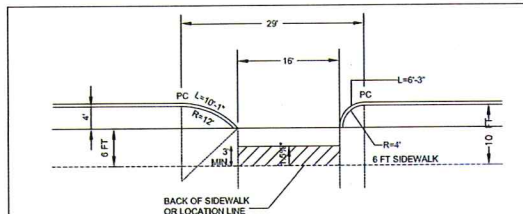
**NOTES:**

- THE DETECTABLE WARNING SHALL BE A CAST-IN-PLACE COMPOSITE PANEL PAVER SYSTEM AS MANUFACTURED BY ADA SOLUTIONS, INC., OR APPROVED EQUAL.
- TRANSITIONS FROM CURB CUTS TO WALKS, GUTTERS OR STREETS MUST BE FLUSH.

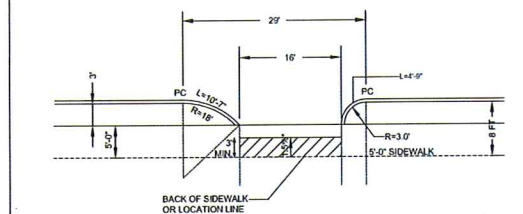
**ACCESSIBLE CURB RAMPS**  
NOT TO SCALE

- THE MAXIMUM ALLOWABLE ROUTE (SIDEWALK) AND CURB RAMP CROSS SLOPES SHALL BE 2.0% MAX.
- THE MAXIMUM ALLOWABLE SLOPE OF ACCESSIBLE ROUTE CURB RAMPS SHALL NOT EXCEED 1:12
- A MINIMUM OF 3 FEET CLEAR SHALL BE MAINTAINED AT ANY PERMANENT OBSTACLE IN ACCESSIBLE ROUTE (I.E., HYDRANTS, UTILITY POLES, TREE WELLS, SIGNS, ETC.,
- BASE OF RAMP SHALL BE GRADED TO PREVENT PONDING.
- SEE TYPICAL SIDEWALK SECTION FOR RAMP CONSTRUCTION.
- ALL SLOPING SURFACES AT WHEEL CHAIR RAMPS FOR SIDEWALKS SHALL HAVE DETECTABLE WARNING STRIPS AND COLOR CONCRETE
- DETECTABLE WARNINGS CONTRAST VISUALLY WITH ADJOINING SURFACES, EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT. THE MATERIAL USED TO PROVIDE CONTRAST SHOULD CONTRAST BY AT LEAST 70%. CONTRAST IN PERCENT IS DETERMINED BY:  $CONTRAST = [(B1 - B2)/B1] \times 100$  WHERE B1 = LIGHT REFLECTANCE VALUE (LRV) OF THE LIGHTER AREA AND B2 = LIGHT REFLECTANCE VALUE (LRV) OF THE DARKER AREA. B1 SHALL NEVER EQUAL 100 AND B2 SHALL ALWAYS BE GREATER THAN 0.
- ALL PROPOSED WHEELCHAIR RAMPS SHALL HAVE A PEDESTRIAN WARNING SURFACE MEETING THE REQUIREMENTS OF THE ADA GUIDELINES. ALL WHEELCHAIR RAMPS SHALL BE CONSTRUCTED WITH COLOR TINTED CONCRETE MEETING THE ADA GUIDELINES FOR CONTRASTING COLOR. FINAL COLOR TO BE DETERMINED BY THE ARCHITECT. ALL CONCRETE SHALL BE 4000 PSI PER ASTM C-260 AND THE PROJECT SPECIFICATIONS.
- AT NO TIME IS ANY PART OF THE WHEELCHAIR RAMP TO BE LOCATED OUTSIDE OF THE CROSSWALK AND IT IS TO BE CENTERED WHENEVER POSSIBLE, WHERE APPLICABLE.
- THE ENTRANCE OF THE WHEELCHAIR RAMP SHALL BE WITHIN 1/2' WITH THE PAVEMENT.

**ACCESSIBLE CURB RAMP / ROUTE NOTES**  
NOT TO SCALE



**10 FT SIDEWALK LAYOUT**



**8 FT SIDEWALK LAYOUT**

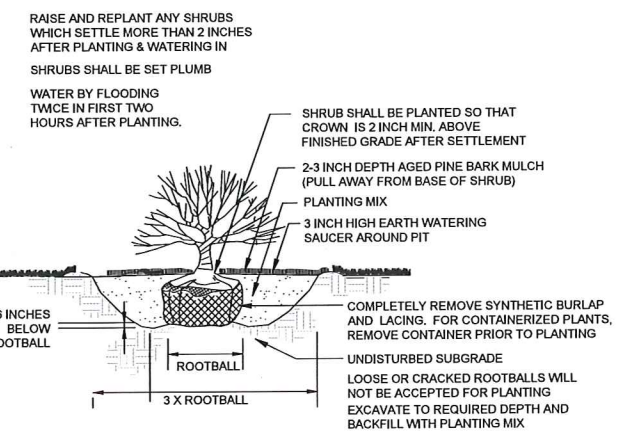
**NOTES:**

- WHEN THE SIDEWALK IS PAVED TO THE CURB LINE, USE SHORT CURB RETURNS AT THE HIGHWAY CURB LINE P.C.S. SHOWN IN THESE DESIGNS.

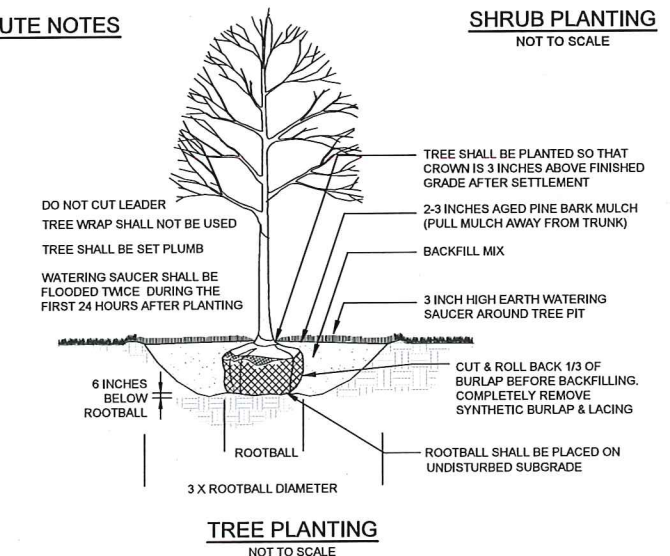
MAINTAIN PATH OF TRAVEL WITH 1.5% CROSS SLOPE (+0.5% CONSTRUCTION TOLERANCE)

**RESIDENTIAL DRIVEWAYS**

DATE OF ISSUE: OCTOBER 2017  
DRAWING NUMBER: E 107.8.1



**SHRUB PLANTING**  
NOT TO SCALE



**TREE PLANTING**  
NOT TO SCALE



| REVISIONS |      |             |
|-----------|------|-------------|
| No.       | DATE | DESCRIPTION |
|           |      |             |
|           |      |             |
|           |      |             |

**PROJECT**  
NYLAH CROSSING LLC

240 EAST CENTRAL STREET &  
9 LEVAS STREET  
FRANKLIN, MASSACHUSETTS 02038

**PROPERTY OWNER:**  
NYLAH CROSSING LLC  
95 EAST MAIN STREET, SUITE 100  
WESTBOROUGH, MASSACHUSETTS 01581

**SHEET TITLE**  
CONSTRUCTION  
DETAILS

|             |                |             |       |
|-------------|----------------|-------------|-------|
| DATE:       | 03/13/2023     | DRAWING NO. | C-111 |
| DRAWN BY:   | CMS            |             |       |
| CHECKED BY: | JG             |             |       |
| FILE:       | NYLAHCROSS.dwg |             |       |