

Town of Franklin



Planning Board

Due to the growing concerns regarding the COVID-19 virus, we will be conducting a remote/virtual Planning Board Meeting. In an effort to ensure citizen engagement and comply with open meeting law regulations, citizens will be able to dial into the meeting using the provided phone number (Cell phone or Landline Required) OR citizens can participate by copying the link (Phone, Computer, or Tablet required).

Please click on the link <https://us02web.zoom.us/j/87378878812> or call on your phone at 312-626-6799, meeting # 87378878812.

September 28, 2020

- 7:00 PM Commencement/General Business
- 7:05 PM **PUBLIC HEARING** – *Continued*
162 Grove Street *Adv.: June 15 & June 22, 2020*
Special Permit & Site Plan *Abuts: June 15, 2020*
- 7:05 PM **PUBLIC HEARING** - *TO BE CONTINUED*
70, 72 & 94 East Central St – Multi-Family *Adv.: Jan 27 & Feb 3, 2020*
Special Permit & Site Plan Modification *Abuts: Jan. 22, 2020*
- 7:10 PM **PUBLIC HEARING** - *Initial*
Scenic Road Permit *Adv.: Sept. 14 & Sept. 21, 2020*
274 Prospect Street *Abuts: September 14, 2020*
- 7:10 PM **PUBLIC HEARING** – *Continued*
164 Grove Street *Adv.: July 13 & July 20, 2020*
Special Permit & Site Plan *Abuts: July 13, 2020*
- 7:10 PM **PUBLIC HEARING** – *TO BE CONTINUED*
Maple Hill *Adv.: Feb. 24 & March 2, 2020*
Definitive Subdivision *Abuts: February 24, 2020*
- 7:15 PM **PUBLIC HEARING** – *Continued*
340 East Central Street *Adv.: June 8 & June 15, 2020*
Special Permit & Site Plan *Abuts: June 5, 2020*

GENERAL BUSINESS:

- A. Partial Form H: Lakeview Terrace Subdivision
- B. Final Form H: 31 Hayward St - Parking

Tel: (508) 520-4907

Fax: (508) 520 4906

This agenda is subject to change. Last updated: September 22, 2020
The next meeting of the Planning Board is scheduled for October 5, 2020.

FORM H - Partial
CERTIFICATE OF PARTIAL RELEASE

WHEREAS, on September 21, 2020, the Town of Franklin, a Massachusetts municipal corporation, acting through its Planning Board, with an address of Municipal Building, 355 E. Central Street, Franklin, Massachusetts 02038 [hereinafter "board"] received a request for a Certificate of Partial Release concerning the construction of ways and installation of municipal services in a subdivision owned by Prudential Builders, Inc., with an address of 344 John L. Dietsch Blvd., N. Attleboro, MA 02760

[hereinafter "owner"];

WHEREAS, on August 10, 2009, based on the owner's application dated _____, and after duly noticed public hearing(s), the board approved a definitive subdivision plan showing Two (2) lots, which is entitled: Definitive Subdivision Plan "Lakeview Terrace" Franklin, Massachusetts by: GLM Engineering Consultants, Inc. and recorded or registered at the Norfolk County Registry of Deeds as Plan # 96 of 2010;

WHEREAS, the approved definitive subdivision plan shows the division of a parcel of land located at 7-11 Brandywine Rd (Assessor's Map 216, Pcl 97, 98 & 103 [hereinafter "subdivision"] and further described in a deed or deeds dated April 26, 2019 and recorded at the Norfolk Registry of Deeds in Book(s) 36953, Page(s) 34 _____; or is registered in Norfolk County Land Registry as Document No. _____, and noted on certificate of title no. _____, in Registration Book _____, Page _____;

NOW THEREFOR, the board has determined that the construction of ways and installation of municipal services have been partially completed to the satisfaction of the board, as set forth in a vote attached hereto; and that:

_____ the following enumerated lots are hereby released from the covenant dated: _____

_____ the performance guarantee, which secures the construction of ways and installation of municipal services, as provided in the _____ agreement dated _____, may be reduced in the amount of _____, and the Treasurer of the town of Franklin is hereby directed to release such sum as provided in the agreement.

Duly executed as a sealed instrument this _____ day of _____, by a majority of the members of the Planning Board of the Town of Franklin.

PLANNING BOARD OF THE TOWN OF FRANKLIN

Planning Board member

Planning Board member

Planning Board member

Planning Board member

Planning Board member

COMMONWEALTH OF MASSACHUSETTS

_____, SS.

_____, 20__

On this _____ day of _____ 20__, before me, the undersigned notary public, personally appeared _____ (*name of document signer*), proved to me through satisfactory evidence of identification, which were _____ to be the person whose name is signed on the preceding document in my presence.

(Official signature and seal of notary)

Notary Public:

My Commission Expires: _____

cc: Town Clerk, Town of Franklin
Treasurer, Town of Franklin
Building Department

Subdivision Work Completion List

(To accompany Form H- Engineer's and Owner's Certificate of Partial Completion)

Subdivision Name: "Lakeview Terrace"
Owner Name: Lakeview Terrace, LLC
Owner's Engineer: Andrews Survey & Engineering, Inc
Date of Partial Certificate of Completion: _____

Outstanding Items:

Required Date of Completion:

1. Grading, Loam & Seeding of Slope Retaining Wall to Right EOP and debris removal
2. Swale along left side 0+0 to 2+50± - grading, loam & seed.
- 3) Swale on left side of cul-de-sac - Grading Loam & Seed
- 4) Installation of Right of Way Bounds as shown on the approved plans.

See letter dated September 21, 2020 from Andrews Survey and Engineering, Inc

Approved by: _____, Town Engineer Date: _____
Signed by: Robert McManis, Engineer Date: 9/21/2020
Signed by: _____, Owner Date: _____

* A Notarized Form H- Engineer's and Owner's Certificate of Partial Completion Part 1 and 2 must accompany this form prior to acceptance by the Planning Board.

Andrews Survey & Engineering, Inc.

Land Surveying • Civil Engineering • Site Planning

September 21, 2020

Planning Board
Town Hall
355 East Central Street
Franklin, MA 02038

**Re: *Lakeview Terrace
Definitive Subdivision, Brandywine Road Franklin, MA
ASE Project # 2018-259***

Dear Planning Board Members:

At the request of the developer of the referenced subdivision, ASE has prepared the attached Franklin Planning Board Form H Parts 1 and 2. ASE has also prepared the attached as built plan of the roadway and is a plan that shows the location of the items listed below.

The following work needs to be completed or revised at the referenced site.

1. There is an erosion problem along slope above the retaining wall. There are several areas where erosion has occurred with piles of sediment at the base of the retaining and on the abutting property. This erosion is occurring in the area where rocks were dumped to mitigate the erosion problem. This problem was created by the covering of the catch basin in the swale. This was done to protect the drainage infiltration system across the street adjacent to the swale and catch basin.

Due to this condition runoff ponded in this low area until it flowed across the street down the slope and over the retaining wall onto the abutting property, thus resulting in soil erosion accumulation at the base of the wall and on the adjacent property.

To eliminate this problem, grading, fill material, loam & seed are required along the right side of pavement to the retaining wall. There are many areas where this erosion has occurred above the retaining wall. Also, the debris at the base of the wall should be removed.

2. The swale along the left side of the roadway from the beginning of the street to the new swale at station 2+50 needs to be regraded, loamed and seeded. A great deal of soil material has filled in the swale near the catch basin at station 1+20 +/-.
3. The swale around the left side of the cul de sac is filled with sediment and needs to be regraded, loamed and seeded.
4. The roadway bounds have not been installed as shown on the approved plans.

Should you require additional information, please call our Uxbridge office.

Very truly yours,

ANDREWS SURVEY & ENGINEERING, INC.



Richard M. Mainville, P.E.
Senior Project Engineer

104 Mendon Street, P.O. Box 312
Uxbridge, MA 01569
Phone (508) 278-3897
Fax (508) 278-2289

500 East Washington Street
North Attleboro, MA 02760
Phone (508) 316-0452
Fax (508) 316-0963



Andrews Survey & Engineering, Inc.
Land Surveying - Civil Engineering - Site Planning

P.O. Box 312, 104 Mendon Street
Uxbridge, Massachusetts 01569
P: 508-278-3897 F: 508-278-2289

LEGEND

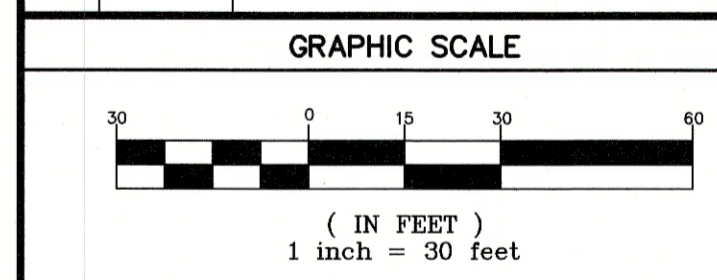
- CATCH BASIN (CONSTRUCTED) □
- CATCH BASIN (EXISTING) □
- DRAINAGE MANHOLE (EXISTING) ⊕
- FLARED END SECTION (CONSTRUCTED) ▽
- CHECK DAM (CONSTRUCTED) [Symbol]
- WOOD GUARDRAIL (CONSTRUCTED) [Symbol]
- BITUMINOUS CONCRETE BIT. CONC.
- SLOPED GRANITE CURB SGC
- BITUMINOUS CONCRETE CURB BCC
- EDGE OF PAVEMENT EOP
- NOW OR FORMERLY N/F
- SQUARE FEET S.F.

FRANKLIN ZONING INFORMATION:
ZONE: RURAL RESIDENTIAL I
SETBACKS:
FRONT 40'
SIDE 40'
REAR 40'

PROJECT: 7 & 11 BRANDYWINE ROAD
FRANKLIN, MA 02038

APPLICANT: STEPHEN ROLLINS
344 JOHN L. DIETSCH BOULEVARD
NORTH ATTLEBORO, MA 02760

REVISIONS		
NO.	DATE	DESCRIPTION



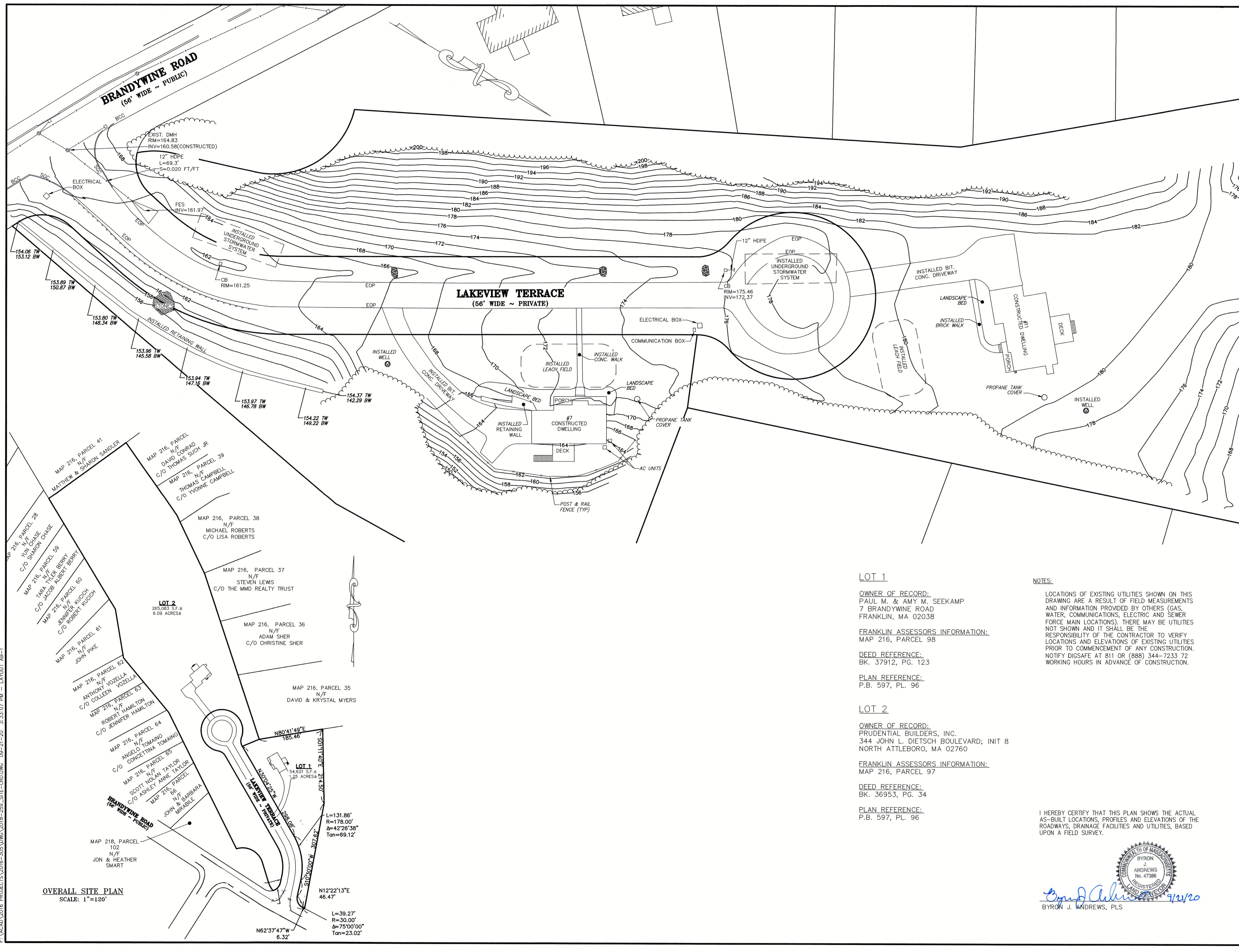
SHEET TITLE

CONSTRUCTION RECORD PLAN

I HEREBY CERTIFY THAT THIS PLAN SHOWS THE ACTUAL AS-BUILT LOCATIONS, PROFILES AND ELEVATIONS OF THE ROADWAYS, DRAINAGE FACILITIES AND UTILITIES, BASED UPON A FIELD SURVEY.

BYRON J. ANDREWS, PLS

DES BY: JJS DATE: 09/09/20
CHK BY: BJA PROJECT NO. 2018-259 **AB-1**



LOT 1

OWNER OF RECORD:
PAUL M. & AMY M. SEEKAMP
7 BRANDYWINE ROAD
FRANKLIN, MA 02038

FRANKLIN ASSESSORS INFORMATION:
MAP 216, PARCEL 98

DEED REFERENCE:
BK. 37912, PG. 123

PLAN REFERENCE:
P.B. 597, PL. 96

LOT 2

OWNER OF RECORD:
PRUDENTIAL BUILDERS, INC.
344 JOHN L. DIETSCH BOULEVARD; INIT 8
NORTH ATTLEBORO, MA 02760

FRANKLIN ASSESSORS INFORMATION:
MAP 216, PARCEL 97

DEED REFERENCE:
BK. 36953, PG. 34

PLAN REFERENCE:
P.B. 597, PL. 96

NOTES:

LOCATIONS OF EXISTING UTILITIES SHOWN ON THIS DRAWING ARE A RESULT OF FIELD MEASUREMENTS AND INFORMATION PROVIDED BY OTHERS (GAS, WATER, COMMUNICATIONS, ELECTRIC AND SEWER FORCE MAIN LOCATIONS). THERE MAY BE UTILITIES NOT SHOWN AND IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION. NOTIFY DIGSAFE AT 811 OR (888) 344-7233 72 WORKING HOURS IN ADVANCE OF CONSTRUCTION.

F:\ACAD\2016 PROJECTS\2016-259 SITE-CRD.DWG 09-21-20 3:33:07 PM - LAYOUT AB-1

© COPYRIGHT ANDREWS SURVEY & ENGINEERING, INC.



FRANKLIN PLANNING & COMMUNITY DEVELOPMENT

355 EAST CENTRAL STREET
FRANKLIN, MA 02038-1352
TELEPHONE: 508-520-4907
FAX: 508-520-4906

MEMORANDUM

DATE: September 23, 2020
TO: Franklin Planning Board
FROM: Department of Planning and Community Development
RE: Lakeview Terrace – Private Subdivision
Partial Form H

General

1. The Planning Board approved a Definitive Subdivision on August 9, 2009, known as Lakeview Terrace, located off of Brandywine Road. The Definitive Subdivision is for a private roadway to construct 2 single family homes. The signed Definitive Plan is included in this packet.
2. The Applicant was issued an occupancy permit for the first single family house in May 2020. They were to complete the site for the final occupancy permit.
3. The applicant has submitted a Partial Form H and Engineer's Certificate of Completion listing several outstanding items.
4. BETA has provided an observation report, with several outstanding items.

Comments

1. Applicant should provide a timeline when all outstanding items will be complete.
2. DPCD notes that the roadway will remain private and never be accepted by the Town.



TOWN OF FRANKLIN - SITE OBSERVATION REPORT

Lakeview Terrace

Report No.:	4831 60 - 11	Date:	September 22, 2020	Arrive:	12:45 PM
Observer:	Matt Crowley, PE	Weather:	Sunny ~60°	Leave:	1:30 PM
Owner:	Tony Marinella 28 Tia Place Franklin, MA 02038	Contractor:	N/A		

Items Observed: **Conformance Observation – Submitted in conjunction with Applicant’s request for acceptance of Form H – Certificate of Partial Release**

OBSERVATIONS

Observation Requested By: Tony Marinella

Met/walked site with: N/A

Current Activity on Site: No current activity

Observed Construction: BETA arrived on-site to perform a construction observation in conjunction with the Applicant’s request for acceptance of Form H – Certificate of Partial Release, dated September 21, 2020. An as-built plan, dated September 9, 2020, and prepared by Andrews Survey and Engineering, Inc. was provided via email. BETA’s site-walk and review of the Approved Plans, confirmed the site to be constructed in general conformance with the Approved Plans and approved field changes, with the following exceptions/ notations:

- Items included on the Subdivision Work Completion List and as described in the letter from Andrews Survey and Engineering, dated September 21, 2020.
- The Engineer of Record has indicated that the erosion problem along the slope above the retaining wall is a result of the catch basin in the swale being covered and flow overtopping the roadway; however, the as-built plan also indicates that the cross-slope of the roadway near the intersection of Brandywine Road is pitched toward the slope above the retaining wall. It is anticipated that stormwater will continue to flow toward the eroded slope unless measures are implemented to direct roadway flow to the swale as depicted on the Approved Plans.
- There is a small gap between the existing bituminous curb on Brandywine Road and the newly installed granite radius curb. The gap should be patched to prevent water from washing out the loam and gravel behind the curb.
- The Department of Planning and Community Development has requested that the Fire Chief confirm that they are satisfied that the existing fire hydrant on Brandywine Road is adequate for fire protection, as there is no water infrastructure on the project roadway.
- The installed wooden guardrail does not appear to conform to the detail/notes on the Approved Plans. BETA has requested that the design engineer confirm the installed guardrail meets the intent of the approved design.

SITE PHOTOS



Typical roadway



Installed guardrail



Recently seeded swale with check dams



Catch basin that requires removal of filter fabric and possible cleaning



Installed radius stone and bituminous patch at Brandywine Road



Recently seeded cul-de-sac island



Gap between curbing at Brandywine Road



Swale that requires regrading plus loam and seed near cul-de-sac



Erosion on slope located above retaining wall



Constructed roadway at intersection of Brandywine Road



FRANKLIN FIRE DEPARTMENT

To : DPCD

FROM : J. S. BARBIERI, DEPUTY FIRE CHIEF

DATE : 23 SEPTEMBER 2020

RE : LAKEVIEW TERRACE –BRANDYWINE ROAD

THE FIRE CHIEF HAS REVIEWED THE PROJECT AND HAS INDICATED THAT A FIRE HYDRANT IS NO LONGER REQUIRED.

cc: file

TOWN OF FRANKLIN
 2009 DEC 28 AM 11:36
 RECEIVED

DEFINITIVE SUBDIVISION PLAN

"LAKEVIEW TERRACE"

FRANKLIN, MASSACHUSETTS

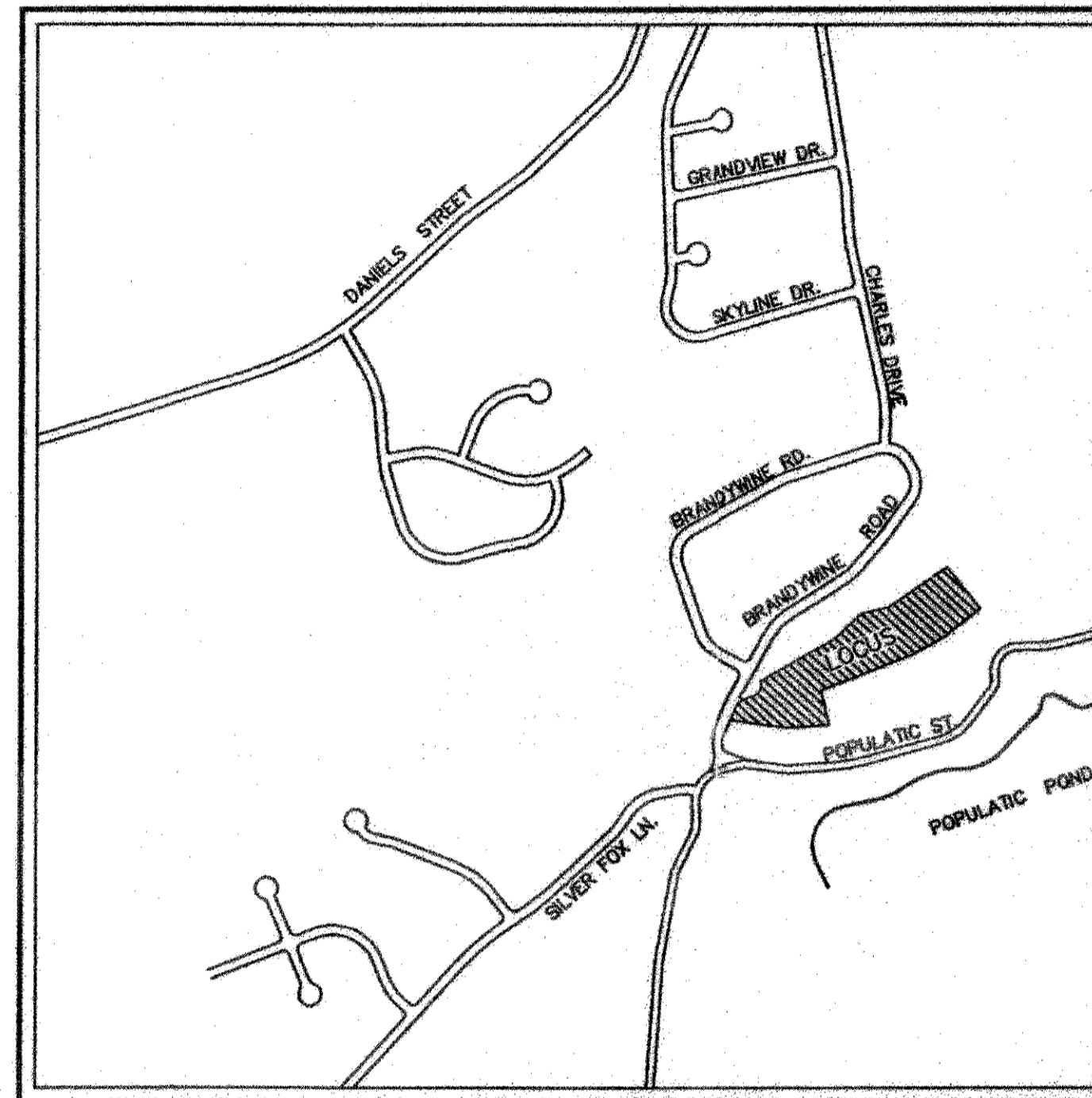
APPROVED BY
 FRANKLIN PLANNING BOARD:
Ronald E. Libler
[Signature]

APPROVED BY: _____
 ENDORSED BY: _____

APPROVED 12/21/09 SUBJECT TO COVENANT
 CONDITIONS SET FORTH IN A COVENANT EXECUTED
 BY _____ DATED _____
 AND TO BE RECORDED HEREWITH.

I TOWN CLERK OF THE TOWN OF NORFOLK,
 RECEIVED & RECORDED FROM THE PLANNING
 BOARD APPROVAL OF THIS PLAN AND NO
 APPEAL HAS BEEN TAKEN FOR TWENTY DAYS
 NEXT AFTER RECEIPT AND RECORDING OF SAME.

TOWN CLERK _____ DATE _____



LOCUS MAP
 1"=800'

NOTES:

- ON AUGUST 10, 2009, THE TOWN OF FRANKLIN PLANNING BOARD VOTED TO APPROVE WITH CONDITIONS THE DEFINITIVE SUBDIVISION OF "LAKEVIEW TERRACE". FOR CONDITIONS OF APPROVAL SEE CERTIFICATE OF VOTE-DEFINITIVE SUBDIVISION P. IN LAKEVIEW TERRACE-REMAND, DATED AUGUST 12, 2009.
- ALL EROSION MITIGATION MEASURES SHALL BE IN PLACE PRIOR TO MAJOR ROAD CONSTRUCTION COMMENCING ON THE SITE.
- LAMP POSTS SHALL BE INSTALLED ON LOTS 1 AND 2 IN LIEU OF STREET LIGHTS.

Waivers Allowed:

- Section 300-10.C.(1) A private roadway shall be constructed with a pavement width of 18' at the subdivision entrance culminating with a turn around with a turning radius of 42' and 12' pavement width continuing with a 12' private driveway to insure safe and timely movement for emergency vehicles.
- Section 300-10.D.(1) Dead End Streets: To allow constructed of a pavement width of 18' at the subdivision entrance culminating with a turn around with a turning radius of 42' and 12' pavement width continuing with a 12' private driveway to insure safe and timely movement for emergency vehicles.
- Section 300-10.H.(1) Curbing: A waiver to require curbing only along the intersection roundings and no curbing along the remaining roadway. (The proposal is to construct a roadway with a continuous cross slope directing the surface water runoff to a proposed swale constructed along one side of the roadway.)
- Section 300-11.A.(3) Stormwater Management: Waiver to allow stormwater drainage components to be located within an easement in lieu of being located on a separate drainage lot that conforms to the zoning requirements.
- Section 300-13.C.(2) Street Lighting: No street lights be required but rather lampposts be installed at each proposed driveway.
- Section 300-13.A.(1) Sidewalks: A waiver for no sidewalk required.
- Section 300-10.D.(5) Right of Way Grade: Waiver to allow the proposed grades within the right of way to exceed 5 feet above or below the existing grades.

Owner / Applicant:
BLUEVIEW CONSTRUCTION INC.
 43 PINE STREET
 FRANKLIN, MASSACHUSETTS 02038

PREPARED BY:
GLM ENGINEERING CONSULTANTS, INC.
 19 EXCHANGE STREET
 HOLLISTON, MASSACHUSETTS 01746
 (508)429-1100 fax:(508)429-7160

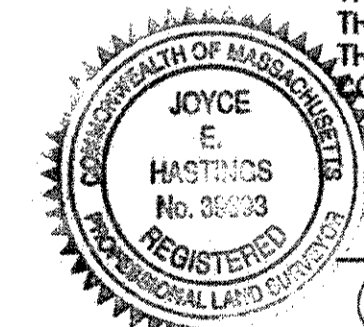
JUNE 26, 2008
 REVISED: SEPTEMBER 22, 2008
 JULY 21, 2009
 AUGUST 4, 2009
 AUGUST 11, 2009

ZONING CLASSIFICATION:
 RURAL RESIDENTIAL I
 MIN. AREA: 40,000 S.F.
 FRONTAGE: 200 FEET
 DEPTH: 200 FEET
 CIRCLE: 180 FT. DIA.
 SETBACKS:
 FRONT: 40 FT.
 SIDE: 40 FT.
 REAR: 40 FT.

FLOOD_ZONE CLASSIFICATION:
 F.E.M.A. COMMUNITY PANEL
 NO. 250240 0002 B
 FLOOD_ZONE "C"

ASSESSOR REFERENCE:
 MAP 218, PARCELS 97 - 103

I CERTIFY THAT THIS PLAN CONFORMS TO THE
 RULES & REGULATIONS OF THE REGISTER OF DEEDS.
 THIS PLAN WAS PREPARED IN ACCORDANCE WITH
 THE PROCEDURAL AND TECHNICAL STANDARDS FOR
 THE PRACTICE OF LAND SURVEYING IN THE
 COMMONWEALTH OF MASSACHUSETTS.



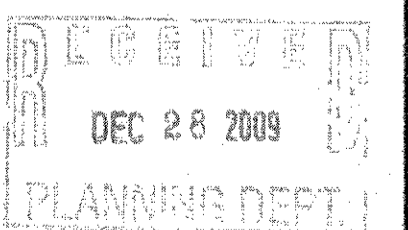
Joyce E. Hastings
 JOYCE E. HASTINGS P.L.S. 12-10-09
 DATE



Paul E. Truax
 PAUL E. TRUAX P.E. 12-10-09
 DATE

SHEET INDEX

- | | |
|---|----------------|
| 1 | COVER |
| 2 | LAYOUT |
| 3 | CONTOURS |
| 4 | PLAN & PROFILE |
| 5 | DETAILS |
| 6 | DETAILS |



JOB No. 9087
 SCALE: AS NOTED
 DATE: JUNE 28, 2006
 SHEET No. 1 OF 6

APPROVED BY
FRANKLIN PLANNING BOARD:

Ronald E. Colburn

I TOWN CLERK OF THE TOWN OF NORFOLK,
RECEIVED & RECORDED FROM THE PLANNING
BOARD APPROVAL OF THIS PLAN AND NO
APPEAL HAS BEEN TAKEN FOR TWENTY DAYS
NEXT AFTER RECEIPT AND RECORDING OF SAME.

TOWN CLERK _____ DATE _____

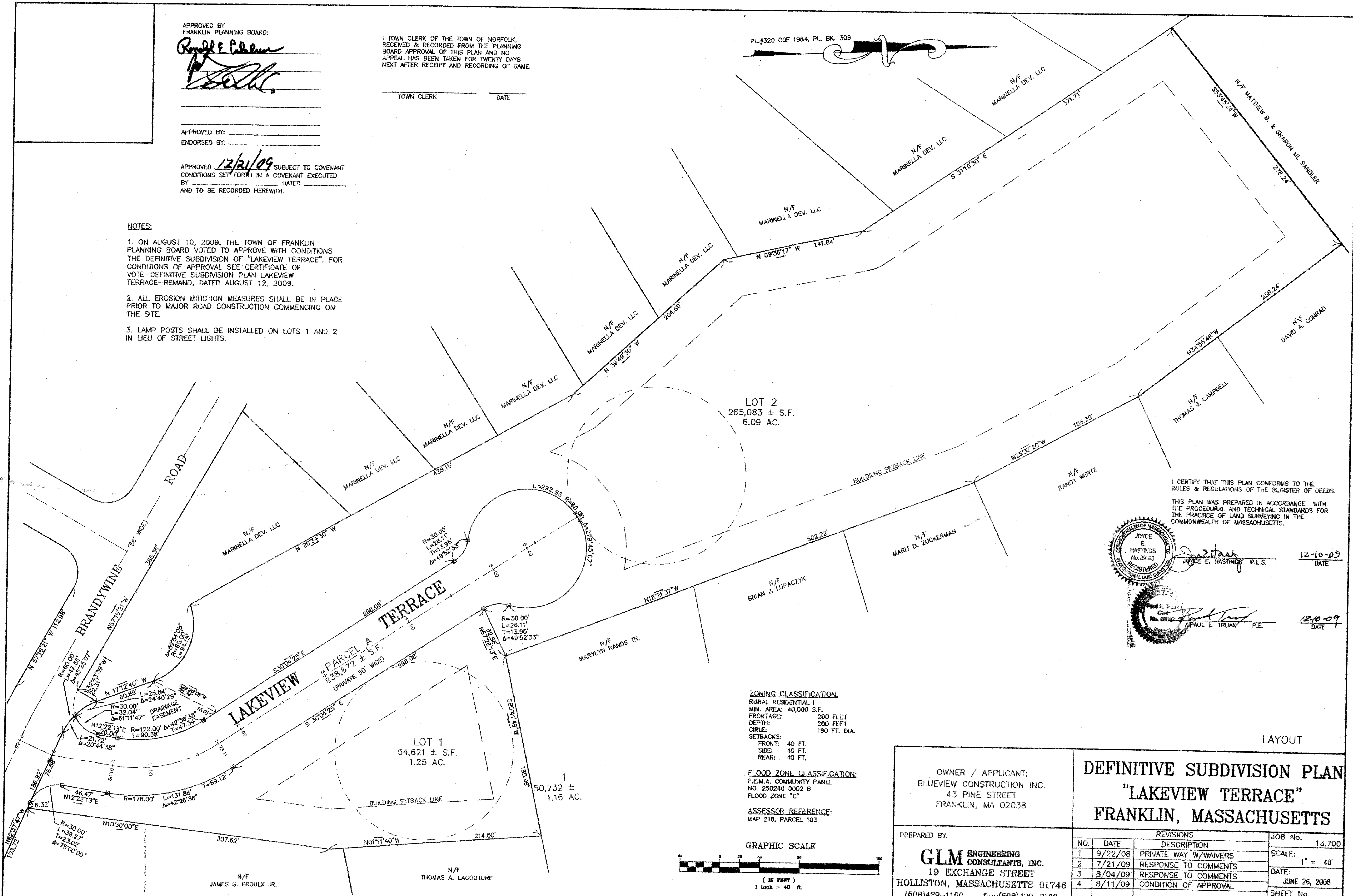
APPROVED BY: _____
ENDORSED BY: _____

APPROVED 12/21/09 SUBJECT TO COVENANT
CONDITIONS SET FORTH IN A COVENANT EXECUTED
BY _____ DATED _____
AND TO BE RECORDED HERewith.

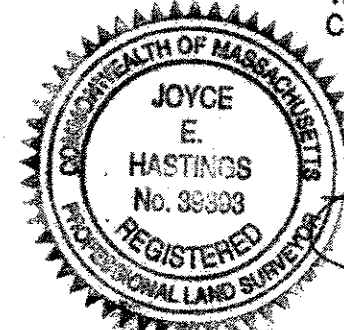
NOTES:

- ON AUGUST 10, 2009, THE TOWN OF FRANKLIN PLANNING BOARD VOTED TO APPROVE WITH CONDITIONS THE DEFINITIVE SUBDIVISION OF "LAKEVIEW TERRACE". FOR CONDITIONS OF APPROVAL SEE CERTIFICATE OF VOTE-DEFINITIVE SUBDIVISION PLAN LAKEVIEW TERRACE-REMAND, DATED AUGUST 12, 2009.
- ALL EROSION MITIGATION MEASURES SHALL BE IN PLACE PRIOR TO MAJOR ROAD CONSTRUCTION COMMENCING ON THE SITE.
- LAMP POSTS SHALL BE INSTALLED ON LOTS 1 AND 2 IN LIEU OF STREET LIGHTS.

PL.#320 OOF 1984, PL. BK. 309

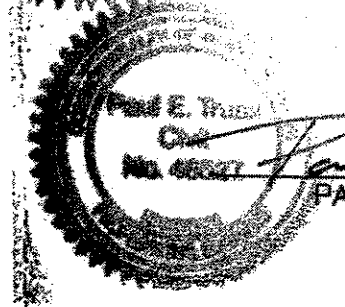


I CERTIFY THAT THIS PLAN CONFORMS TO THE RULES & REGULATIONS OF THE REGISTER OF DEEDS.
THIS PLAN WAS PREPARED IN ACCORDANCE WITH THE PROCEDURAL AND TECHNICAL STANDARDS FOR THE PRACTICE OF LAND SURVEYING IN THE COMMONWEALTH OF MASSACHUSETTS.



Joyce E. Hastings
JOYCE E. HASTINGS P.L.S.

12-10-09
DATE



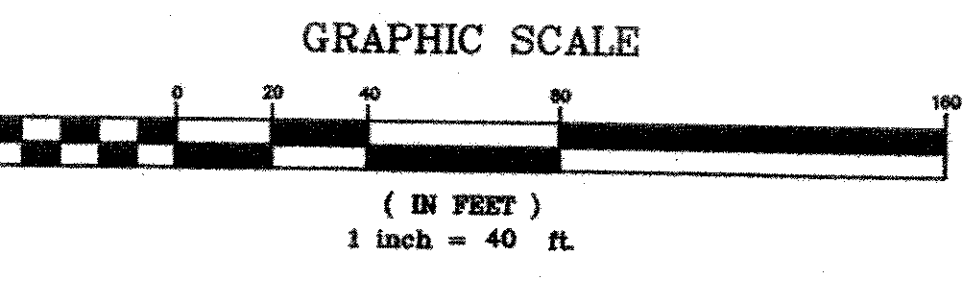
Paul E. Truax
PAUL E. TRUAX P.E.

12-10-09
DATE

ZONING CLASSIFICATION:
RURAL RESIDENTIAL I
MIN. AREA: 40,000 S.F.
FRONTAGE: 200 FEET
DEPTH: 200 FEET
CIRCLE: 180 FT. DIA.
SETBACKS:
FRONT: 40 FT.
SIDE: 40 FT.
REAR: 40 FT.

FLOOD ZONE CLASSIFICATION:
F.E.M.A. COMMUNITY PANEL
NO. 250240 0002 B
FLOOD ZONE "C"

ASSESSOR REFERENCE:
MAP 218, PARCEL 103



OWNER / APPLICANT: BLUEVIEW CONSTRUCTION INC. 43 PINE STREET FRANKLIN, MA 02038		DEFINITIVE SUBDIVISION PLAN "LAKEVIEW TERRACE" FRANKLIN, MASSACHUSETTS	
PREPARED BY: GLM ENGINEERING CONSULTANTS, INC. 19 EXCHANGE STREET HOLLISTON, MASSACHUSETTS 01746 (508)429-1100 fax:(508)429-7160			
LAYOUT			
		JOB No. 13,700	
		SCALE: 1" = 40'	
		DATE: JUNE 26, 2008	
		SHEET No. 2 OF 6	
REVISIONS			
NO.	DATE	DESCRIPTION	JOB No.
1	9/22/08	PRIVATE WAY W/WAIVERS	13,700
2	7/21/09	RESPONSE TO COMMENTS	
3	8/04/09	RESPONSE TO COMMENTS	
4	8/11/09	CONDITION OF APPROVAL	

APPROVED BY
FRANKLIN PLANNING BOARD:

[Signature]
[Signature]

APPROVED BY: _____
ENDORSED BY: _____

APPROVED 12/21/09 SUBJECT TO COVENANT
CONDITIONS SET FORTH IN A COVENANT EXECUTED
BY _____ DATED _____
AND TO BE RECORDED HEREWITH.

I, TOWN CLERK OF THE TOWN OF NORFOLK,
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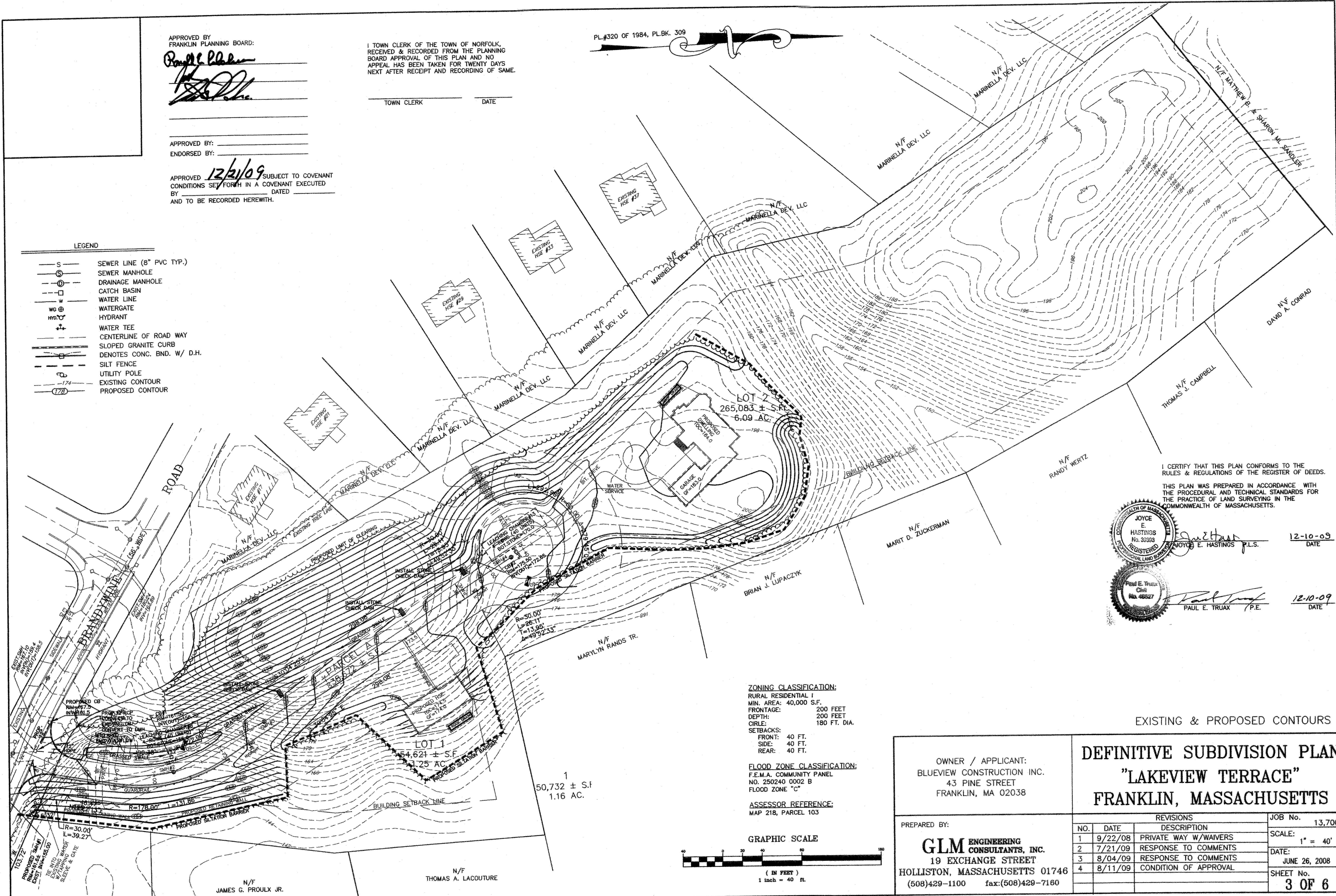
TOWN CLERK _____ DATE _____

PL.#320 OF 1984, PL.BK. 309



LEGEND

- S — SEWER LINE (8" PVC TYP.)
- ⊙ SEWER MANHOLE
- ⊖ DRAINAGE MANHOLE
- CATCH BASIN
- W — WATER LINE
- ⊕ WATERGATE
- ⊕ HYDRANT
- ⊕ WATER TEE
- C — CENTERLINE OF ROAD WAY
- S — SLOPED GRANITE CURB
- D — DENOTES CONC. BND. W/ D.H.
- F — SILT FENCE
- ⊕ UTILITY POLE
- 174 — EXISTING CONTOUR
- 178 — PROPOSED CONTOUR



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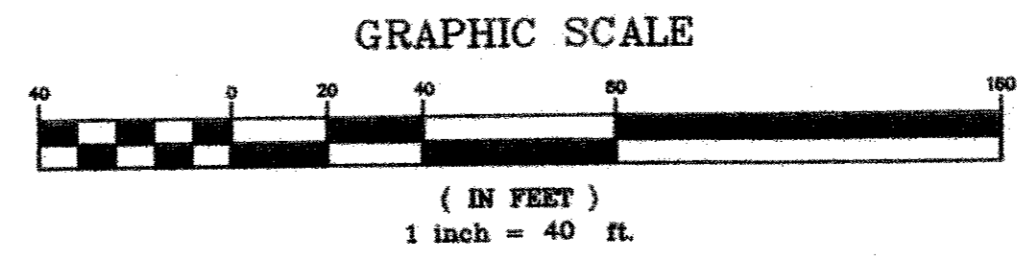


[Signature] JOYCE E. HASTINGS P.L.S. 12-10-09 DATE
[Signature] PAUL E. TRUAX P.E. 12-10-09 DATE

ZONING CLASSIFICATION:
RURAL RESIDENTIAL 1
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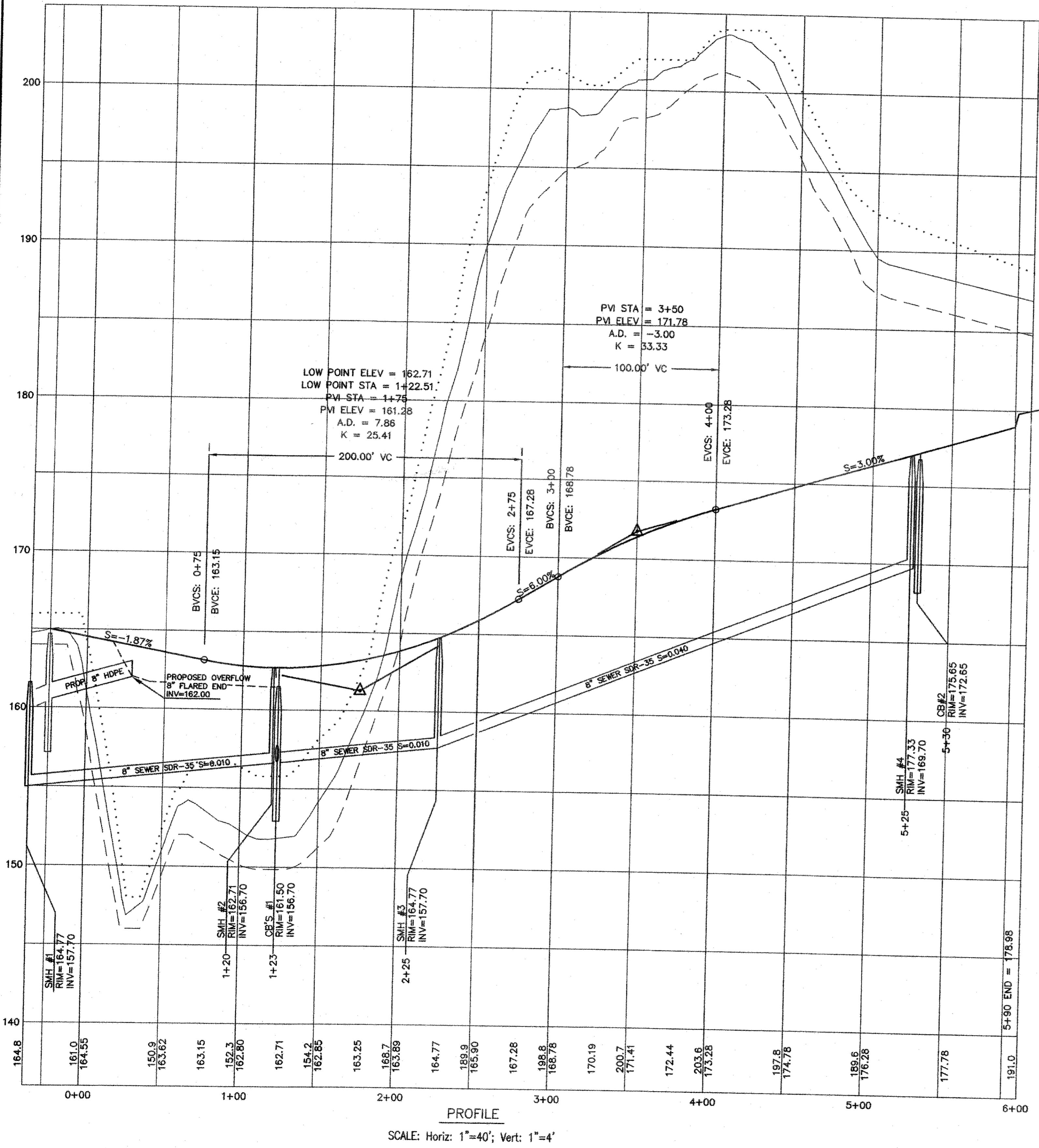
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ASSESSOR REFERENCE:
MAP 218, PARCEL 103

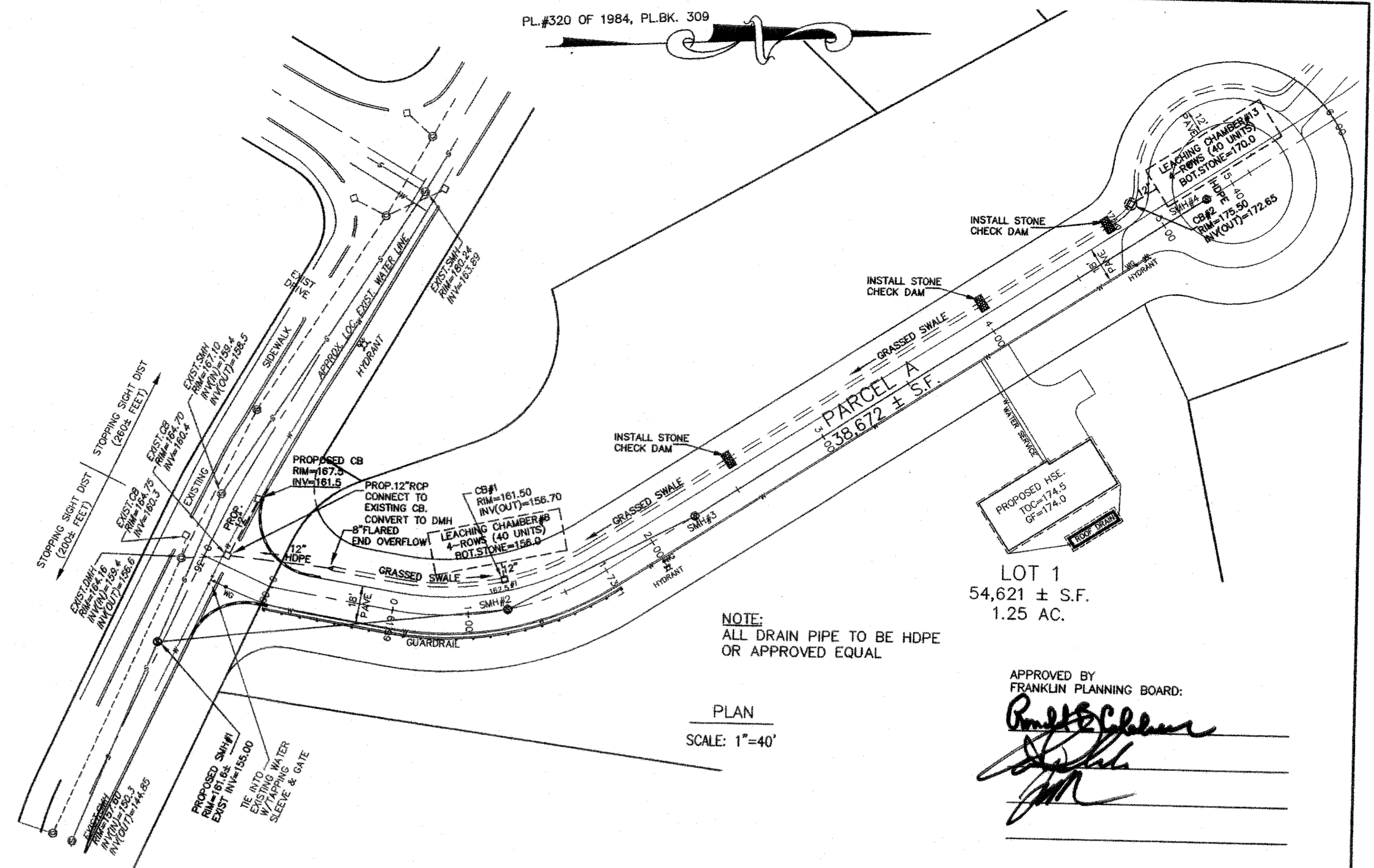


EXISTING & PROPOSED CONTOURS

OWNER / APPLICANT: BLUEVIEW CONSTRUCTION INC. 43 PINE STREET FRANKLIN, MA 02038		DEFINITIVE SUBDIVISION PLAN "LAKEVIEW TERRACE" FRANKLIN, MASSACHUSETTS	
PREPARED BY: GLM ENGINEERING CONSULTANTS, INC. 19 EXCHANGE STREET HOLLISTON, MASSACHUSETTS 01746 (508)429-1100 fax:(508)429-7180			
JOB No. 13,700		SCALE: 1" = 40'	
DATE: JUNE 26, 2008		SHEET No. 3 OF 6	
NO.	DATE	REVISIONS DESCRIPTION	
1	9/22/08	PRIVATE WAY W/WAIVERS	
2	7/21/09	RESPONSE TO COMMENTS	
3	8/04/09	RESPONSE TO COMMENTS	
4	8/11/09	CONDITION OF APPROVAL	



PROFILE
SCALE: Horiz: 1"=40'; Vert: 1"=4'



PLAN
SCALE: 1"=40'

- LEGEND**
- S — SEWER LINE (8" PVC TYP.)
 - ⊙ SEWER MANHOLE
 - ⊕ DRAINAGE MANHOLE
 - CATCH BASIN
 - W — WATER LINE
 - ⊕ W/G WATERGATE
 - ⊕ H/H HYDRANT
 - ⊕ W/T WATER TEE
 - C — CENTERLINE OF ROAD WAY
 - CG — SLOPED GRANITE CURB
 - ⊕ DENOTES CONC. BND. W/ D.H.
 - S — SILT FENCE
 - ⊕ U UTILITY POLE
 - 174 — EXISTING CONTOUR
 - 178 — PROPOSED CONTOUR

ZONING CLASSIFICATION:
RURAL RESIDENTIAL 1
MIN. AREA: 40,000 S.F.
FRONTAGE: 200 FEET
DEPTH: 200 FEET
CIRCLE: 180 FT. DIA.
SETBACKS:
FRONT: 40 FT.
SIDE: 40 FT.
REAR: 40 FT.

FLOOD ZONE CLASSIFICATION:
F.E.M.A. COMMUNITY PANEL
NO. 250240 0002 B
FLOOD ZONE "C"

ASSESSOR REFERENCE:
MAP 218, PARCEL 103

LOT 1
54,621 ± S.F.
1.25 AC.

APPROVED BY:
FRANKLIN PLANNING BOARD:
[Signature]

APPROVED BY: _____
ENDORSED BY: _____

APPROVED 12/21/09 SUBJECT TO COVENANT
CONDITIONS SET FORTH IN A COVENANT EXECUTED
BY _____ DATED _____
AND TO BE RECORDED HEREWITH.

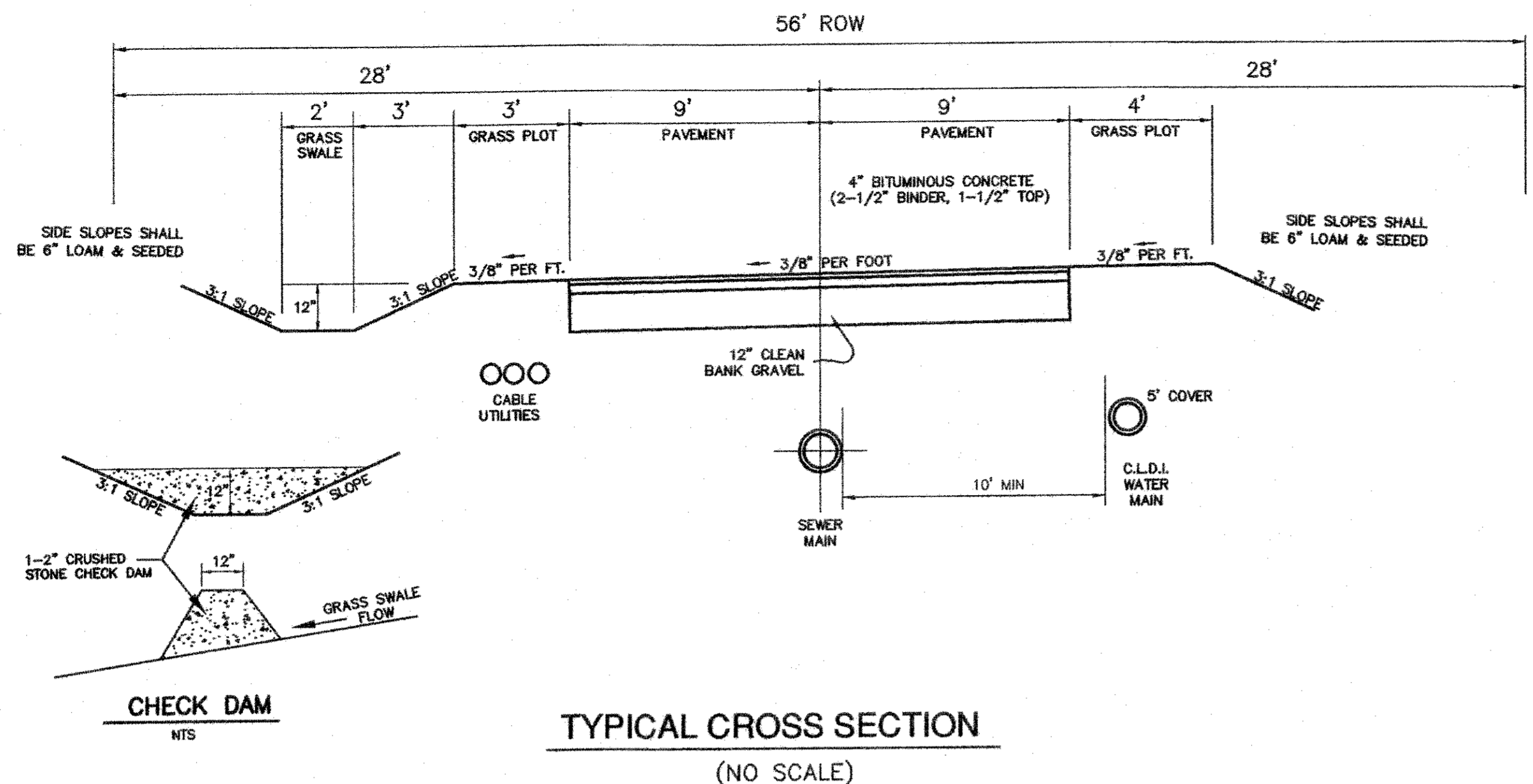
I, TOWN CLERK OF THE TOWN OF NORFOLK,
RECEIVED & RECORDED FROM THE PLANNING
BOARD APPROVAL OF THIS PLAN AND NO
APPEAL HAS BEEN TAKEN FOR TWENTY DAYS
NEXT AFTER RECEIPT AND RECORDING OF SAME.



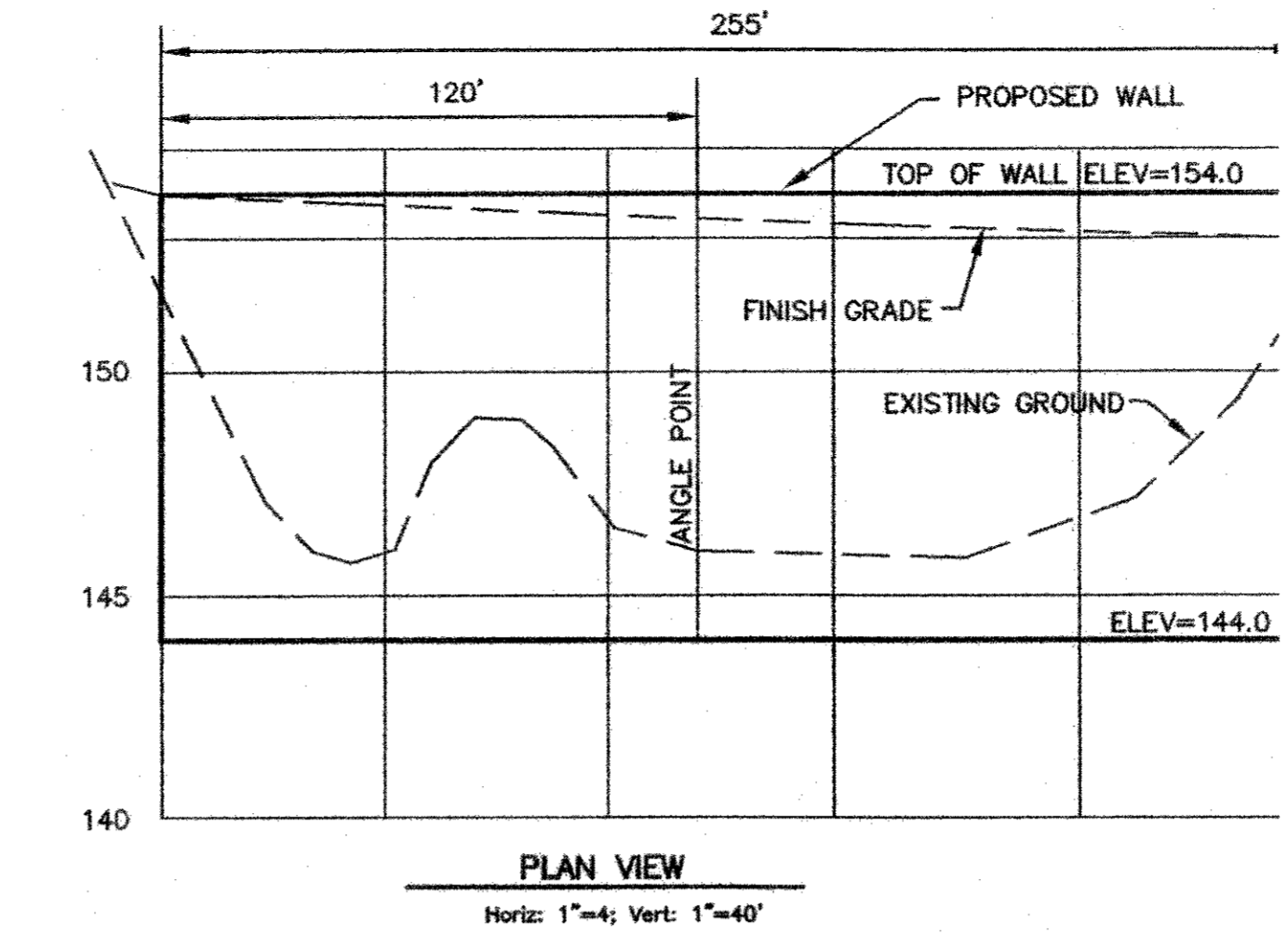
TOWN CLERK _____ DATE _____
[Signature]
PAUL E. TRUAX P.E. 12-10-09
DATE

PLAN & PROFILE

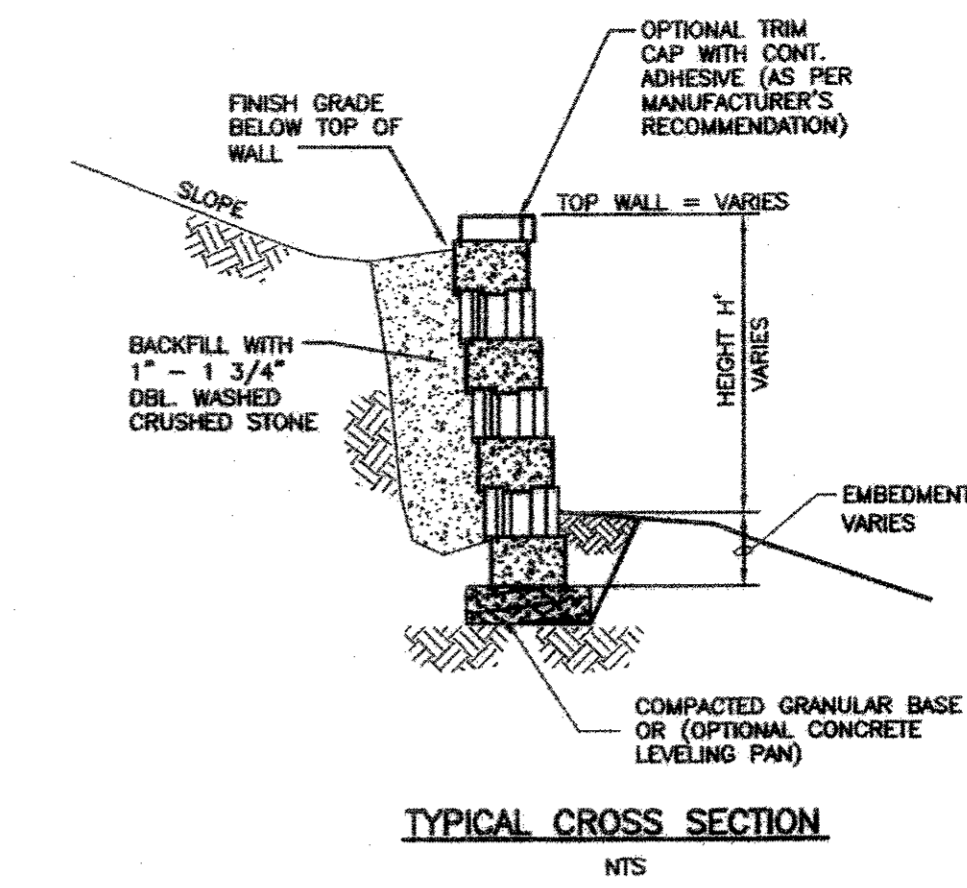
OWNER / APPLICANT: BLUEVIEW CONSTRUCTION INC. 43 PINE STREET FRANKLIN, MA 02038		DEFINITIVE SUBDIVISION PLAN "LAKEVIEW TERRACE" FRANKLIN, MASSACHUSETTS																					
PREPARED BY: GLM ENGINEERING CONSULTANTS, INC. 19 EXCHANGE STREET HOLLISTON, MASSACHUSETTS 01746 (508)429-1100 fax:(508)429-7160				<table border="1"> <thead> <tr> <th>NO.</th> <th>DATE</th> <th>DESCRIPTION</th> <th>JOB No.</th> </tr> </thead> <tbody> <tr> <td>1</td> <td>9/22/08</td> <td>PRIVATE WAY W/WAIVERS</td> <td>13,700</td> </tr> <tr> <td>2</td> <td>7/21/09</td> <td>RESPONSE TO COMMENTS</td> <td>SCALE: AS SHOWN</td> </tr> <tr> <td>3</td> <td>8/04/09</td> <td>RESPONSE TO COMMENTS</td> <td>DATE: JUNE 26, 2008</td> </tr> <tr> <td>4</td> <td>8/11/09</td> <td>CONDITION OF APPROVAL</td> <td>SHEET No. 4 OF 6</td> </tr> </tbody> </table>		NO.	DATE	DESCRIPTION	JOB No.	1	9/22/08	PRIVATE WAY W/WAIVERS	13,700	2	7/21/09	RESPONSE TO COMMENTS	SCALE: AS SHOWN	3	8/04/09	RESPONSE TO COMMENTS	DATE: JUNE 26, 2008	4	8/11/09
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4	8/11/09	CONDITION OF APPROVAL	SHEET No. 4 OF 6																				



TYPICAL CROSS SECTION
(NO SCALE)



ROCKWOOD RETAINING WALL OR APPROVED EQUAL
(NOT TO SCALE)



TYPICAL CROSS SECTION
N.T.S.

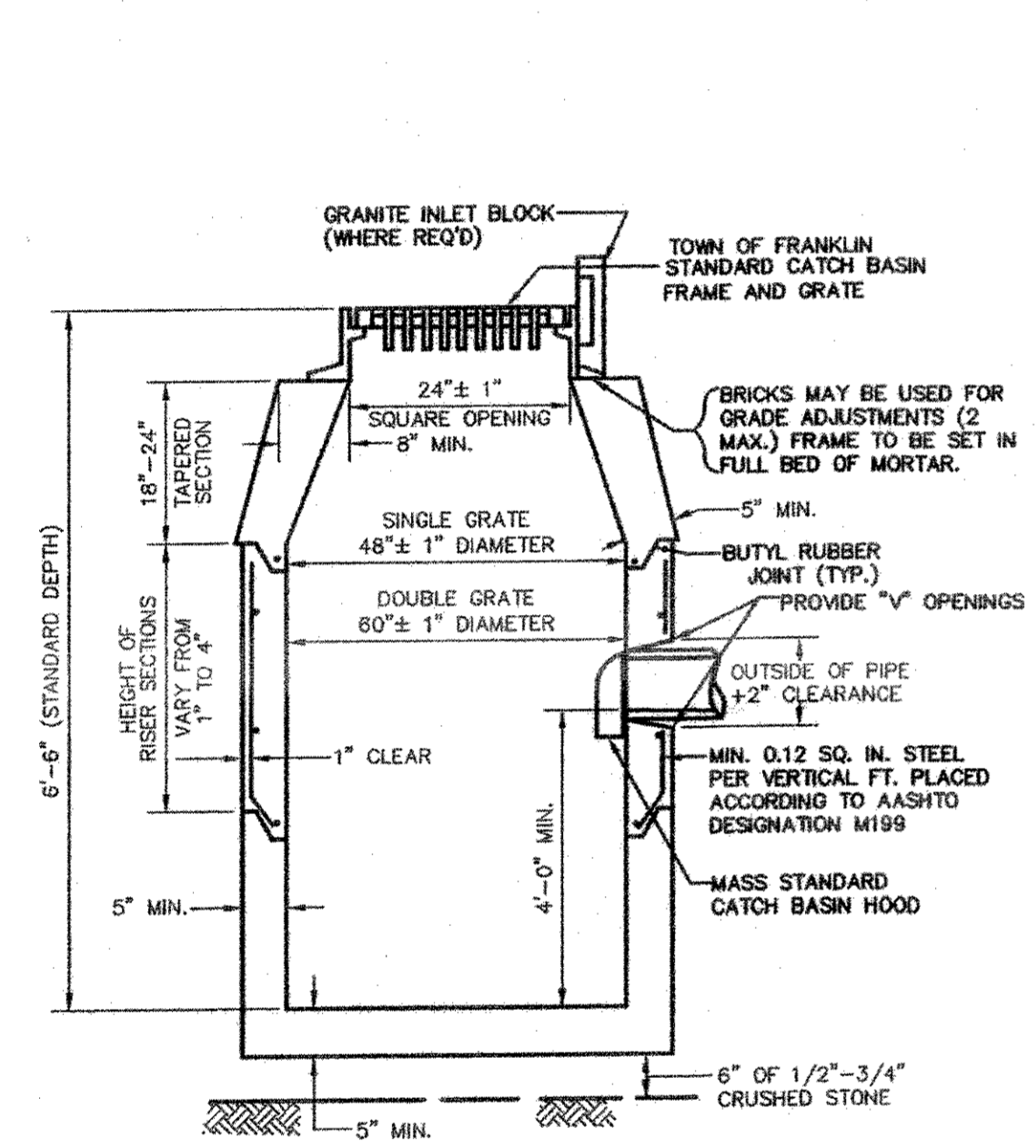
APPROVED BY
FRANKLIN PLANNING BOARD:
Paul E. Truax

APPROVED BY: _____
ENDORSED BY: _____

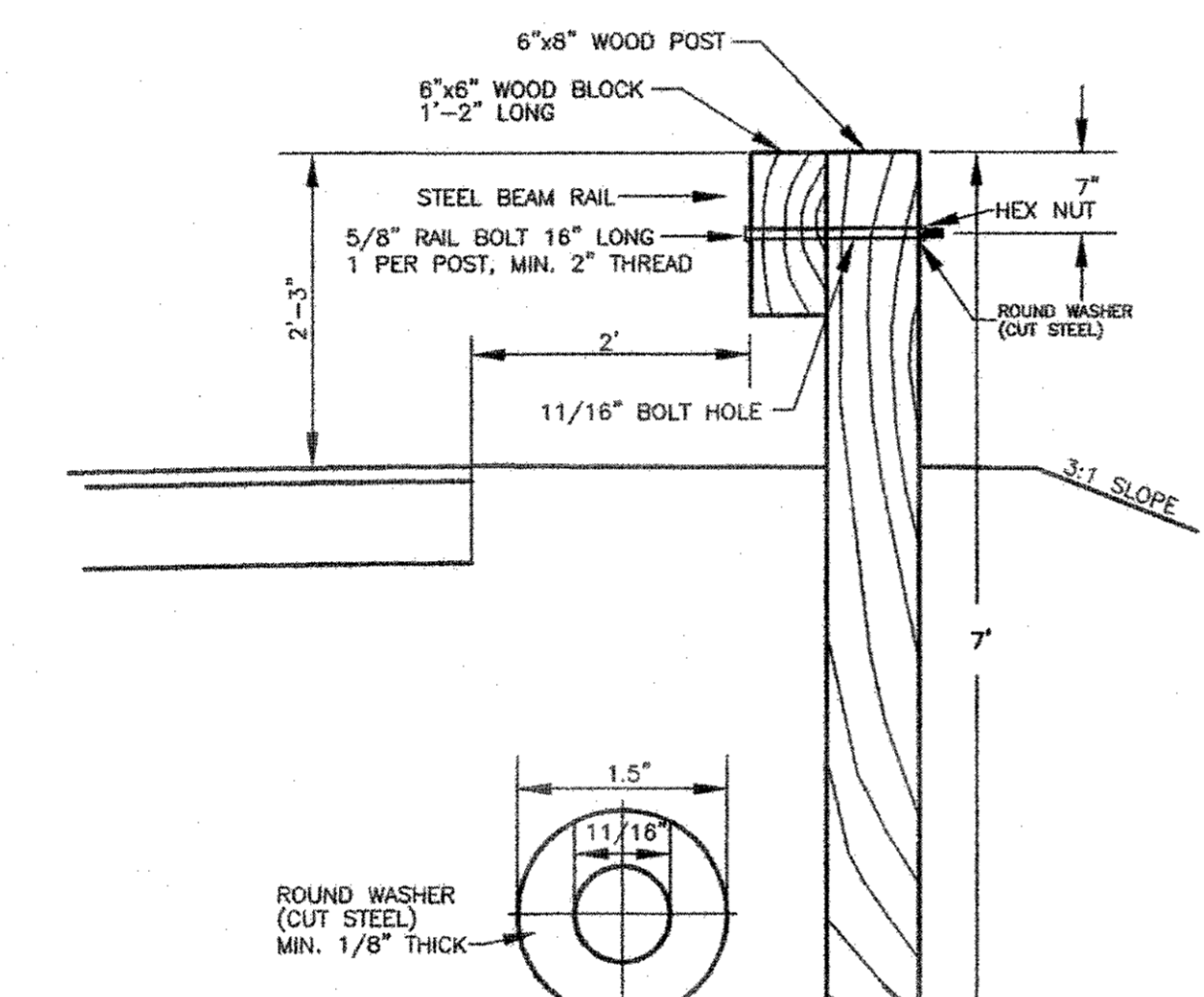
APPROVED 12/10/09 SUBJECT TO COVENANT
CONDITIONS SET FORTH IN A COVENANT EXECUTED
BY _____ DATED _____
AND TO BE RECORDED HEREWITH.

I, TOWN CLERK OF THE TOWN OF NORFOLK,
RECEIVED & RECORDED FROM THE PLANNING
BOARD APPROVAL OF THIS PLAN AND NO
APPEAL HAS BEEN TAKEN FOR TWENTY DAYS
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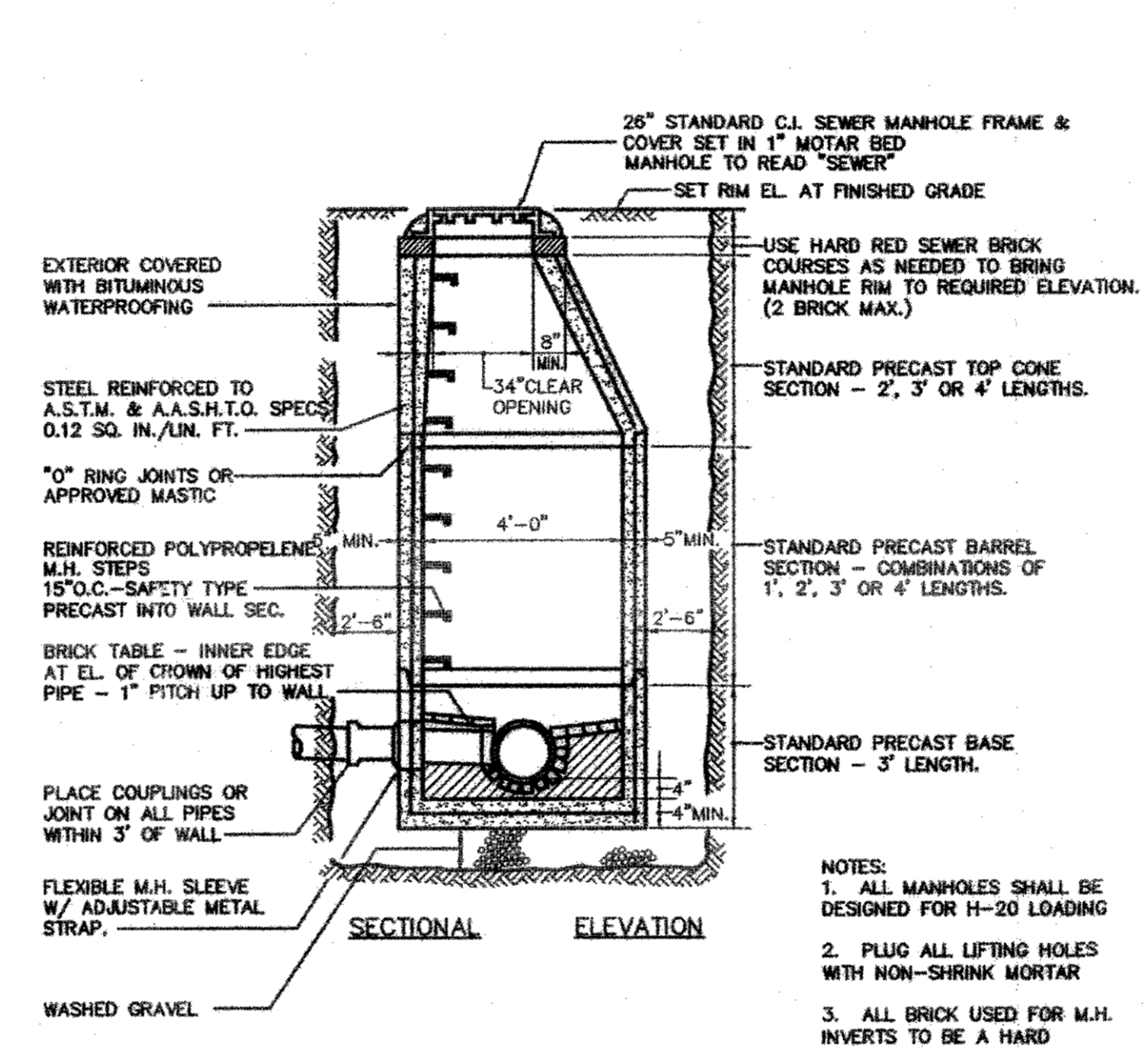
TOWN CLERK _____ DATE _____



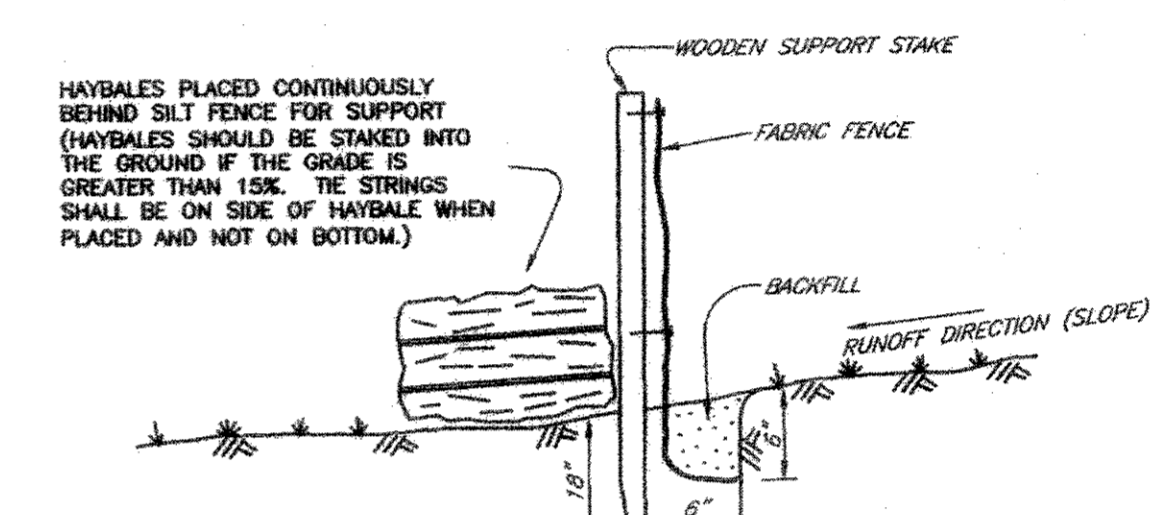
PRECAST CONCRETE CATCH BASIN DETAILS
NOT TO SCALE



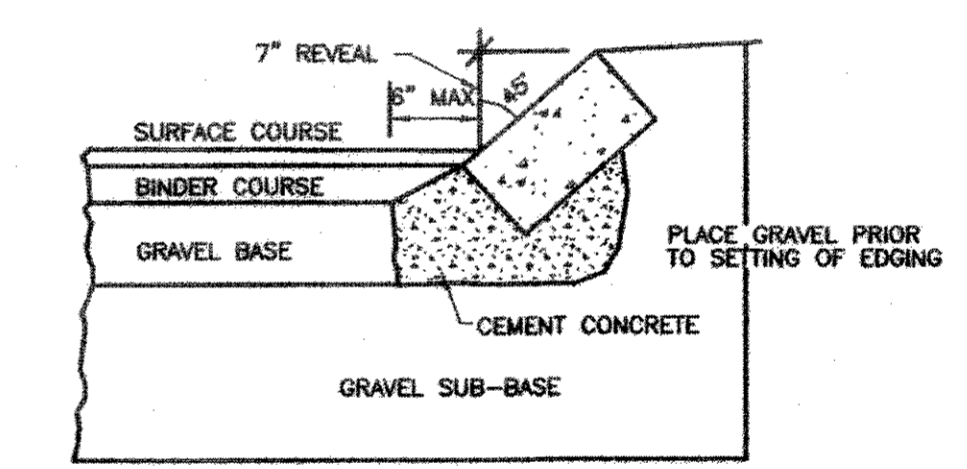
SINGLE FACE GUARD RAIL WITH WOOD POST
N.T.S.



TYPICAL SEWER MANHOLE DETAIL
NOT TO SCALE

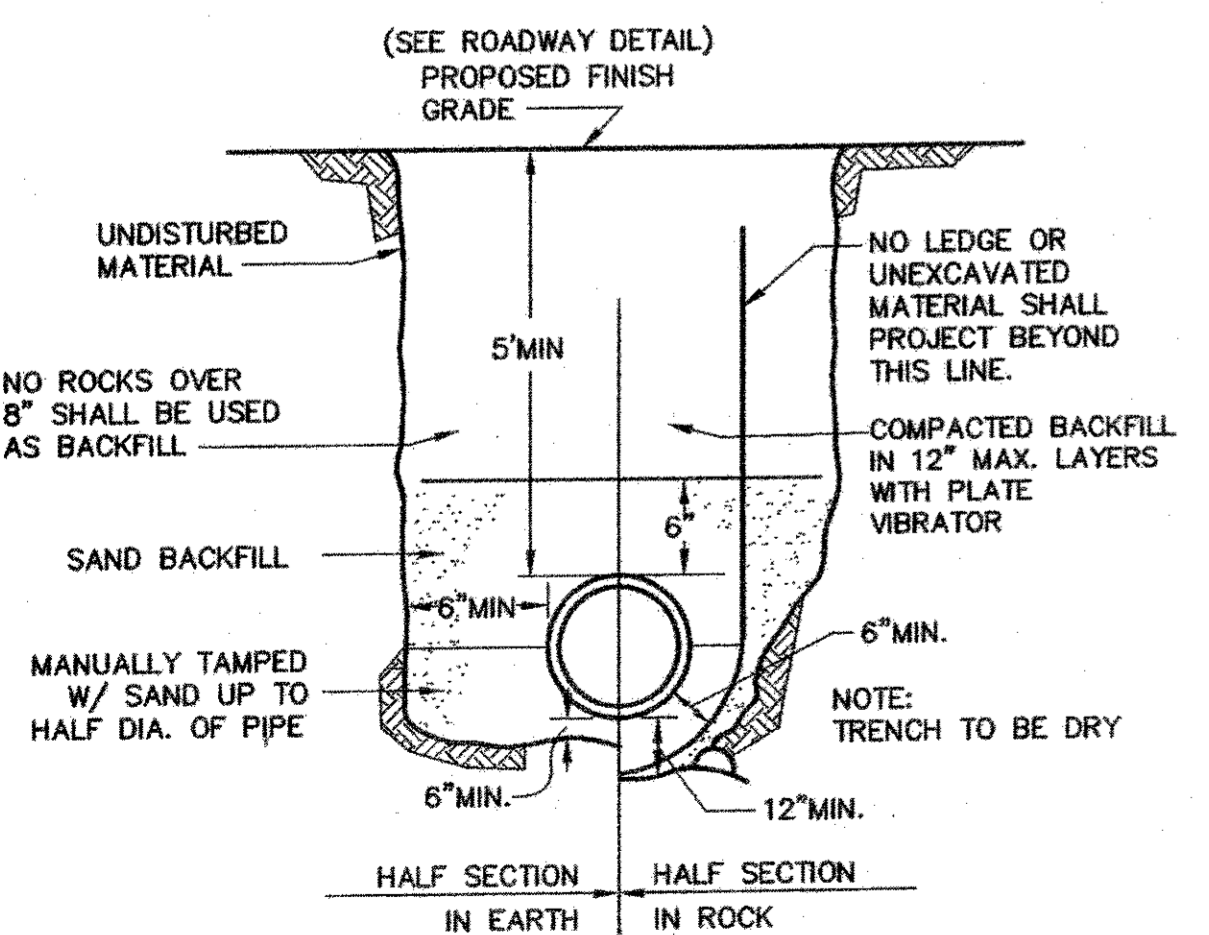


SILT FENCE DETAIL (TYP.)
(NOT TO SCALE)

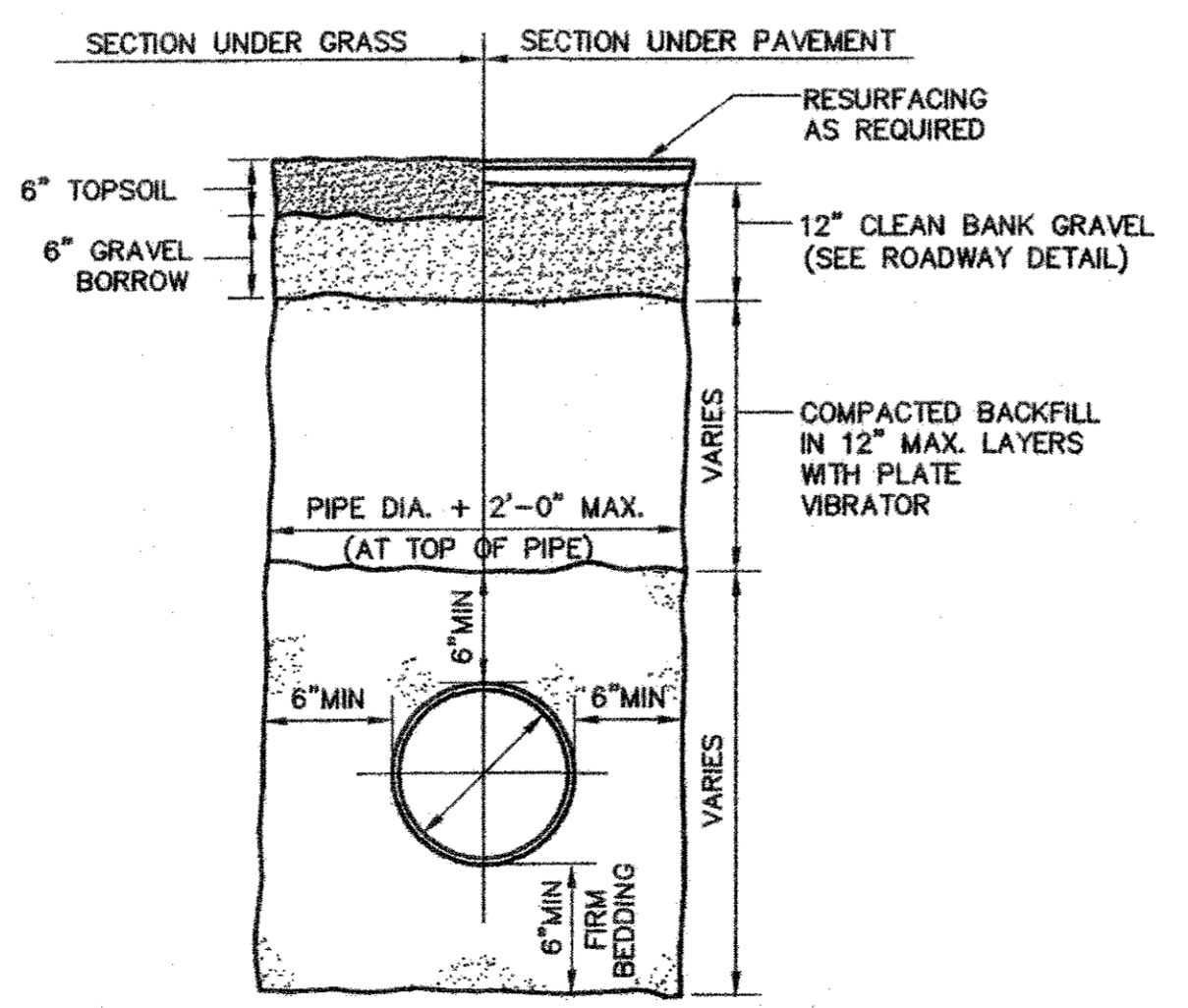


METHOD OF SETTING PRE-CAST
GRANITE EDGING
N.T.S.

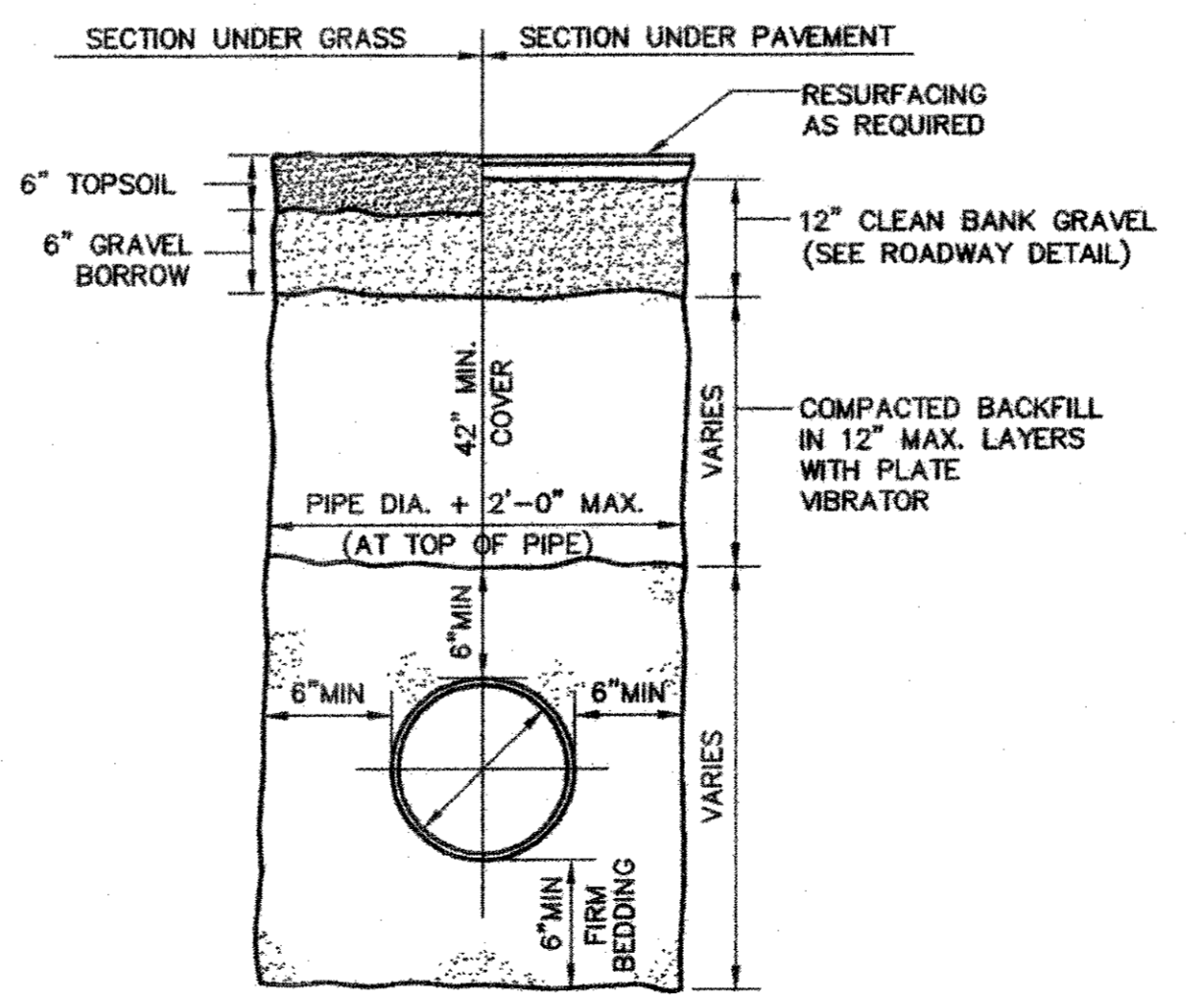
NOTE:
IT SHALL BE THE RESPONSIBILITY OF THE
CONTRACTOR TO VERIFY LOCATIONS AND
ELEVATIONS OF EXISTING UTILITIES PRIOR
TO COMMENCEMENT OF ANY CONSTRUCTION.
DIGSAFE IS TO BE NOTIFIED 72 WORKING
HOURS IN ADVANCE OF CONSTRUCTION.
DIGSAFE 1-888-DIG-SAFE



TYPICAL WATER TRENCH DETAIL
NOT TO SCALE



TYPICAL SEWER TRENCH DETAIL
NOT TO SCALE



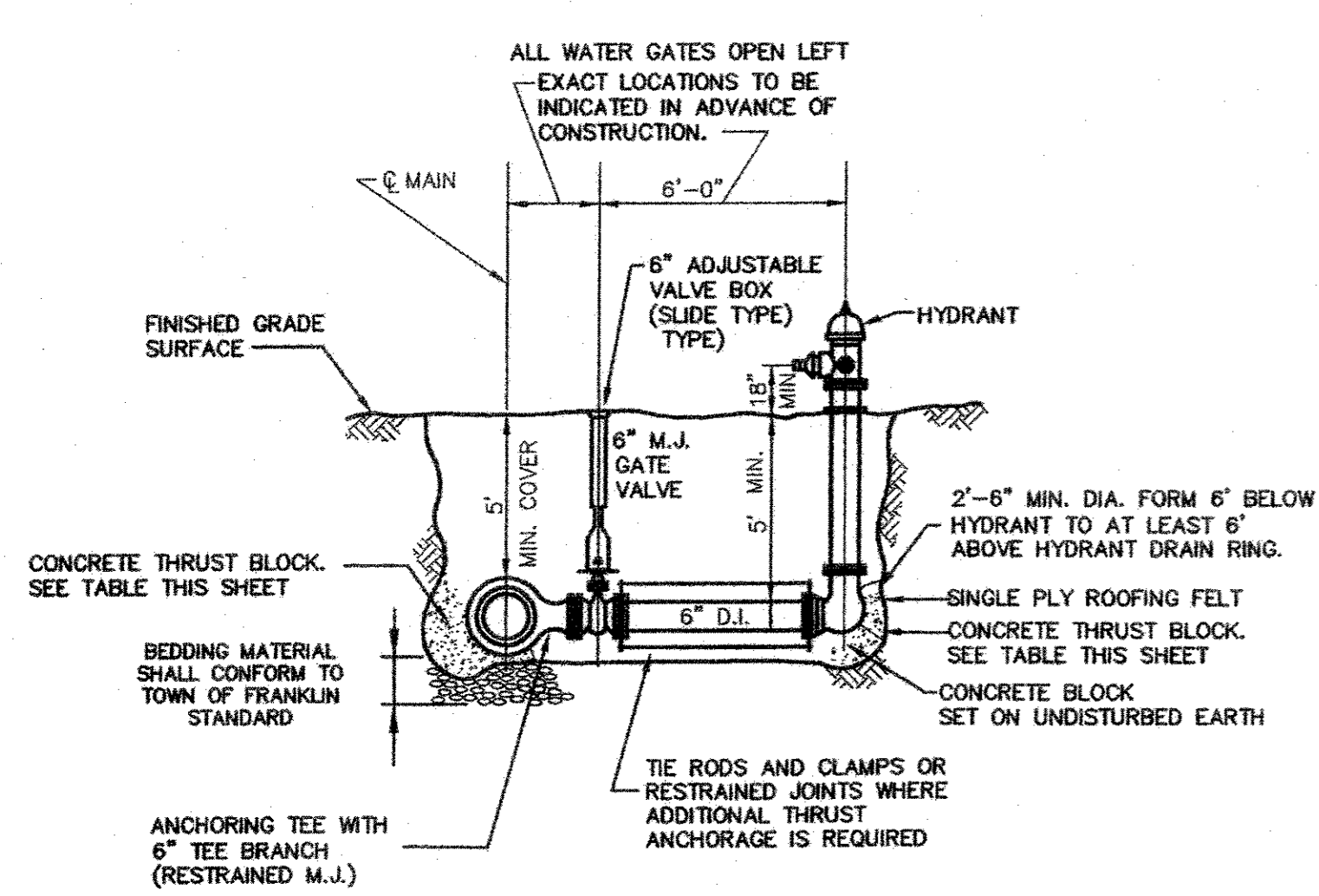
TYPICAL DRAIN TRENCH DETAIL
NOT TO SCALE



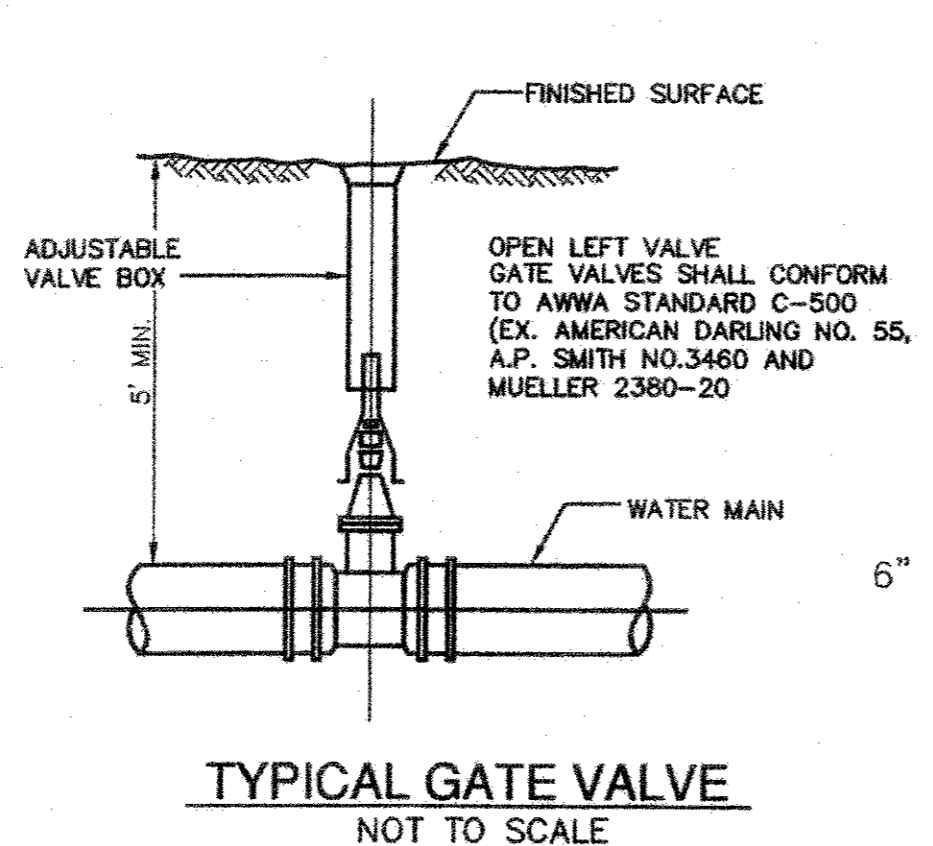
Paul E. Truax
PAUL E. TRUAX P.E. 12-10-09
DATE

DETAILS

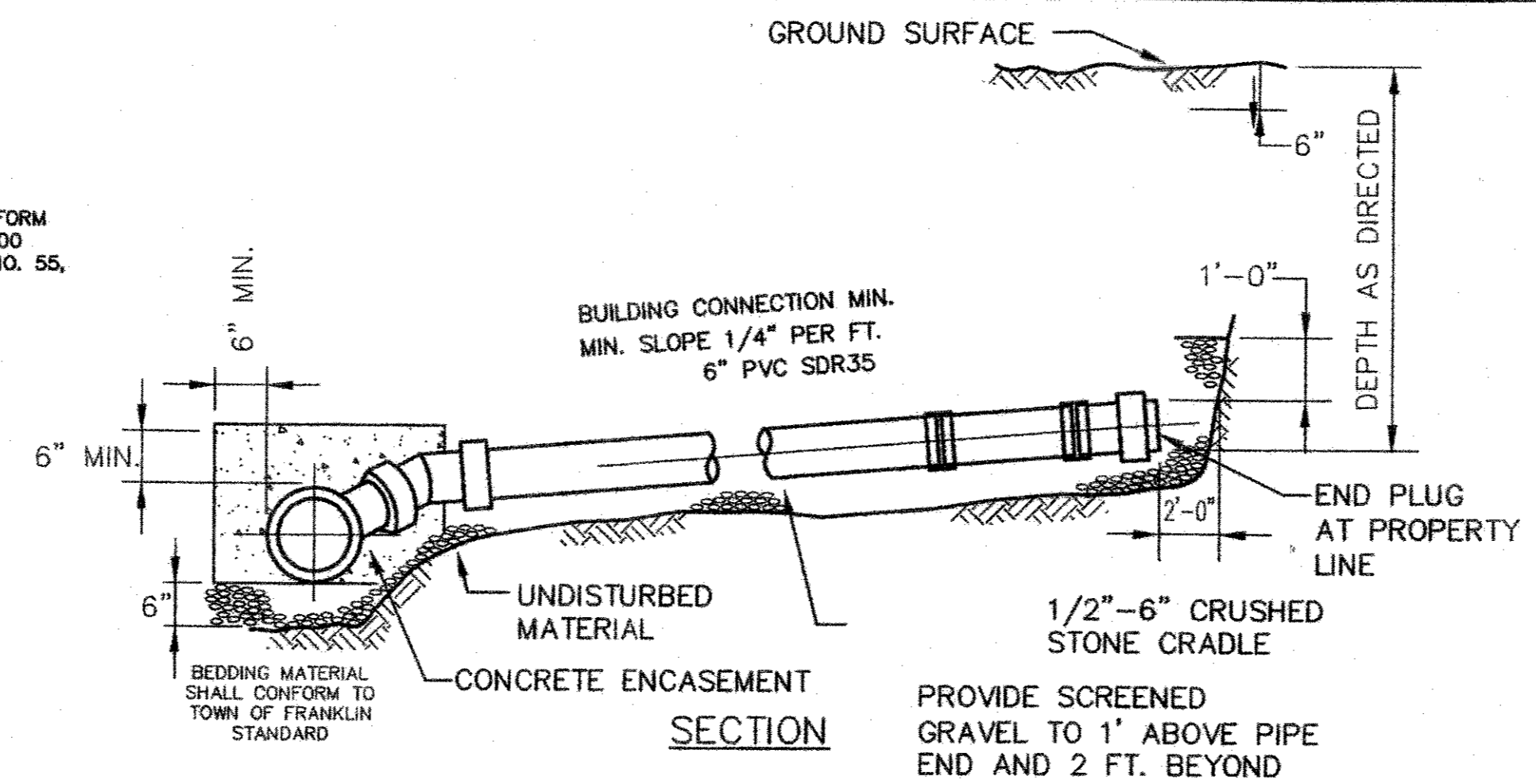
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PREPARED BY: GLM ENGINEERING CONSULTANTS, INC. 19 EXCHANGE STREET HOLLISTON, MASSACHUSETTS 01746 (508)429-1100 fax:(508)429-7160				NO. DATE DESCRIPTION
		1 9/22/08 PRIVATE WAY W/WAIVERS	DATE: JUNE 26, 2008	
		2 7/21/09 RESPONSE TO COMMENTS	SHEET No. 5 OF 6	
		3 8/04/09 RESPONSE TO COMMENTS		
		4 8/11/09 CONDITION OF APPROVAL		



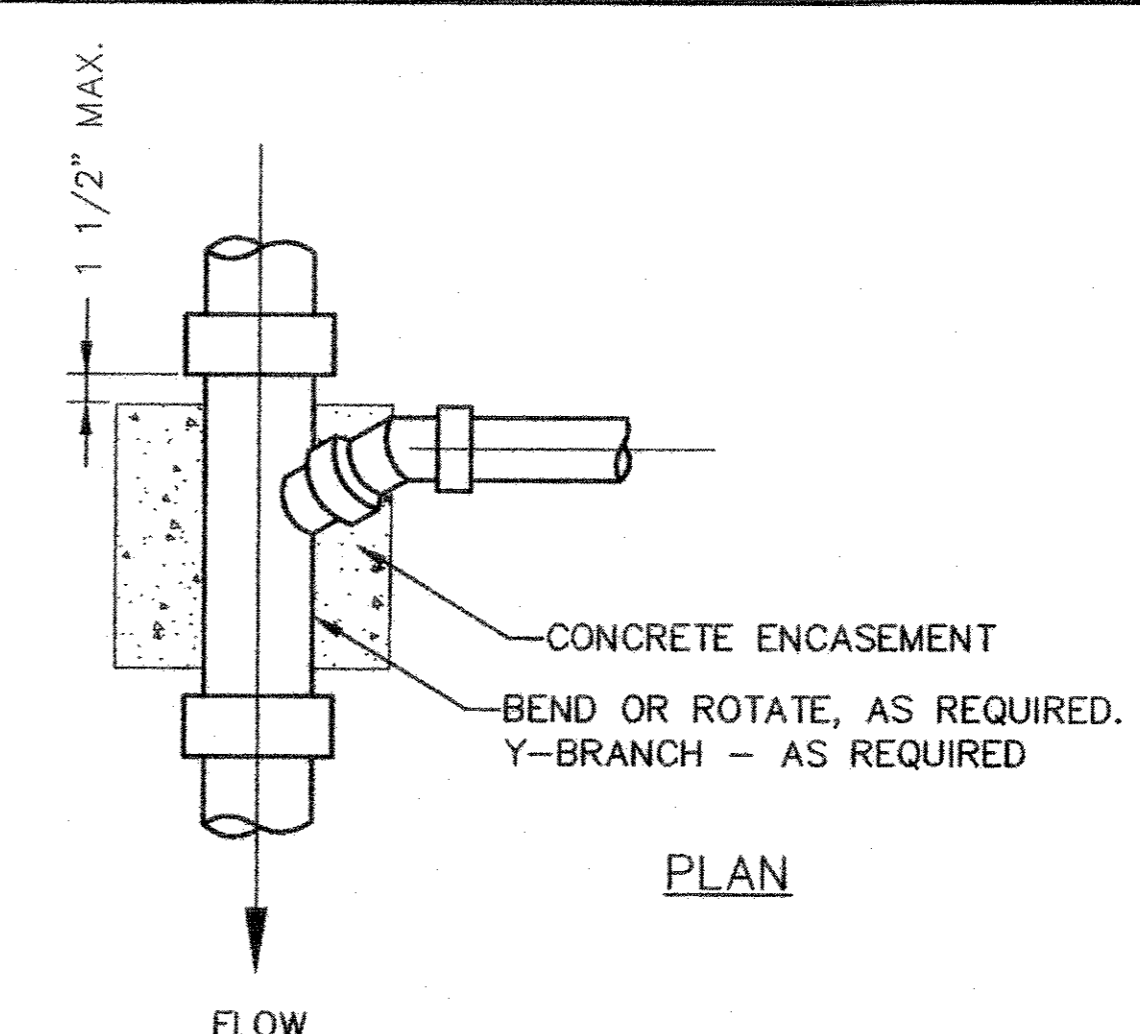
TYPICAL HYDRANT ASSEMBLY DETAIL
NOT TO SCALE



TYPICAL GATE VALVE
NOT TO SCALE



TYPICAL BUILDING CONNECTION
NOT TO SCALE



PLAN

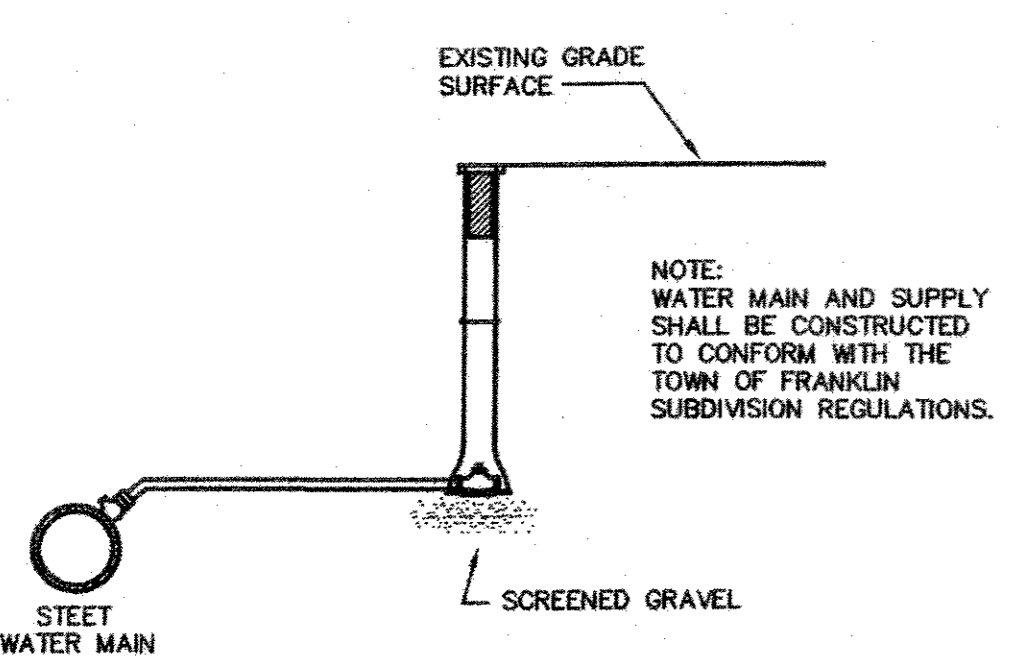
APPROVED BY
FRANKLIN PLANNING BOARD:
Paul E. Truax

APPROVED BY: _____
ENDORSED BY: _____

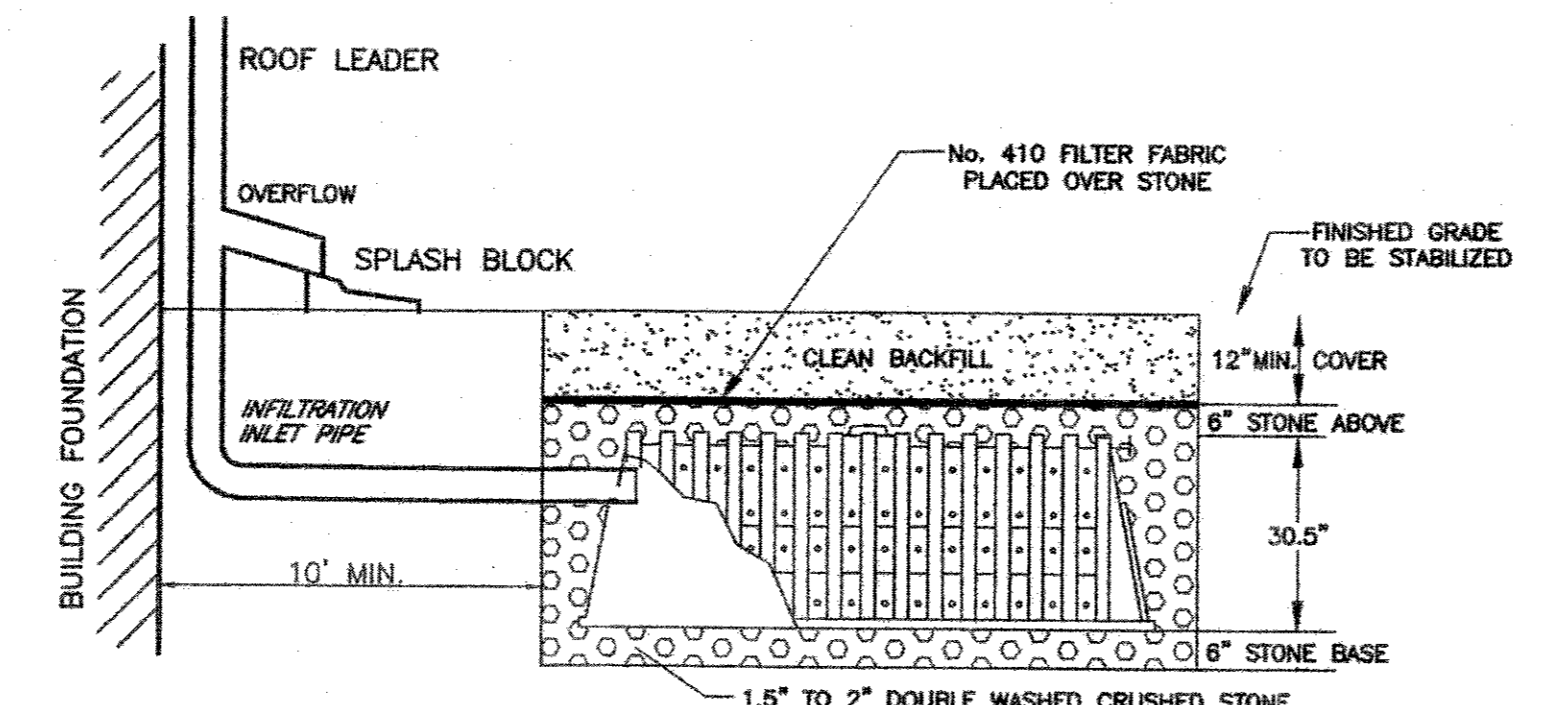
APPROVED 12/21/09 SUBJECT TO COVENANT
CONDITIONS SET FORTH IN A COVENANT EXECUTED
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NEXT AFTER RECEIPT AND RECORDING OF SAME.

TOWN CLERK _____ DATE _____

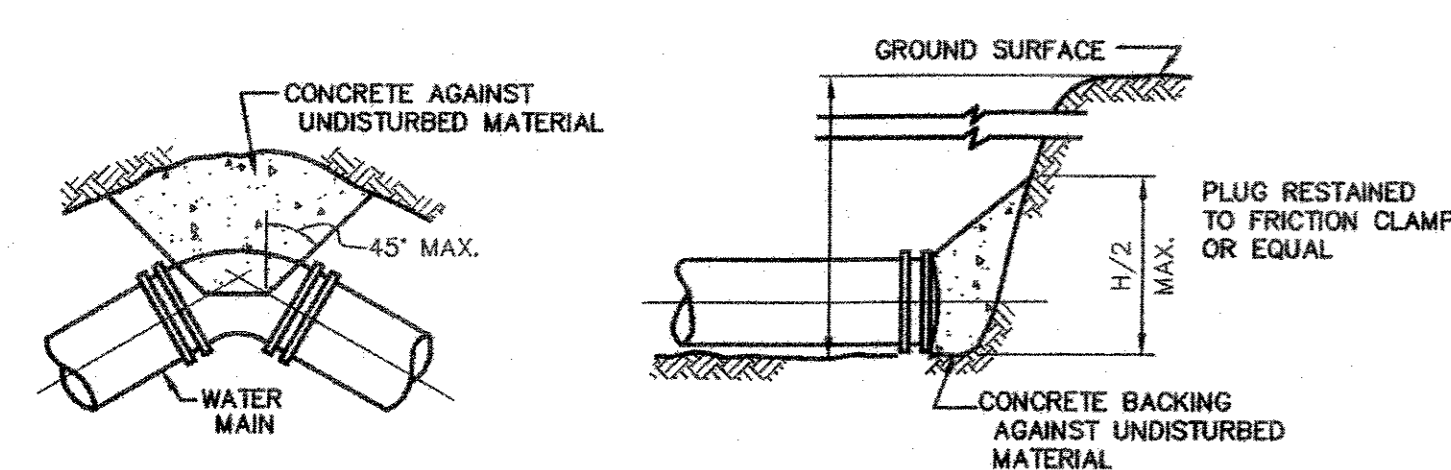


TYPICAL NEW WATER SERVICE CONNECTION DETAIL
NOT TO SCALE



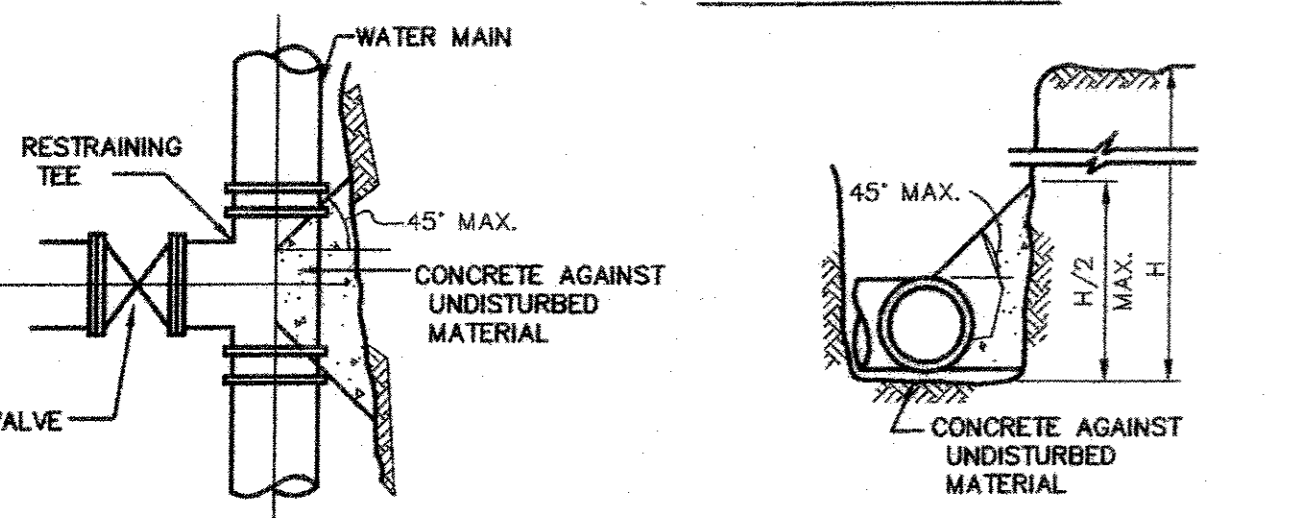
(TOTAL LENGTH = 27' WIDTH = 6.33')
EACH DWELLING SHALL HAVE A MINIMUM OF 4 CHAMBERS
ALL ROOF AREAS SHALL BE DIRECTED INTO THE LEACHING SYSTEM.

ROOF DRAIN LEACHING SYSTEM DETAIL
(NOT TO SCALE)



TYPICAL BEND

TYPICAL PLUG



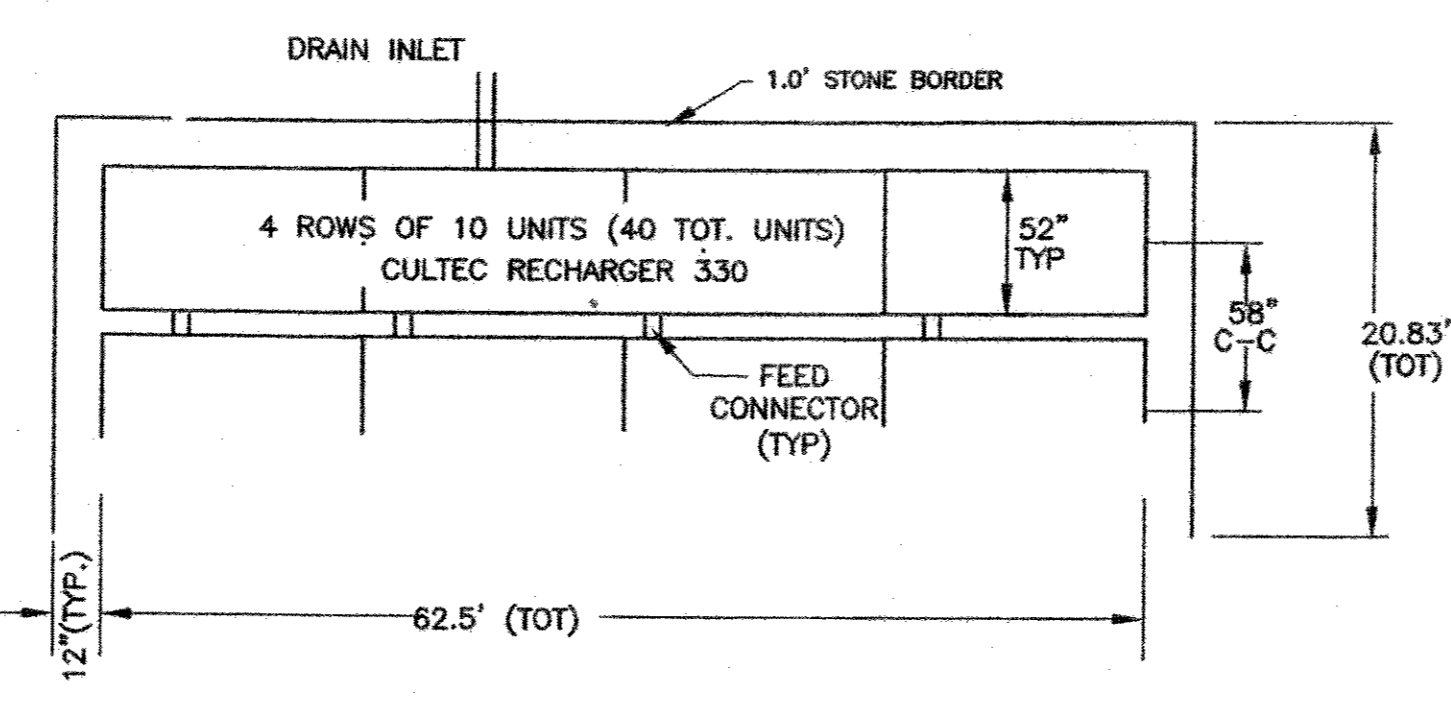
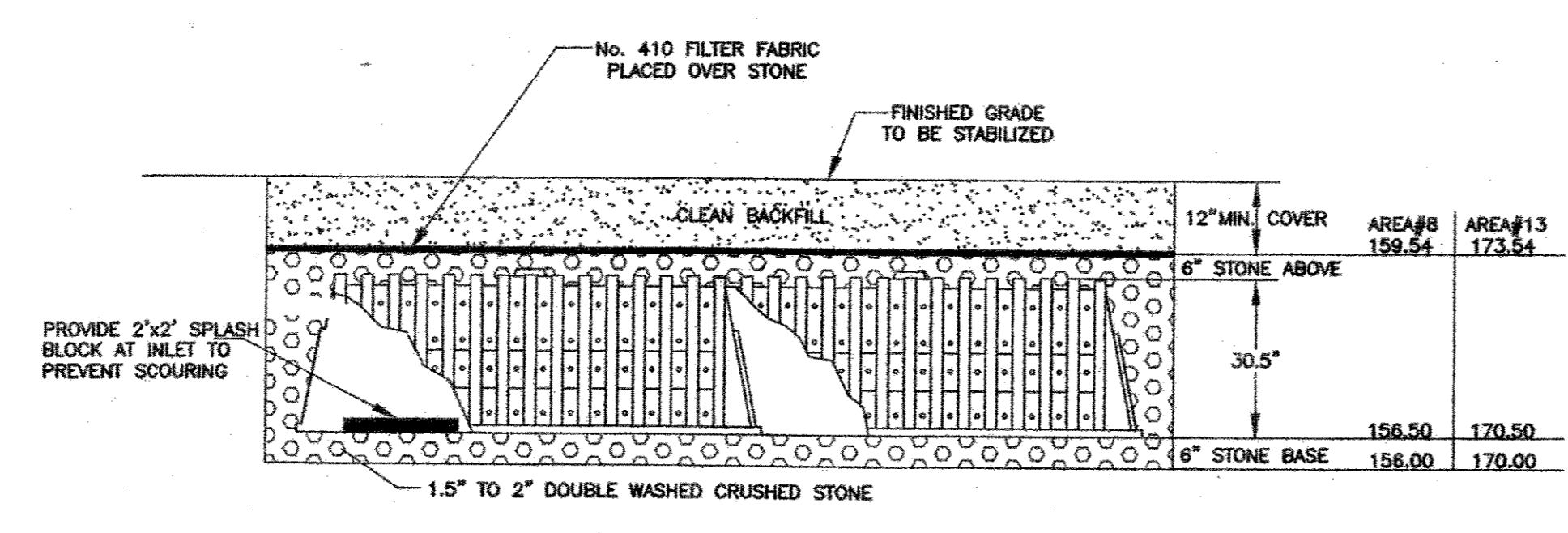
TYPICAL TEE

TYPICAL SECTION

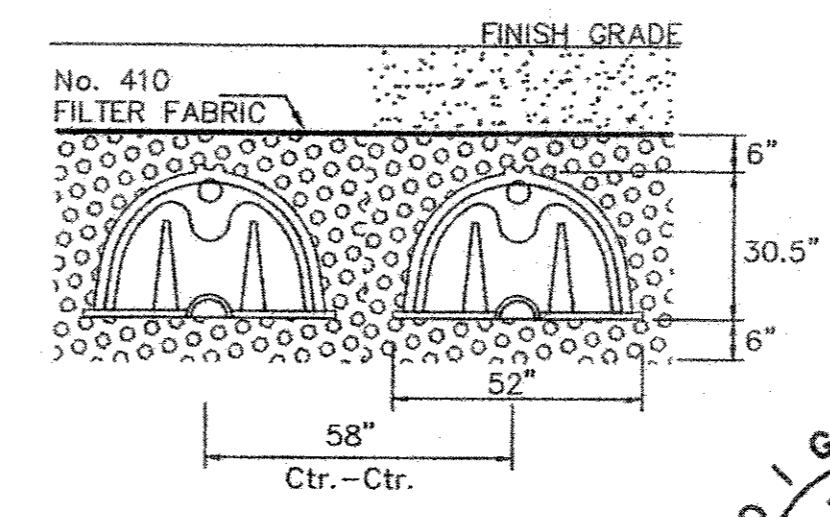
NOTE: CONCRETE FOR THRUST BLOCKS SHALL BE NO LONGER THAN THE RATIO OF 2 1/2 : 5 1/2 AND SHALL HAVE A MINIMUM COMPRESSION STRENGTH OF 2000 PSI (SO THAT FLANGES AND BOLTS ARE ACCESSIBLE.)

BEARING AREAS OF THRUST BLOCKS (BEARING AREA IN SQUARE FT.)				
PIPE SIZE INCHES	1/4 BEND	1/8 BEND	1/16 BEND OR LESS	PLUG TEES
6 AND 8	8	8	8	8
10 AND 12	22	13	8	16

TYPICAL THRUST BLOCK DETAIL
NOT TO SCALE



CULTEC RECHARGER 330 LEACHING CHAMBER DETAIL - AREAS #8 & #13
N.T.S.



NOTE:
IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION.



Paul E. Truax
PAUL E. TRUAX P.E. 12-10-09
DATE

DETAILS

OWNER / APPLICANT: BLUEVIEW CONSTRUCTION INC. 43 PINE STREET FRANKLIN, MA 02038		DEFINITIVE SUBDIVISION PLAN "LAKEVIEW TERRACE" FRANKLIN, MASSACHUSETTS																			
PREPARED BY: GLM ENGINEERING CONSULTANTS, INC. 19 EXCHANGE STREET HOLLISTON, MASSACHUSETTS 01746 (508)429-1100 fax:(508)429-7180	<table border="1"> <thead> <tr> <th colspan="3">REVISIONS</th> </tr> <tr> <th>NO.</th> <th>DATE</th> <th>DESCRIPTION</th> </tr> </thead> <tbody> <tr> <td>1</td> <td>9/22/08</td> <td>PRIVATE WAY W/WAIVERS</td> </tr> <tr> <td>2</td> <td>7/21/09</td> <td>RESPONSE TO COMMENTS</td> </tr> <tr> <td>3</td> <td>8/04/09</td> <td>RESPONSE TO COMMENTS</td> </tr> <tr> <td>4</td> <td>8/11/09</td> <td>CONDITION OF APPROVAL</td> </tr> </tbody> </table>		REVISIONS			NO.	DATE	DESCRIPTION	1	9/22/08	PRIVATE WAY W/WAIVERS	2	7/21/09	RESPONSE TO COMMENTS	3	8/04/09	RESPONSE TO COMMENTS	4	8/11/09	CONDITION OF APPROVAL	JOB No. 13,700 SCALE: 1" = 40' DATE: JUNE 26, 2008 SHEET No. 6 OF 6
REVISIONS																					
NO.	DATE	DESCRIPTION																			
1	9/22/08	PRIVATE WAY W/WAIVERS																			
2	7/21/09	RESPONSE TO COMMENTS																			
3	8/04/09	RESPONSE TO COMMENTS																			
4	8/11/09	CONDITION OF APPROVAL																			

SITE PLAN OF LAND

FORM H
ENGINEER'S CERTIFICATE OF COMPLETION
(to be executed by developer's engineer)

Site Plan known as "Site Plan Modification for Franklin Innovation Center at 31 Hayward Street, Franklin, MA"

I hereby certify that all improvements required for the above referenced site plan have been completed in all respects in accordance with the Town of Franklin zoning requirements and the approved plans entitled *** prepared by Guerriere & Halnon, Inc. and dated July 5, 2017, as approved by the said Planning Board on December 18, 2017

***Site Plan Modification for Franklin Innovation Center at 31 Hayward Street, Franklin, MA

Signed this 17th day of September, 2020

By [Signature] Reg. C.E.

COMMONWEALTH OF MASSACHUSETTS

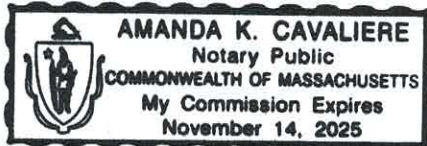
Norfolk, SS.

September 17, 2020

On this 17th day of September 2020, before me, the undersigned notary public, personally appeared Elizabeth-Maurini Sordiboni (name of engineer), proved to me through satisfactory evidence of identification, which were MA License to be the person whose name is signed on the preceding document in my presence.

Amanda K. Cavaliere (Official signature and seal of notary)

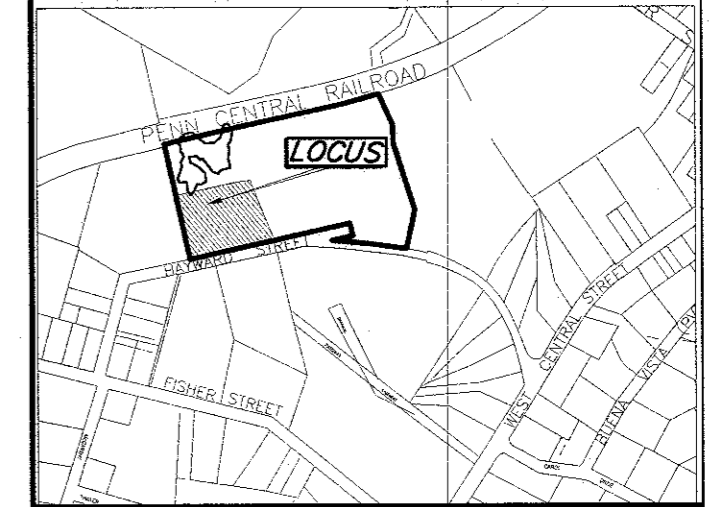
Notary Public: My Commission Expires: _____



PL.BK. 341 NO. 1131 OF 1986

NOTES

1. THIS PLAN REFERS TO THE TOWN OF FRANKLIN ASSESSORS MAP 278 / LOT 024.
2. THIS SITE DOES NOT FALL WITHIN A SPECIAL FLOOD HAZARD ZONE PER FEMA FLOOD MAP 25021C0308E, EFFECTIVE DATE 07/17/2012.
3. THIS PLAN IS PREPARED FOR PURPOSES OF COMPLYING WITH THE ORDER OF CONDITIONS MASSDEP FILE # CE 159-1160.

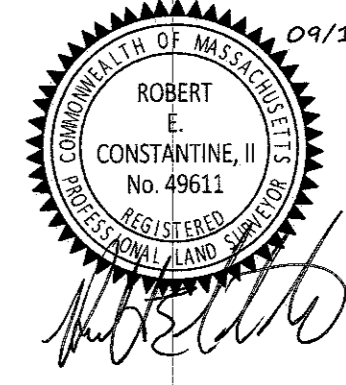


JOB NO. **F2640**

VICINITY MAP

(NOT TO SCALE)

"I certify that this plan was prepared from an on the ground survey and that the improvements are located on the lot as shown hereon."



Professional Land Surveyor

OWNER

MOSELEY REALTY, LLC
P.O. BOX 645
31 HAYWARD STREET
FRANKLIN, MA 02038

LEGAL REFERENCE

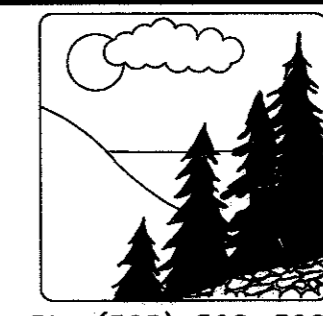
ASSESSORS MAP 278 / LOT 024
DEED BOOK 14105 / PAGE 467

PLAN REFERENCE

PLAN 1131 OF 1986 / PLAN BOOK 341

REVISIONS

DATE	REVISED
09.15.2020	UPDATED AS-BUILT CONDITIONS.

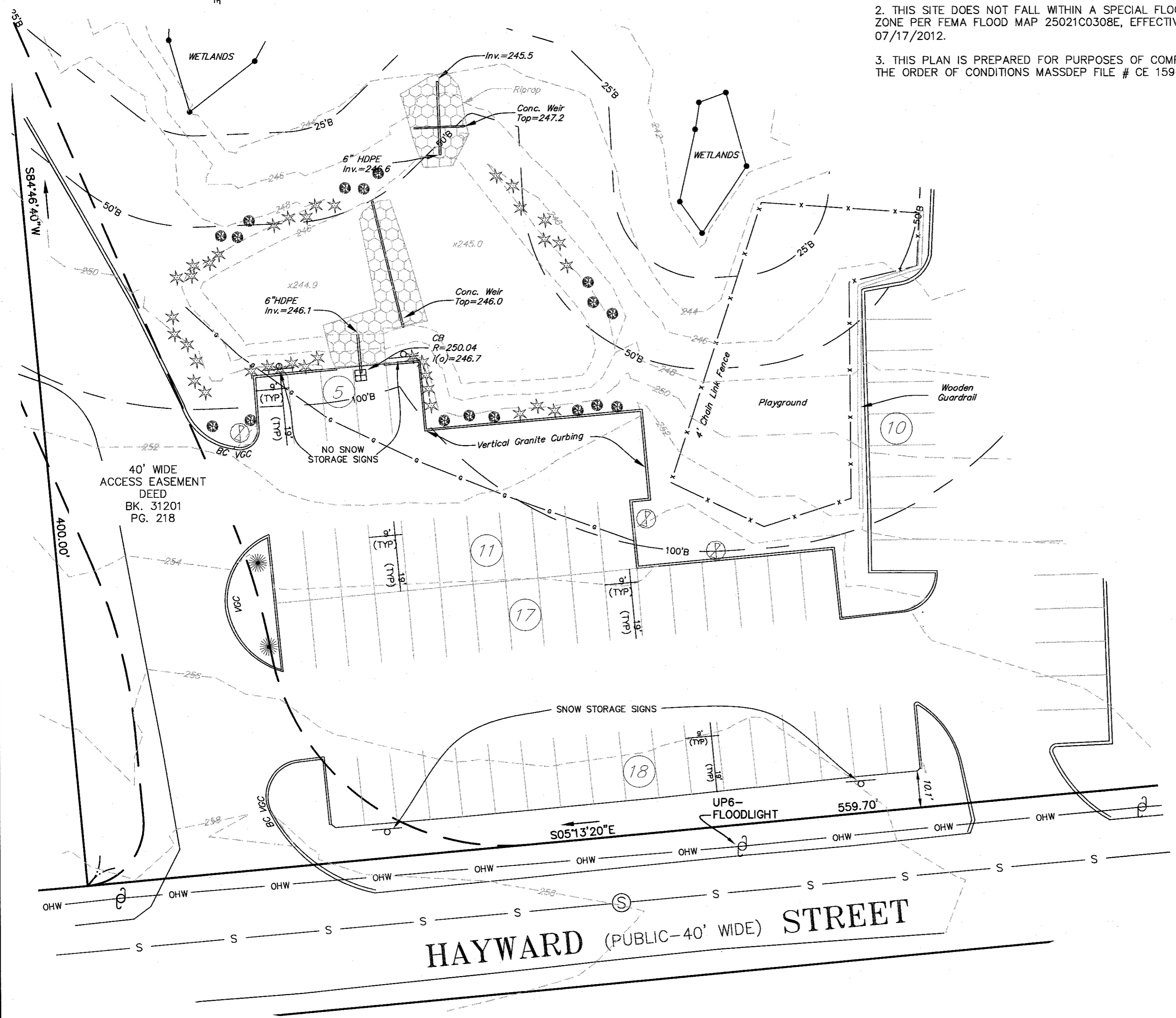


Guerriere & Halnon, Inc.

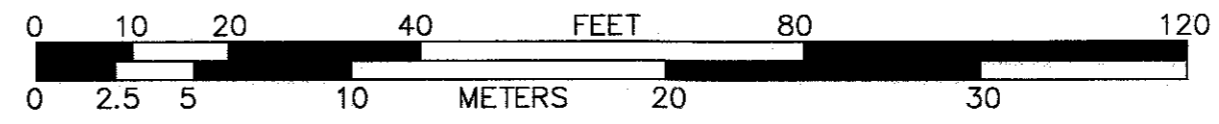
Engineering & Land Surveying
55 WEST CENTRAL STREET
FRANKLIN, MA 02038
Ph. (508) 528-3221
Fx. (508) 528-7921
www.gandhengineering.com

**AS-BUILT
PLOT PLAN
31 HAYWARD STREET
FRANKLIN
MASSACHUSETTS**

DATE SEPTEMBER 2, 2020	SCALE 1"=20'
SHEET 1 OF 1	JOB NO. F2640



CONSTRUCTION ON THIS LAND IS SUBJECT TO ANY EASEMENTS, RIGHTS-OF-WAY, RESTRICTIONS, RESERVATIONS, OR OTHER LIMITATIONS WHICH MAY BE REVEALED BY AN EXAMINATION OF THE TITLE.



c:\f2640\dwg\F2640-AB-01.dwg, 9/15/2020 11:25:13 AM, [REC]



TOWN OF FRANKLIN - SITE OBSERVATION REPORT

31 Hayward Street

Report No.:	4831 733-03	Date:	September 22, 2020	Arrive:	11:00 AM
Observer:	Matt Crowley, PE	Weather:	Sunny ~60°	Leave:	11:30 PM
Owner:	Moseley Realty Trust, LLC 31 Hayward Street Franklin, MA 02038	Contractor:	PJ Hayes, Inc 435 West Street Walpole, MA 02081		

Items Observed: **Conformance Observation – Submitted in conjunction with Applicant’s request for acceptance of Form H – Certificate of Completion**

OBSERVATIONS

Observation Requested By: Amanda Cavaliere – Guerriere and Halnon

Met/walked site with: N/A

Current Activity on Site: No current activity

Observed Construction: BETA arrived on site to perform a construction observation in conjunction with the Applicant’s request for acceptance of Form H – Certificate of Completion. The required Form H, dated September 17, 2020, and as-built plan, dated September 2, 2020 and revised September 15, 2020, were provided via email. BETA’s site walk and review of the Approved Plans confirmed the site to be constructed in general conformance with the Approved Plans with the following exceptions/notations:

- A flood light has been installed on Utility Pole 6 in place of the proposed ground mounted flood light between the new parking area and Hayward Street. It is anticipated that the installed light will provide similar lighting levels throughout the parking area for safety and security.
- Although not depicted on the approved plans, there is no barrier between two parking spaces and the adjacent infiltration basin. In consideration of grading of the parking area the designer should consider adding a barrier (e.g. guardrail or car stops) at this location.

Photos Attached:

SITE PHOTOS



Constructed parking area



Area of consideration for guardrail or car stop installation



Infiltration basin



Flood light installation on Utility Pole 6



FRANKLIN PLANNING & COMMUNITY DEVELOPMENT

355 EAST CENTRAL STREET
FRANKLIN, MA 02038-1352
TELEPHONE: 508-520-4907
FAX: 508-520-4906

MEMORANDUM

DATE: September 23, 2020
TO: Franklin Planning Board
FROM: Department of Planning and Community Development
RE: 31 Hayward Street – Expanded Parking Area
Final Form H

General

1. The Planning Board approved a Site Plan Modification on November 13, 2017. The project entailed reconfiguring and expanding one parking lot that created an additional 22 parking spaces. In addition, stormwater management and landscaping were constructed with the additional parking spaces.
2. The endorsed Site Plan Modification is included in this packet.
3. The applicant has submitted a Final Form H and Engineer's Certificate of Completion along with an as-built plan.
4. BETA has provided an observation report.

Comments

DPCD has no further comments.

FRANKLIN INNOVATION CENTER

SITE PLAN MODIFICATION

ADDITIONAL SOUTHEAST PARKING

AT

31 HAYWARD STREET

IN

FRANKLIN,

MASSACHUSETTS

TOWN OF FRANKLIN
TOWN CLERK
2017 DEC 21 AM 11:08

RECEIVED

FILE COPY

RECEIVED
DEC 21 2017
PLANNING DEPT
JOB NO. F2640

APPROVED DATE:
FRANKLIN PLANNING BOARD
John Conroy
William...
Guy Rankin
DATE: 12/18/17
BEING A MAJORITY

DRAWING INDEX

SHEET	LAST REVISION	COMMENTS
1.	COVER SHEET	10-21-2017 REVISIONS PER TOWN COMMENTS
2.	EXISTING CONDITIONS	10-21-2017 REVISIONS PER TOWN COMMENTS
3.	LAYOUT PLAN	10-21-2017 REVISIONS PER TOWN COMMENTS
4.	EROSION CONTROLS	10-21-2017 REVISIONS PER TOWN COMMENTS
5.	GRADING AND DRAINAGE	10-21-2017 REVISIONS PER TOWN COMMENTS
6.	DRAINAGE DETAILS	10-21-2017 REVISIONS PER TOWN COMMENTS
7.	SITE DETAILS	10-21-2017 REVISIONS PER TOWN COMMENTS

CERTIFICATE OF VOTE

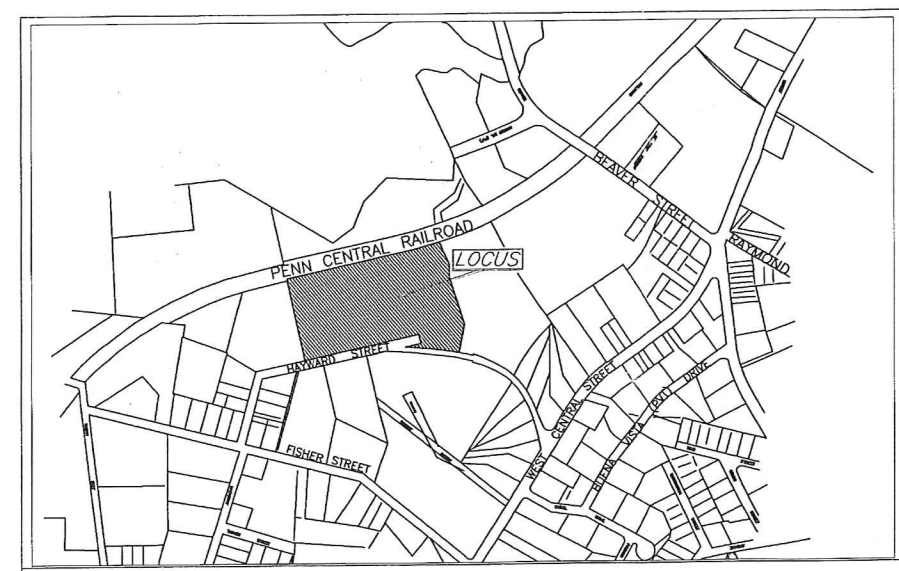
SEE CERTIFICATE OF VOTE FOR PLAN ENTITLED 'FRANKLIN INNOVATION CENTER SITE PLAN MODIFICATION ADDITIONAL SOUTHEAST PARKING AT 31 HAYWARD STREET' BY GUERRIERE AND HALNON, INC. FROM THE FRANKLIN PLANNING BOARD VOTING TO APPROVE WITH CONDITIONS AT THE MEETING ON NOVEMBER 13, 2017 CONDITIONS LISTED BELOW.

CERTIFICATE OF VOTE

Site Plan Modification

31 Hayward Street - Franklin Innovation Center

- The Planning Board will use outside consultant services to complete construction inspections upon the commencement of construction. The Franklin Department of Public Works Director, directly and through employees of the Department of Public Works and outside consultant services shall act as the Planning Board's inspector to assist the Board with inspections necessary to ensure compliance with all relevant laws, regulations and Planning Board approved plan specifications. Such consultants shall be selected and retained upon a majority vote of the Board.
- Actual and reasonable costs of inspection consulting services shall be paid by the owner/applicant before or at the time of the pre-construction meeting. Should additional inspections be required beyond the original scope of work, the owner/applicant shall be required to submit fees prior to the issuance of a Final Certificate of Completion by the Planning Board (Form H). Said inspection is further outlined in condition #1.
- No alteration of these plans shall be made or affected other than by an affirmative vote of the members of the Board at a duly posted meeting and upon the issuance of a written amended decision.
- All applicable laws, by-laws, rules, regulations, and codes shall be complied with, and all necessary licenses, permits and approvals shall be obtained by the owner/applicant.
- Prior to the endorsement of the site plan, the following shall be done:
 - The owner/applicant shall make a notation on the plan that references the conditions and dates of this Certificate of Vote.
 - A notation shall be made on the plans that all erosion mitigation measures shall be in place prior to major construction or soil disturbance commencing on the site.
 - All outstanding invoices for services rendered by the Town's Engineers and other reviewing Departments of the Town relative to their review of the owner/applicant's application and plans shall have been paid in full.
 - The owner/applicant shall submit a minimum of six copies of the approved version of the plan.
- All required improvements specified in this Certificate of Vote shall be constructed within a one-year period unless the Board grants an extension. No final Certificate of Occupancy shall be issued until all requirements of the approved plan have been completed to the satisfaction of the Board unless the applicant has submitted a Partial Certificate of Completion for the remainder of the required improvements. The applicant's engineer or surveyor, upon completion of all required improvements, shall submit a Certificate of Completion. The Board or its agent(s) shall complete a final inspection of the site upon filing of the Certificate of Completion by the applicant. Said inspection is further outlined in condition #1.
- Prior to any work commencing on the subject property, the owner/applicant shall provide plans to limit construction debris and materials on the site. In the event that debris is carried onto any public way, the owner/applicant and his assigns shall be responsible for all cleanup of the roadway. All cleanups shall occur within twenty-four (24) hours after first written notification to the owner/applicant by the Board or its designee. Failure to complete such cleanup may result in suspension of construction of the site until such public way is clear of debris.
- The owner/applicant shall install erosion control devices as necessary and as directed by the Town's Construction Inspector.
- Maintenance and repair of the parking area, water supply system, sewer pipes, and stormwater system shall be the responsibility of the owner/applicant and shall never be the responsibility of the Town and the Town shall never be required to perform any service, repair or maintenance with respect to said areas, or any of the aforementioned systems within the subject property.
- Prior to construction activities, there shall be a pre-construction meeting with the owner/applicant, and his contractor(s), the Department of Public Works and the Planning Board's Inspector.
- Bollards or Vertical Granite curbing shall be installed along the parking area that abuts the Playground.



LOCUS MAP
(1"=400')

JULY 5, 2017

OWNER/APPLICANT

MOSLEY REALTY, LLC
P.O. BOX 645
31 HAYWARD STREET
FRANKLIN, MA 02038

TENANTS AS OF THE DATE OF THIS PLAN:

- O.R. Colan Associates
- New England CM, Inc. (Office)
- Ten Key Accounting
- Fenton Financial
- Hills Pension Associates
- Marc H Clough, LMHC
- The Bench Group
- HFA Allevato
- McSharry Entr LLC dba Closet & Storage Concepts
- Law Office of Barbara Levine Kravetz
- Pilates by Gwen
- RZ Design Associates
- JBT-Jerue Logistics
- RSS Industries, Inc.
- Vicario Studio
- Steve Light Counseling
- Therapy Connections
- Sanctuary Spas Corporation
- Carol Hetherington dba Blue Heron Acupuncture
- Dahab Associates
- The Moseley Corporation
- Interactions Corp
- Next Level
- Anglican Church
- Dobritte Norris
- Dennis O'Brien
- New England CM, Inc.
- Summit Int'l Medical Technologies
- Xpression Prints Inc.
- Abbey Investments LLC
- Bioactive Labs, Inc.
- Berry Insurance Agency
- Sunrise Montessori School Inc.



PROFESSIONAL LAND SURVEYOR
PROFESSIONAL ENGINEER

OWNER(S):

PARCEL "A"
MOSELEY REALTY, LLC
31 HAYWARD STREET
FRANKLIN, MA 02038
DEED BK. 14105 PG. 467
PARCEL B-1
MOSELEY REALTY, LLC
31 HAYWARD STREET
FRANKLIN, MA 02038
DEED BK. 33147 PG. 571

REVISIONS

DATE	REVISED
9-18-2017	DRAWING INDEX UPDATED
10-21-2017	REVISIONS PER TOWN COMMENTS
11/14/17	ADDED CERTIFICATE OF VOTE

Guerriere & Halnon, Inc.
Engineering & Land Surveying
Ph. (508) 528-3221 55 WEST CENTRAL STREET
Fr. (508) 528-7921 FRANKLIN, MA 02038
www.guerriereandhalnon.com

COVER SHEET

SITE PLAN MODIFICATION
ADDITIONAL SE PARKING
AT 31 HAYWARD STREET
FRANKLIN
MASSACHUSETTS

DATE	JULY 5, 2017	SCALE	1"=40'
SHEET	1 OF 7	JOB NO.	F2640

INDUSTRIAL
 FRANKLIN ZONING BYLAW SECTION 185
 ATTACHMENT 9: LAST AMENDED
 1-22-2014 BY AMENDMENT 13-726
 MINIMUM LOT AREA 40,000 SF
 MINIMUM LOT FRONTAGE 175'
 MINIMUM LOT DEPTH 200'
 MINIMUM LOT WIDTH 157.5'
 MINIMUM YARDS
 FRONT 40'_s
 SIDE 30'_s
 REAR 30'_s
 INCREASE BY THE COMMON BUILDING HEIGHT
 OF THE STRUCTURE, WHEN ABUTTING A
 RESIDENTIAL DISTRICT OR USE.

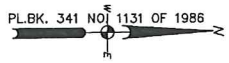
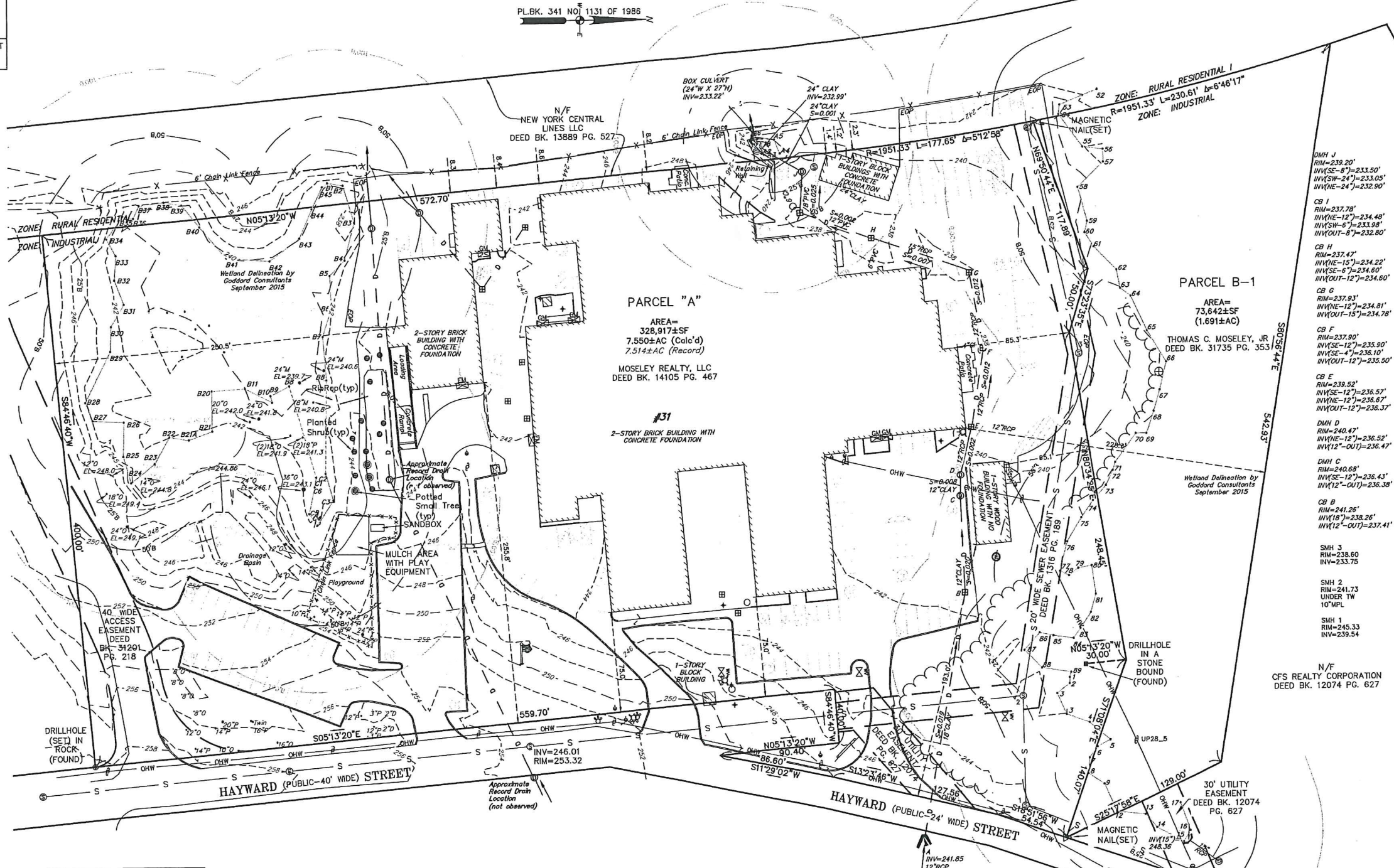
LEGAL REFERENCE
 ASSESSORS MAP: 278 / LOT 024
 ASSESSORS MAP: 278 / LOT 024.1

PLAN REFERENCE
 PLANS RECORDED AT THE NORFOLK
 COUNTY REGISTRY OF DEEDS
 PLAN NO. 880-1984 PLAN BK. 313
 PLAN NO. 1131-1986 PLAN BK. 341

ZONING NOTE
 THIS LAND IS ZONED INDUSTRIAL.

FLOOD HAZARD NOTE
 THIS SITE DOES NOT FALL WITHIN A
 SPECIAL FLOOD HAZARD ZONE PER FEMA
 FLOOD MAP 25021C0308E, EFFECTIVE
 DATE 07/17/2012.

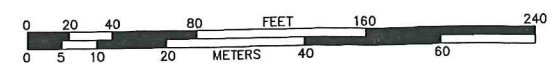
SPECIAL NOTES:
 1. WETLAND LOCATIONS SHOWN HEREON
 ARE BASED ON A FIELD SURVEY BY
 GUERRIERE & HALNON, INC IN OCTOBER
 2015. WETLAND WAS DELINEATED BY
 GODDARD CONSULTANTS IN SEPTEMBER
 2015.
 2. PARKING COUNT REFLECTS PAINTED
 SPACES. NO OBSERVATION IS MADE AS
 TO FUNCTIONALITY OR COMPLIANCE WITH
 TOWN OF FRANKLIN ZONING
 REGULATIONS.



LEGEND	
—	EDGE OF PAVEMENT
⊗	WATER VALVE
⊕	FIRE HYDRANT
⊙	WELL
+	POST INDICATOR VALVE
⊠	WETLAND (SEE NOTE)
⊞	ELECTRIC METER
⊚	SEWER MANHOLE
⊛	DRAIN MANHOLE
⊜	CATCH BASIN
⊝	MANHOLE (UNKNOWN TYPE)
⊞	GAS SHUT OFF
⊞	GAS METER
⊞	UTILITY POLE
⊞	GUY WIRE
⊞	PAINTED PARKING SPACE COUNT

UTILITIES ARE PLOTTED AS A COMPILATION OF RECORD DOCUMENTS, MARKINGS, AND OTHER OBSERVED EVIDENCE TO DEVELOP A VIEW OF THE UNDERGROUND UTILITIES AND SHOULD BE CONSIDERED APPROXIMATE. LACKING EXCAVATION, THE EXACT LOCATION OF UNDERGROUND FEATURES CANNOT BE ACCURATELY, COMPLETELY AND RELIABLY DEPICTED. ADDITIONAL UTILITIES, NOT EVIDENCED BY RECORD DOCUMENTS OR OBSERVED PHYSICAL EVIDENCE, MAY EXIST. CONTRACTORS (IN ACCORDANCE WITH MASS.G.L. CHAPTER 92 SECTION 40 AS AMENDED) MUST CONTACT ALL UTILITY COMPANIES BEFORE EXCAVATING AND DRILLING AND CALL DIGSAFE-AT 1(888)DIGSAFE(7233).

CONSTRUCTION ON THIS LAND IS SUBJECT TO ANY EASEMENTS, RIGHTS-OF-WAY, RESTRICTIONS, RESERVATIONS, OR OTHER LIMITATIONS WHICH MAY BE REVEALED BY AN EXAMINATION OF THE TITLE.



JOB NO. F2640

APPROVED DATE:
 FRANKLIN PLANNING BOARD
John Small
William J. Small
Ray Rowe

DATE: 12/18/17
 BEING A MAJORITY



PROFESSIONAL LAND SURVEYOR
 PROFESSIONAL ENGINEER

OWNER(S):

PARCEL "A"
 MOSELEY REALTY, LLC
 31 HAYWARD STREET
 FRANKLIN, MA 02038
 DEED BK. 14105 PG. 467
PARCEL B-1
 MOSELEY REALTY, LLC
 31 HAYWARD STREET
 FRANKLIN, MA 02038
 DEED BK. 33147 PG. 571

REVISIONS

DATE	REVISION
10-21-2017	REVISIONS PER TOWN COMMENTS
11/14/17	ADDED CERTIFICATE OF VOTE

Guerriere & Halnon, Inc.
 Engineering & Land Surveying
 Ph. (508) 528-3221 55 WEST CENTRAL STREET
 Fx. (508) 528-7921 FRANKLIN, MA 02038
 www.guerriereandhalnon.com

EXISTING CONDITIONS

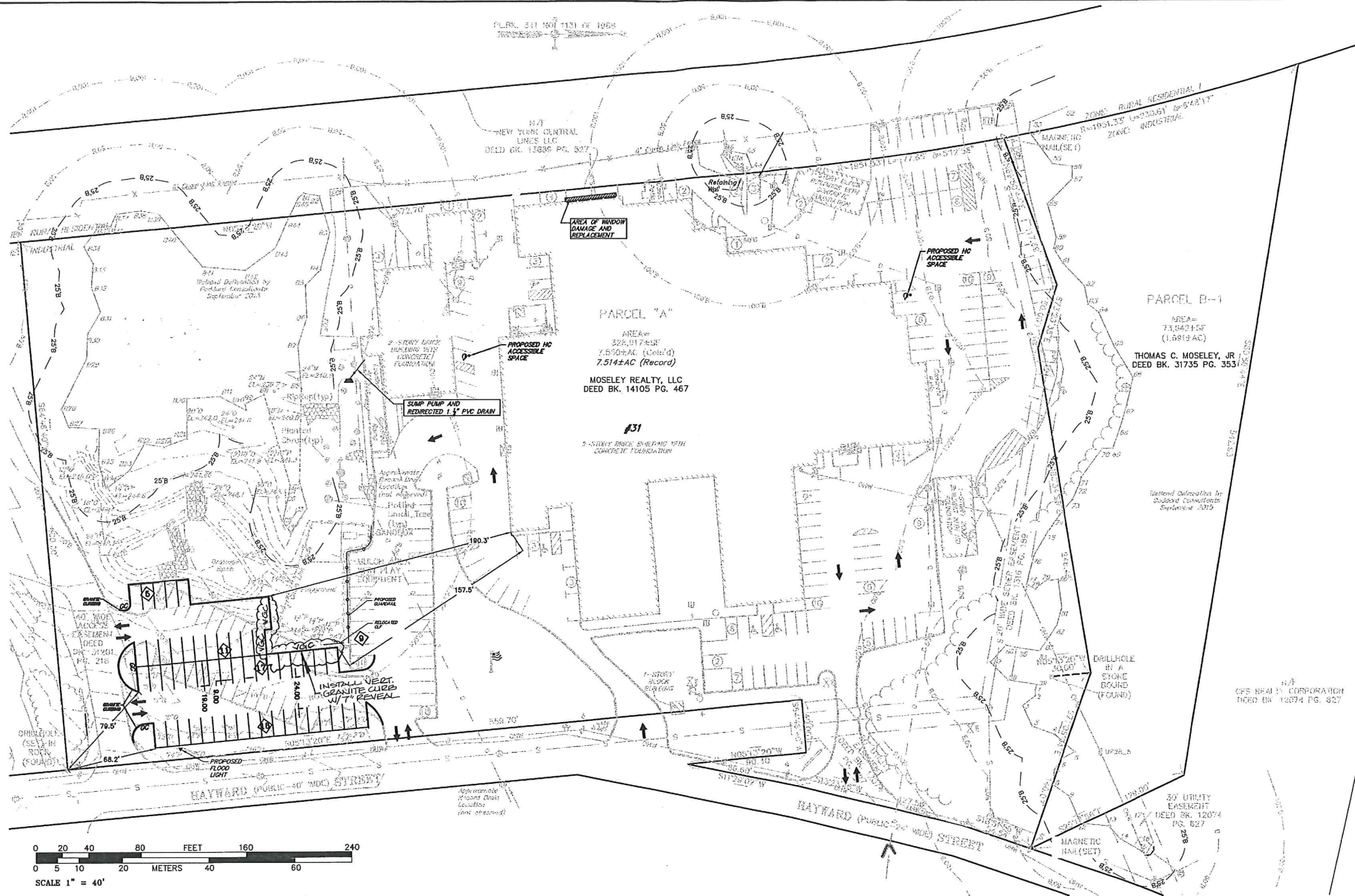
SITE PLAN MODIFICATION
 ADDITIONAL SE PARKING
 AT 31 HAYWARD STREET
 FRANKLIN
 MASSACHUSETTS

DATE JULY 5, 2017	SCALE 1"=40'
SHEET 2 OF 7	JOB NO. F2640

INDUSTRIAL
 FRANKLIN ZONING BYLAW SECTION 185
 ATTACHMENT 9; LAST AMENDED
 1-22-2014 BY AMENDMENT 13-726

MINIMUM LOT AREA 40,000 SF
 MINIMUM LOT FRONTAGE 175'
 MINIMUM LOT DEPTH 200'
 MINIMUM LOT WIDTH 157.5'
 MINIMUM YARDS
 FRONT 40'
 SIDE 30'
 REAR 30'

INCREASE BY THE COMMON BUILDING HEIGHT
 OF THE STRUCTURE, WHEN ABUTTING A
 RESIDENTIAL DISTRICT OR USE.



LEGEND

EQP	EDGE OF PAVEMENT
WV	WATER VALVE
FH	FIRE HYDRANT
W	WELL
PIV	POST INDICATOR VALVE
W	WETLAND (SEE NOTE)
EM	ELECTRIC METER
SM	SEWER MANHOLE
DM	DRAIN MANHOLE
CB	CATCH BASIN
M	MANHOLE (UNKNOWN TYPE)
GS	GAS SHUT OFF
GM	GAS METER
UP	UTILITY POLE
GW	GUY WIRE
PS	PAINTED PARKING SPACE COUNT

CONSTRUCTION ON THIS LAND IS SUBJECT TO ANY EASEMENTS, RIGHTS-OF-WAY, RESTRICTIONS, RESERVATIONS, OR OTHER LIMITATIONS WHICH MAY BE REVEALED BY AN EXAMINATION OF THE TITLE.

UTILITIES ARE PLOTTED AS A COMPILATION OF RECORD DOCUMENTS, MARKINGS AND OTHER OBSERVED EVIDENCE TO DEVELOP A VIEW OF THE UNDERGROUND UTILITIES AND SHOULD BE CONSIDERED APPROXIMATE. LACKING EXCAVATION, THE EXACT LOCATION OF UNDERGROUND FEATURES CANNOT BE ACCURATELY, COMPLETELY AND RELIABLY DEPICTED. ADDITIONAL UTILITIES, NOT EVIDENCED BY RECORD DOCUMENTS OR OBSERVED PHYSICAL EVIDENCE, MAY EXIST. CONTRACTORS (IN ACCORDANCE WITH MASS.G.L. CHAPTER 82 SECTION 40 AS AMENDED) MUST CONTACT ALL UTILITY COMPANIES BEFORE EXCAVATING AND DRILLING AND CALL DIGSAFE AT (888)DIGSAFE(7233).

CONSTRUCTION ON THIS LAND IS SUBJECT TO ANY EASEMENTS, RIGHTS-OF-WAY, RESTRICTIONS, RESERVATIONS, OR OTHER LIMITATIONS WHICH MAY BE REVEALED BY AN EXAMINATION OF THE TITLE.

PARKING APPROVALS AND REQUIREMENTS

EXISTING	PEAK USE PARKING REQUIRED	PROPOSED
221* SPACES**	302.23 (SEE PARKING COUNT TEL)	243 SPACES

PARKING NOTES:
 PROPOSED 51 PARKING SPACES, ACHIEVED THROUGH RE-STRIPPING AND NEW PAVEMENT, LOOSING 29 EXISTING SPACES IN THE PROCESS NET GAIN OF 22 ADDITIONAL 9' X 19' PARKING SPACES.

* 10 SPACES NOT INCLUDED IN EXISTING PARKING SPOT COUNT. MOSELEY REALTY LLC AND NEW YORK CENTRAL LINES LLC ARE UNDER NEGOTIATION TO PURCHASE / LEASE THE LAND FOR PARKING THAT EXTENDS BEYOND THE NORTH WEST PROPERTY LINE.

** EXISTING PARKING COUNT REFLECTS PAINTED SPACES. NO OBSERVATION IS MADE AS TO FUNCTIONALITY OR COMPLIANCE WITH TOWN OF FRANKLIN ZONING REGULATIONS.

DETERMINATION ON THE NUMBER OF PARKING SPACES IS REQUESTED UNDER SECTION 185-21.A.(4).

PAVEMENT CALCULATIONS
 TOTAL PAVEMENT TO BE ADDED DURING THE CONSTRUCTION OF THE ADDITIONAL PARKING = 6,185 SF.

CERTIFICATE OF COMPLIANCE REQUESTS
 APPLICANT IS NOTIFYING THE TOWN OF THE FOLLOWING IMPROVEMENTS COMPLETED AT THE REQUESTS OF THE CONSERVATION COMMISSION AND SEEKS A CERTIFICATE OF COMPLIANCE:

1. AN EMERGENCY WINDOW REPLACEMENT UNDERTAKEN IN THE WINTER OF 2015. FOUR WINDOWS DAMAGED DURING SNOW PLOW OPERATIONS WERE REMOVED, WINDOW WELLS FILLED IN, WINDOWS REPLACED AND A STRIP OF PAVEMENT 39.5' X 2.5' WAS REPLACED WITHIN THE 100 FT BUFFER ZONE.
2. A 1 1/2" SUMP PUMP OUTFALL REDIRECTED FROM THE WETLANDS TO A DRAINAGE DETENTION BASIN.

WAIVER
 APPLICANT IS SEEKING A WAIVER FROM PLANTING APPROVED ON 9/23/2013 AS PART OF PHASE III SITE MODIFICATION DUE TO UPCOMING CONSTRUCTION IN THE AREA.

IMPERVIOUS COVERAGE NOTES:

TOTAL AREA OF SITE	= 402,559 ±SF (PARCEL A AND PARCEL B-1)
TOTAL EXISTING IMPERVIOUS EXISTING CONDITIONS	= 193,220 ±SF (PAVEMENT AND ROOF AREAS) = 48% IMPERVIOUS
PROPOSED PAVEMENT POST DEVELOPMENT IMPERV. PROPOSED CONDITIONS	= 6,185 SF = 199,405 ±SF = 50% IMPERVIOUS

UPLAND AREAS NOTES:

WETLAND AREAS ON SITE	= 68,600 ±SF (ON PARCEL B-1)
TOTAL WETLAND ON PROPERTY	= 21,225 ±SF (ON PARCEL A) = 89,825 ±SF
TOTAL UPLANDS ON PROPERTY	= 402,559 - 89,825 = 312,734 ±SF
IMPERVIOUS COVERAGE OF UPLANDS ON SITE	= 199,405/312,734 = 64% < 80% PERMITTED

JOB NO. **F2640**

APPROVED DATE:
 FRANKLIN PLANNING BOARD
John Connolly
William D'Amico
Greg Rankin

DATE: 12/18/17
 BEING A MAJORITY

PROFESSIONAL LAND SURVEYOR
 PROFESSIONAL ENGINEER

GEORGE HANCOCK CIVIL NO. 48068 REGISTERED PROFESSIONAL ENGINEER
 11-27-17

OWNER:

PARCEL "A"
 MOSELEY REALTY, LLC
 31 HAYWARD STREET
 FRANKLIN, MA 02038
 DEED BK. 14105 PG. 467

PARCEL B-1
 MOSELEY REALTY, LLC
 31 HAYWARD STREET
 FRANKLIN, MA 02038
 DEED BK. 33147 PG. 571

REVISIONS

DATE	REVISED
9-18-2017	REVISED NOTES PER TOWN COMMENTS
10-21-2017	REVISIONS PER TOWN COMMENTS
11/14/17	ADDED CERTIFICATE OF VOTE

Guerriere & Halnon, Inc.
 Engineering & Land Surveying
 Ph. (508) 528-3221 55 WEST CENTRAL STREET
 Fx. (508) 528-7921 FRANKLIN, MA 02038
 www.guerriereandhalnon.com

LAYOUT PLAN

**SITE PLAN MODIFICATION
 ADDITIONAL SE PARKING
 AT 31 HAYWARD STREET
 FRANKLIN
 MASSACHUSETTS**

DATE: JULY 5, 2017 SCALE: 1" = 40'
 SHEET: 3 OF 7 JOB NO. F2640

PLANTING SCHEDULE			
SYMBOL	NAME	SIZE	QUANTITY
☀	FOUNTAIN GRASS PENNISETUM ALOPECUROIDES	2 GAL. MIN.	16
☀	TUSSOCK SEDGE CAREX STRICTA	2 GAL. MIN.	22
☀	ARROWWOOD VIBURNUM DENTATUM	2 GAL. MIN.	17
☀	RIVER BIRCH BETULA NIGRA	4 FT. HIGH MIN. 2 1/2" MIN.	3
☀	WHITE PINE PINUS STROBUS	4 FT. HIGH MIN. 2 1/2" MIN.	2

ALL PLANTINGS ARE TAKEN FROM THE TOWN OF FRANKLIN BEST DEVELOPMENT PRACTICES QUIDEBOOK AND/OR U MASS AMHERST AGRICULTURE AND LANDSCAPE PROGRAM PLANT LISTING

APPROVED DATE:
FRANKLIN PLANNING BOARD
John Small
William...
Greg...

DATE: 12/18/17
BEING A MAJORITY



PROFESSIONAL LAND SURVEYOR
PROFESSIONAL ENGINEER

OWNER:
PARCEL "A"
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31 HAYWARD STREET
FRANKLIN, MA 02038
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31 HAYWARD STREET
FRANKLIN, MA 02038
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REVISIONS

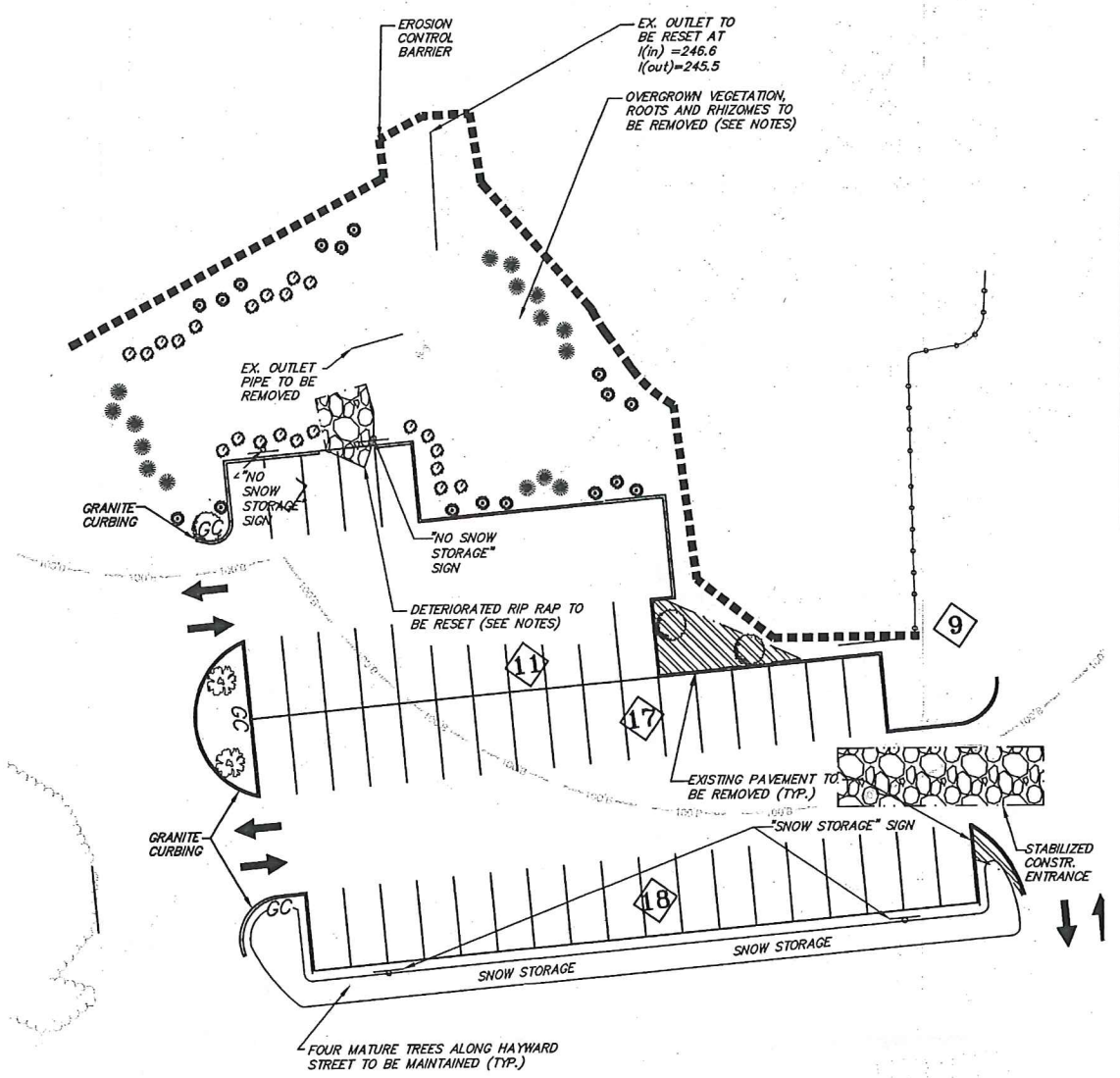
DATE	REVISED
8-29-17	TEMP. SED. REVISED PER BETA COMMENTS
10-21-2017	REVISIONS PER TOWN COMMENTS
11/14/17	ADDED CERTIFICATE OF VOTE

Guerriere & Halnon, Inc.
Engineering & Land Surveying
55 WEST CENTRAL STREET
FRANKLIN, MA 02038
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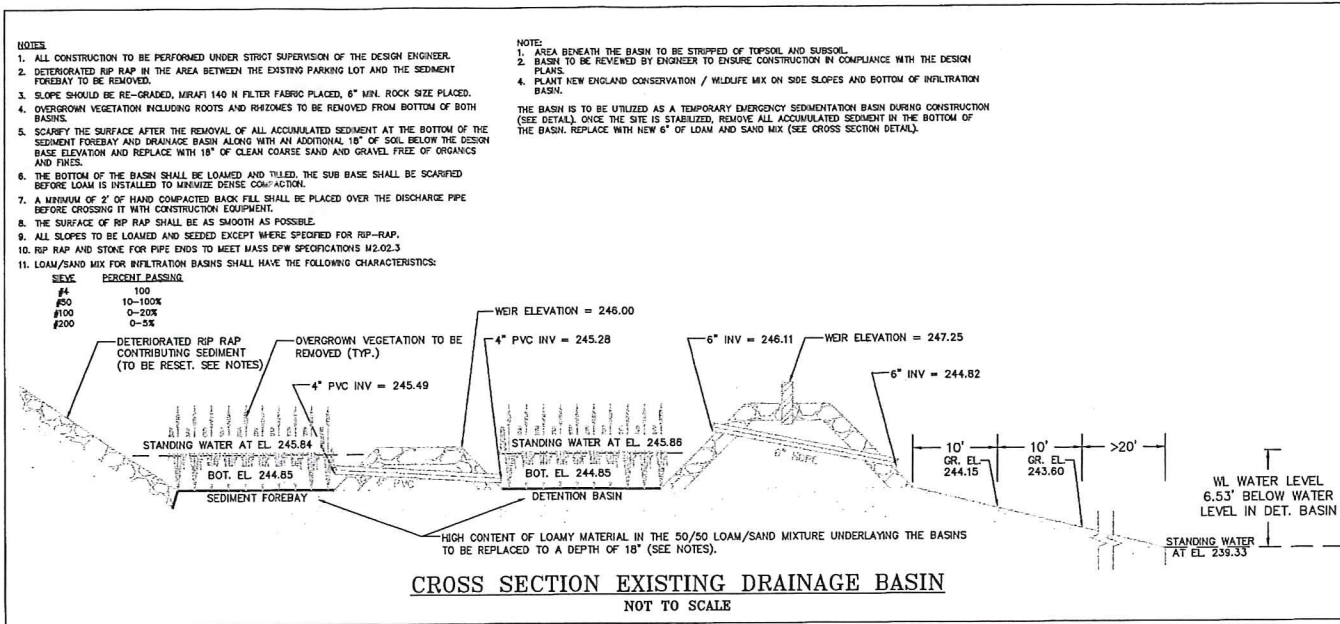
EROSION CONTROLS

SITE PLAN MODIFICATION
ADDITIONAL SE PARKING
AT 31 HAYWARD STREET
FRANKLIN
MASSACHUSETTS

DATE	JULY 5, 2017	SCALE	AS NOTED
SHEET	4 OF 7	JOB NO.	F2640



DRAINAGE SYSTEM LAYOUT
SCALE: 1" = 20'



APPROVED DATE:
FRANKLIN PLANNING BOARD
John Connell
William Deane
Greg Rowley

DATE: 12/18/17
BEING A MAJORITY



PROFESSIONAL LAND SURVEYOR

PROFESSIONAL ENGINEER

OWNER:

PARCEL "A"

MOSELEY REALTY, LLC
31 HAYWARD STREET
FRANKLIN, MA 02038
DEED BK. 14105 PG. 467

PARCEL B-1

MOSELEY REALTY, LLC
31 HAYWARD STREET
FRANKLIN, MA 02038
DEED BK. 33147 PG. 571

REVISIONS

DATE	REVISED
9-18-2017	EDITS PER TOWN COMMENTS
10-21-2017	REVISIONS PER TOWN COMMENTS
11/14/17	ADDED CERTIFICATE OF VOTE



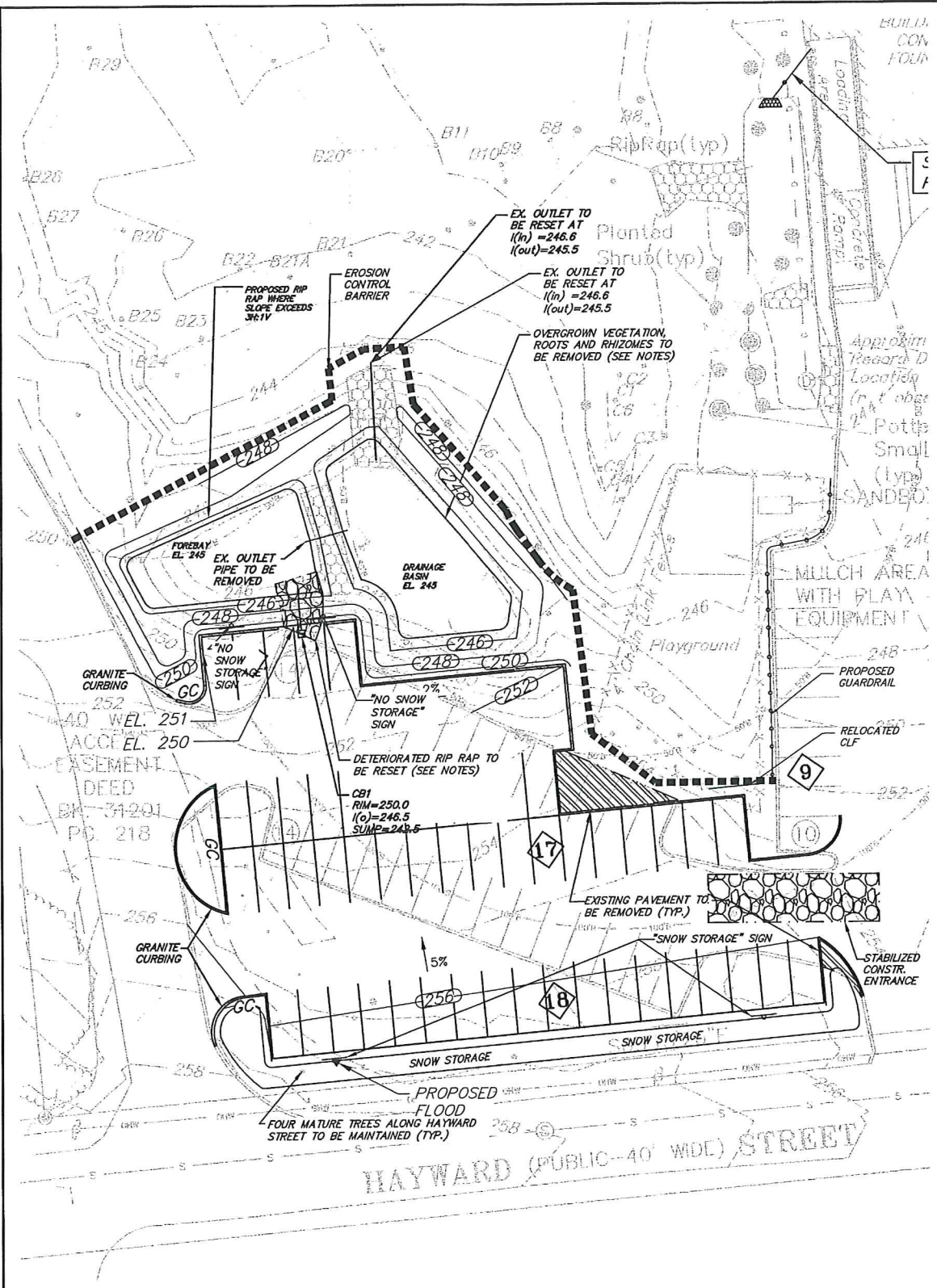
Guerriere & Halnon, Inc.
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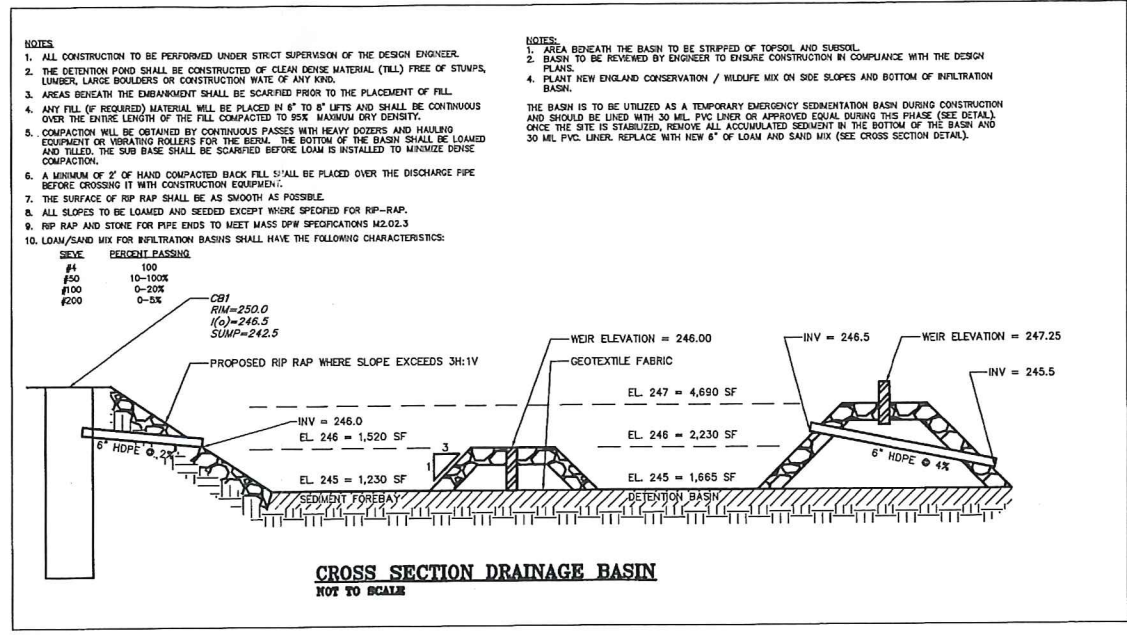
DRAINAGE DETAILS

**SITE PLAN MODIFICATION
ADDITIONAL SE PARKING
AT 31 HAYWARD STREET
FRANKLIN
MASSACHUSETTS**

DATE JULY 5, 2017	SCALE AS NOTED
SHEET 6 OF 7	JOB NO. F2640



DRAINAGE SYSTEM LAYOUT
SCALE: 1" = 20'



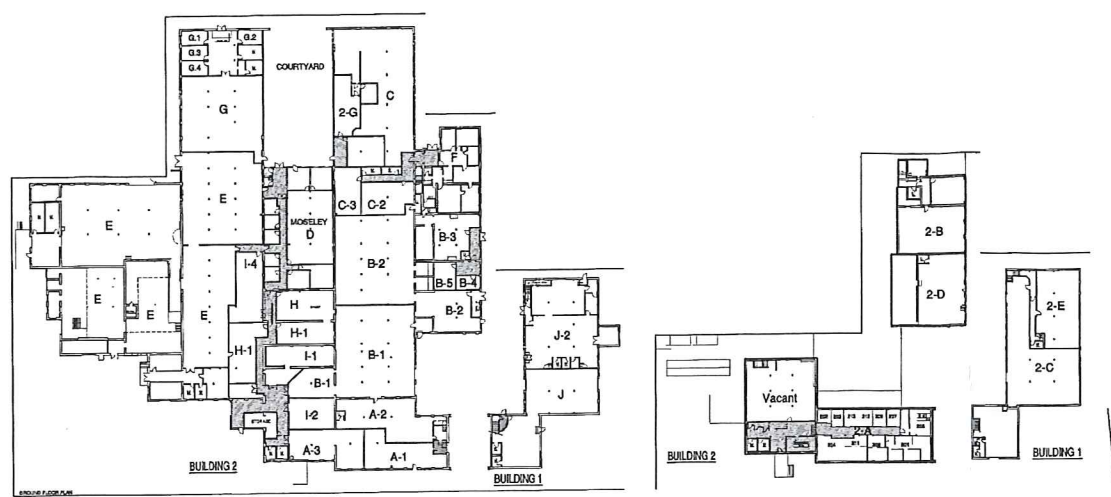
CROSS SECTION DRAINAGE BASIN
NOT TO SCALE

- NOTES:**
1. ALL CONSTRUCTION TO BE PERFORMED UNDER STRICT SUPERVISION OF THE DESIGN ENGINEER.
 2. THE DETENTION POND SHALL BE CONSTRUCTED OF GLENN DENSE MATERIAL (TLL) FREE OF STUMPS, LIMBER, LARGE BOULDERS OR CONSTRUCTION WASTE OF ANY KIND.
 3. AREAS BENEATH THE EMBANKMENT SHALL BE SCARIFIED PRIOR TO THE PLACEMENT OF FILL.
 4. ANY FILL (IF REQUIRED) MATERIAL WILL BE PLACED IN 6" TO 8" LIFTS AND SHALL BE CONTINUOUS OVER THE ENTIRE LENGTH OF THE FILL COMPACTED TO 95% MAXIMUM DRY DENSITY.
 5. COMPACTION WILL BE OBTAINED BY CONTINUOUS PASSES WITH HEAVY DOZERS AND HAULING EQUIPMENT OR VIBRATING ROLLERS FOR THE BERM. THE BOTTOM OF THE BASIN SHALL BE LOADED AND TILLED. THE SUB BASE SHALL BE SCARIFIED BEFORE LOAM IS INSTALLED TO MAXIMIZE DENSE COMPACTION.
 6. A MINIMUM OF 2" OF HAND COMPACTED BACK FILL SHALL BE PLACED OVER THE DISCHARGE PIPE BEFORE CROSSING IT WITH CONSTRUCTION EQUIPMENT.
 7. THE SURFACE OF RIP RAP SHALL BE AS SMOOTH AS POSSIBLE.
 8. ALL SLOPES TO BE LOAMED AND SEEDED EXCEPT WHERE SPECIFIED FOR RIP-RAP.
 9. RIP RAP AND STONE FOR PIPE ENDS TO MEET MASS DPW SPECIFICATIONS M2.02.3
 10. LOAM/SAND MIX FOR INFILTRATION BASINS SHALL HAVE THE FOLLOWING CHARACTERISTICS:
- | SIZE | PERCENT PASSING |
|------|-----------------|
| #100 | 10-100% |
| #200 | 0-20% |
| #400 | 0-5% |

PARKING COUNTS 2017						
Tenant	UNIT #	Use	S/F	Employees	Required Parking	
O.R. Colan Associates	2A/201	OFFICE	570	2	2.28	
New England CM, Inc. (Office)	2A/202	LT. MAN	120	2	0.48	
VACANT	2A/203	OFFICE	250		0	
VACANT	2A/204	OFFICE	250		0	
Ten Key Accounting	2A/205	OFFICE	275	1	1.1	
Fenton Financial	2A/206	OFFICE	350	1	1.4	
VACANT	2A/207	OFFICE	175		0	
VACANT	2A/209	OFFICE	120		0	
Hills Penson Associates	2A/211	OFFICE	250	3	1	
VACANT	2A/212	OFFICE	120		0	
Marc H Clough, LMHC	2A/213	OFFICE	120	1	0.48	
The Bench Group	2-B	OFFICE	1,500	4	7.6	
HFA Allevato	2-C	OFFICE	4,498	28	22.49	
McSharry Entrp LLC dba Closet & Storage Concepts	2-D B-2	LT. MAN/OFFICE	7,418	6	18.545	
Law Office of Barbara Levine Kravetz	2-E	OFFICE	2,026	6	10.13	
Plates by Owen	2-G		500	1	2	
RZ Design Associates	A-1	OFFICE	1,364	6	5.456	
JBT-Janus Logistics	A-2	OFFICE	2,000	5	8	
RSS Industries, Inc.	B1	LT. MAN/OFFICE	5,012	7	12.53	
Vicario Studio	B-3		800	1	3.2	
Siewe Light Counseling	B-4	OFFICE	200	1	0.8	
Therapy Connections	B-5	OFFICE	350	1	1.75	
Sanctuary Spas Corporation	C	RETAIL	5,000	10	20	
Carol Hetherington dba Blue Heron Acupuncture	C-2	OFFICE	880	1	4.4	
Dahab Associates	C-3	OFFICE	700	1	2.8	
The Moseley Corporation	D	OFFICE	4,172	5	16.688	
Interactions Corp	E	OFFICE	20,449	60	81.796	
Next Level	F	OFFICE	2,268	4	9.072	
Anglican Church	G	RELIGIOUS	4,100	1	16.4	
Dobritte Norris	G-2	OFFICE	250	1	1	
Dennis O'Brien	G-3	OFFICE	300	4	1.2	
New England CM, Inc.	H	LT. MAN.	933	2	3.732	
Summit Int'l Medical Technologies	H-1	OFFICE / WAREHOUSE	1,777	4	1.777	
Xpression Prints Inc.	I-1, I-2	RETAIL	1,800	5	9	
Abbey Investments LLC	A-3	OFFICE	1,490	4	5.96	
Bioactive Labs, Inc.	I-4	OFFICE	1,100	1	4.4	
Berry Insurance Agcy.	J	OFFICE	2,742	10	10.968	
Sunrise Montessori School	J-2	EDUCATION	3,450	5	13.8	
		AL				

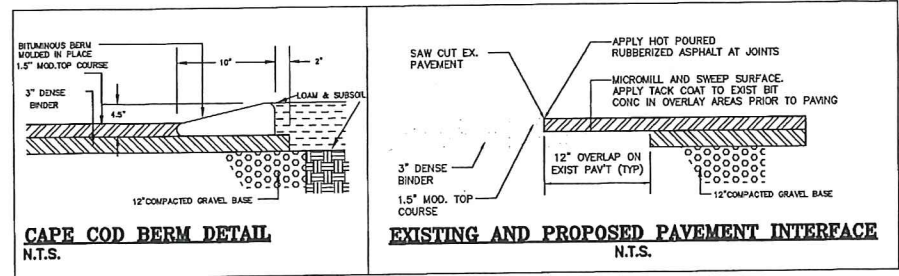
TOTAL SQUARE FOOTAGE 80,079
 TOTAL EMPLOYEES 194
 TOTAL PARKING REQUIRED 302.23

**CURRENT OCCUPANT LAYOUT
FRANKLIN INNOVATION CENTER**



**FIRST FLOOR PLAN
NOT TO SCALE**

**SECOND FLOOR PLAN
NOT TO SCALE**



**CAPE COD BERM DETAIL
N.T.S.**

**EXISTING AND PROPOSED PAVEMENT INTERFACE
N.T.S.**

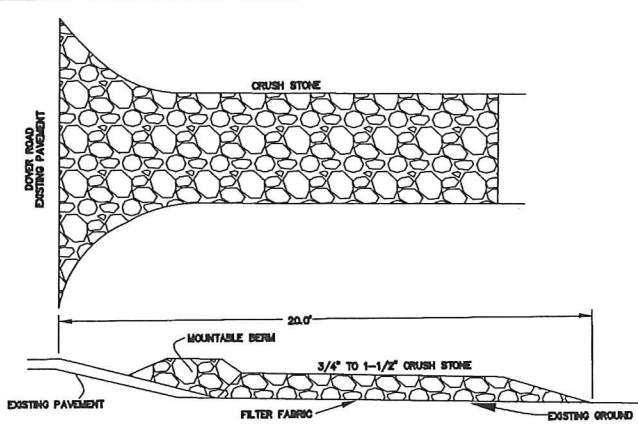
STORMWATER: LONG-TERM MAINTENANCE PLAN

A. NAMES OF PERSONS OR ENTITIES RESPONSIBLE FOR PLAN COMPLIANCE:
 CARL REED
 FRANKLIN INNOVATION CENTER
 31 HAYWARD STREET
 BOX 645
 FRANKLIN, MA 02038
 TEL: 508-541-1323

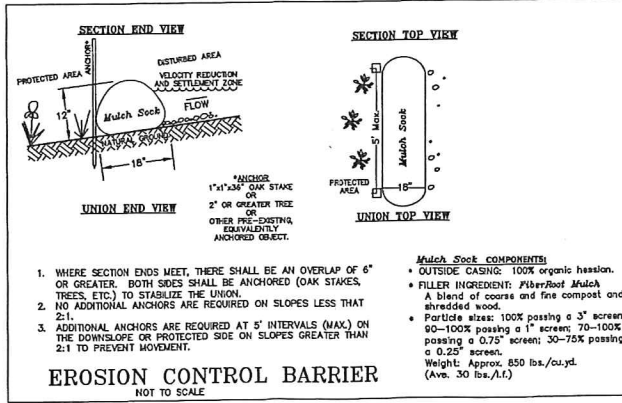
1. PAVEMENT: THE OWNER SHALL KEEP THE PAVEMENT SWEEPED WITH A MECHANICAL SWEEPER OR HAND SWEEP SEMI-ANNUALLY AT A MINIMUM.
2. DEEP SUMP CATCH BASINS: SHALL BE CLEANED BY EXCAVATING, PUMPING OR VACUUMING. THE SEDIMENT SHALL BE DISPOSED OF OFF-SITE BY THE OWNER. INSPECT QUARTERLY, REMOVE SILT WHEN 1/4 FULL.
3. DETENTION/RETENTION POND: INSPECT TWICE PER YEAR AND AFTER EVERY MAJOR EVENT FOR THE FIRST FEW MONTHS. CLEAN SEDIMENT OUT OF BASIN 2 TIMES PER YEAR OR AS 0.8 FT OF SEDIMENT ACCUMULATES.
4. A PROHIBITION FOR SNOW DISPOSAL IN THE FOREBAY OR DETENTION POND SHALL BE IN PLACE. SNOW SHOULD BE DISPOSED ONLY IN THE DESIGNATED AREAS EAST OF THE PARKING LOT.

INTERIM EROSION CONTROL AND CONSTRUCTION SEQUENCE

1. INSTALL EROSION/SEDIMENT CONTROL BARRIERS.
2. CLEAR SITE OF ALL TREES DESIGNATED TO BE REMOVED IN AREAS ADJACENT TO WETLANDS. CONSTRUCT BERMS AND SLOPES WITHIN THE SITE TO MAINTAIN STORMWATER RUNOFF.
3. STOCKPILE LOAM.
4. DRAINAGE SHALL BE MONITORED BY THE CONTRACTOR.
5. BRING SITE TO SUB-GRADE.
6. ALL SLOPES WITHIN THE PROPERTY LINE SHALL BE MULCHED TEMPORARILY.
7. TEMPORARY STONE (3/4\"-1 1/2\") SHALL BE PLACED AT THE PROJECT ENTRANCE TO CONTROL SILT ONTO THE ACCESS ROAD. SEE STABILIZE CONSTRUCTION ENTRANCE DETAIL.
8. ALL DISTURBED AREAS NOT TREATED WITH PERMANENT LOAM AND SEED SHALL BE COVERED WITH MULCH.
9. INSTALL SILT TRAP AT THE PROPOSED AND EXISTING CATCH BASINS FOR THE DURATION OF CONSTRUCTION.
10. ALL CONSTRUCTION GRADES IN THE INTERIM SHALL BE SLOPED TO FLOW INTO THE PROPOSED CATCH BASIN, WHERE POSSIBLE.
11. THE SITE MITIGATION OF EROSION IN THOSE AREAS TO BE LANDSCAPED OR MULCHED SHALL BE TO INSTALL TEMPORARY MULCH.
12. ONCE SITE HAS BEEN STABILIZED CLEAN ALL SEDIMENTS OUT OF THE GRASS AREAS, INSTALL 6\" OF LOAM AND SEED.
13. INSTALL PERMANENT MULCH AND LANDSCAPING ALONG THE PAVED AREAS.
14. SEDIMENT CONTROL SHALL REMAIN IN PLACE UNTIL THE SITE IS STABILIZED.
15. KEEP SITE SWEEPED AND MAINTAINED PER STORMWATER MANAGEMENT PLAN.



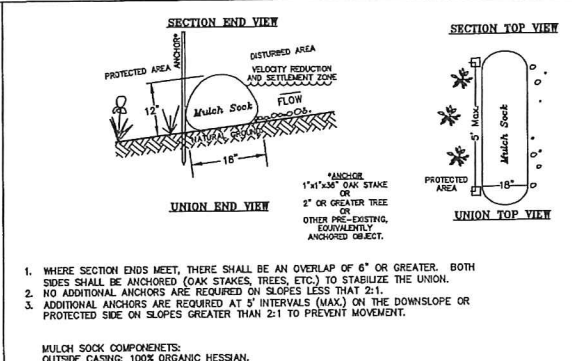
**STABILIZED CONSTRUCTION ENTRANCE
NOT TO SCALE**



**EROSION CONTROL BARRIER
NOT TO SCALE**

1. WHERE SECTION ENDS MEET, THERE SHALL BE AN OVERLAP OF 6\" OR GREATER. BOTH SIDES SHALL BE ANCHORED (OAK STAKES, TREES, ETC.) TO STABILIZE THE UNION.
2. NO ADDITIONAL ANCHORS ARE REQUIRED ON SLOPES LESS THAN 2:1.
3. ADDITIONAL ANCHORS ARE REQUIRED AT 5' INTERVALS (MAX.) ON THE DOWNSLOPE OR PROTECTED SIDE ON SLOPES GREATER THAN 2:1 TO PREVENT MOVEMENT.

- Mulch Sock COMPONENTS**
- OUTSIDE CASING: 100% organic hessian.
 - FILLER INGREDIENTS: FiberRoot Mulch A blend of coarse and fine compost and shredded wood.
 - Particle sizes: 100% passing a 3\" screen; 90-100% passing a 1\" screen; 70-100% passing a 0.75\" screen; 30-75% passing a 0.25\" screen.
 - Weight: Approx. 850 lbs./cu.yd. (Ave. 30 lbs./ft.)



**EROSION CONTROL BARRIER
NOT TO SCALE**

1. WHERE SECTION ENDS MEET, THERE SHALL BE AN OVERLAP OF 6\" OR GREATER. BOTH SIDES SHALL BE ANCHORED (OAK STAKES, TREES, ETC.) TO STABILIZE THE UNION.
2. NO ADDITIONAL ANCHORS ARE REQUIRED ON SLOPES LESS THAN 2:1.
3. ADDITIONAL ANCHORS ARE REQUIRED AT 5' INTERVALS (MAX.) ON THE DOWNSLOPE OR PROTECTED SIDE ON SLOPES GREATER THAN 2:1 TO PREVENT MOVEMENT.

- MULCH SOCK COMPONENTS:**
- OUTSIDE CASING: 100% ORGANIC HESSIAN.
 - FILLER INGREDIENTS: FIBER ROOT MULCH. A blend of coarse and fine compost and shredded wood.
 - Particle sizes: 100% passing a 3\" screen; 90-100% passing a 1\" screen; 70-100% passing a 0.75\" screen; 30-75% passing a 0.25\" screen.
 - Weight: Approx. 850 lbs./cu.yd. (Ave. 30 lbs./ft.)

JOB NO. F2640

APPROVED DATE:
 FRANKLIN PLANNING BOARD
Carl Reed
William Sam
Sean Rankin

DATE: 12/18/17
 BEING A MAJORITY



PROFESSIONAL LAND SURVEYOR

PROFESSIONAL ENGINEER

OWNER:

PARCEL "A"
 MOSELEY REALTY, LLC
 31 HAYWARD STREET
 FRANKLIN, MA 02038
 DEED BK. 14105 PG. 467

PARCEL B-1
 MOSELEY REALTY, LLC
 31 HAYWARD STREET
 FRANKLIN, MA 02038
 DEED BK. 33147 PG. 571

REVISIONS

DATE	REVISED
8-29-17	EROSION BARRIER DETAIL REPLACED
10-21-2017	REVISIONS PER TOWN COMMENTS
11/14/17	ADDED CERTIFICATE OF VOTE

Guerriere & Halnon, Inc.
 Engineering & Land Surveying
 Ph. (508) 528-3221 55 WEST CENTRAL STREET
 Fx. (508) 528-7921 FRANKLIN, MA 02038
 www.guerriereandhalnon.com

SITE DETAILS

**SITE PLAN MODIFICATION
ADDITIONAL SE PARKING
AT 31 HAYWARD STREET
FRANKLIN
MASSACHUSETTS**

DATE JULY 5, 2017	SCALE AS NOTED
SHEET 7 OF 7	JOB NO. F2640



**FRANKLIN PLANNING & COMMUNITY
DEVELOPMENT**

355 EAST CENTRAL STREET, ROOM 120
FRANKLIN, MA 02038-1352
TELEPHONE: 508-520-4907

MEMORANDUM

DATE: September 22, 2020
TO: Franklin Planning Board
FROM: Department of Planning and Community Development
RE: 162 Grove Street – NETA
Special Permit & Site Plan Modification

General:

1. The site is approximately 4 acres and is located at 162 Grove Street in the Industrial Zoning and Marijuana Overlay District; Assessor's Map 306 Lot 003.
2. Applicant has filed for a Special Permits: To allow Non-medical marijuana facility under 185 Attachment 3, Part II Section 2.23 and To allow Medical Marijuana under 185-49(4)(b)(iii)
3. The footprint of the existing buildings is approximately 12,421 square feet. NETA proposes to expand the existing buildings, as shown on the proposed Site Plans and to convert the existing buildings into approximately 3,856 square feet of retail space, approximately 4,647 square feet of office space, and approximately 7,584 square feet of warehouse space. There will be no product manufacturing, testing or research operations at the Facility.
4. Applicant has been approved by the Conservation Commission.
5. Applicant has received recommendation from Design Review.

Suggested Special Conditions based on the last Meeting:

1. **Town Council will authorize funding in the form of a traffic control light at the intersection of Washington and Grove Streets.**
2. The proposed facility will operate as a Reserve Ahead-only dispensary, which would require customers and patients to place an order in advance and select a scheduled pick up time to retrieve the product. Applicant may request this be reviewed after 30 days of opening.
3. The Transportation Demand Management Plan, submitted by the applicant, shall be included with the Certificate of Vote.
4. Design Review color recommendations shall be included in the endorsed set of plans.

Records on File:

1. Application for Site Plan and Special Permit
2. Certificate of Ownership
3. Special Permit Criteria
4. Abutters certified mailing
5. Overview of Proposed project and Special Permit Findings
6. Site Plans
7. Traffic Study
8. Stormwater Management Plans

ROLE CALL VOTE:

This determination shall be in addition to the following specific findings:

If you vote NO on any of the following, please state reason why you are voting NO:

- (1) Special Permits:** To allow Non-medical marijuana facility under 185 Attachment 3, Part II Section 2.23 and To allow Medical Marijuana under 185-49(4)(b)(iii).

- (a) Proposed project addresses or is consistent with neighborhood or Town need.

Anthony Padula	YES	NO	Joseph Halligan	YES	NO
Rick Power	YES	NO	Gregory Rondeau	YES	NO
William David	YES	NO			

- (b) Vehicular traffic flow, access and parking and pedestrian safety are properly addressed.

Anthony Padula	YES	NO	Joseph Halligan	YES	NO
Rick Power	YES	NO	Gregory Rondeau	YES	NO
William David	YES	NO			

- (c) Public roadways, drainage, utilities and other infrastructure are adequate or will be upgraded to accommodate development.

Anthony Padula	YES	NO	Joseph Halligan	YES	NO
Rick Power	YES	NO	Gregory Rondeau	YES	NO
William David	YES	NO			

- (d) Neighborhood character and social structure will not be negatively impacted.

Anthony Padula	YES	NO	Joseph Halligan	YES	NO
Rick Power	YES	NO	Gregory Rondeau	YES	NO
William David	YES	NO			

- (e) Project will not destroy or cause substantial damage to any environmentally-significant natural resource, habitat, or feature or, if it will, proposed mitigation, remediation, replication or compensatory measures are adequate.

Anthony Padula	YES	NO	Joseph Halligan	YES	NO
Rick Power	YES	NO	Gregory Rondeau	YES	NO
William David	YES	NO			

(f) Number, height, bulk, location and siting of building(s) and structure(s) will not result in abutting properties being deprived of light or fresh air circulation or being exposed to flooding or subjected to excessive noise, odor, light, vibrations, or airborne particulates.

Anthony Padula	YES	NO	Joseph Halligan	YES	NO
Rick Power	YES	NO	Gregory Rondeau	YES	NO
William David	YES	NO			

(g) Water consumption and sewer use, taking into consideration current and projected future local water supply and demand and wastewater treatment capacity, will not be excessive.

Anthony Padula	YES	NO	Joseph Halligan	YES	NO
Rick Power	YES	NO	Gregory Rondeau	YES	NO
William David	YES	NO			

The proposed use will not have adverse effects which overbalance its beneficial effects on either the neighborhood or the Town, in view of the particular characteristics of the site and of the proposal in relation to that site.

Anthony Padula	YES	NO	Joseph Halligan	YES	NO
Rick Power	YES	NO	Gregory Rondeau	YES	NO
William David	YES	NO			

STANDARD CONDITIONS OF APPROVAL

1. This Special Permit shall not be construed to run with the land and shall run with the Site Plan as endorsed by the Planning Board. A new Special Permit shall be required from the Planning Board if any major change of use or major change to the site plan is proposed.
2. This Special Permit shall lapse if a substantial use or construction has not begun, except for good cause, within twenty four (24) months of approval, unless the Board grants an extension. No final Certificate of Occupancy shall be issued until all requirements of the Special Permit have been completed to the satisfaction of the Board unless the applicant has submitted a Partial Certificate of Completion for the remainder of the required improvements and received approval by the Planning Board. The applicant's engineer or surveyor, upon completion of all required improvements, shall submit a Certificate of Completion. The Board or its agent(s) shall complete a final inspection of the site upon filing of the Certificate of Completion by the applicant. Said inspection is further outlined in condition #4.
3. Construction or operations under this Special Permit shall conform to any subsequent amendment of the Town of Franklin Zoning Bylaw (§185) unless the use or construction is commenced within a period of six (6) months after the issuance of this Special Permit and, in cases involving construction, unless such construction is continued through to completion as continuously and expeditiously as is reasonable.
4. **The Planning Board will use outside consultant services to complete construction inspections upon the commencement of construction.** The Franklin Department of Public Works Director, directly and through employees of the Department of Public Works and outside consultant services shall act as the Planning Board's inspector to assist the Board with inspections necessary to ensure compliance with all relevant laws, regulations and Planning Board approved plan specifications. Such consultants shall be selected and retained upon a majority vote of the Board.

5. Actual and reasonable costs of inspection consulting services shall be paid by the owner/applicant before or at the time of the pre-construction meeting. Should additional inspections be required beyond the original scope of work, the owner/applicant shall be required to submit fees prior to the issuance of a Final Certificate of Completion by the Planning Board (Form H). Said inspection is further outlined in condition #4.
6. No alteration of the Special Permit and the plans associated with it shall be made or affected other than by an affirmative vote of the members of the Board at a duly posted meeting and upon the issuance of a written amended decision.
7. All applicable laws, by-laws, rules, regulations, and codes shall be complied with, and all necessary licenses, permits and approvals shall be obtained by the owner/applicant.
8. Prior to the endorsement of the site plan, the following shall be done:
 - The owner/applicant shall make a notation on the site plan that references the Special Permit and the conditions and dates of this Certificate of Vote.
 - A notation shall be made on the plans that all erosion mitigation measures shall be in place prior to major construction or soil disturbance commencing on the site.
 - All outstanding invoices for services rendered by the Town's Engineers and other reviewing Departments of the Town relative to their review of the owner/applicant's application and plans shall have been paid in full.
 - The owner/applicant shall submit a minimum of six copies of the approved version of the plan.
9. Prior to any work commencing on the subject property, the owner/applicant shall provide plans to limit construction debris and materials on the site. In the event that debris is carried onto any public way, the owner/applicant and his assigns shall be responsible for all cleanup of the roadway. All cleanups shall occur within twenty-four (24) hours after first written notification to the owner/applicant by the Board or its designee. Failure to complete such cleanup may result in suspension of construction of the site until such public way is clear of debris.
10. The owner/applicant shall install erosion control devices as necessary and as directed by the Town's Construction Inspector.
11. **Prior to construction activities, there shall be a pre-construction meeting with the owner/applicant, and his contractor(s), the Department of Public Works and the Planning Board's Inspector.**
12. Any signage requires the Applicant to file with the Design Review Commission.
13. Prior to the endorsement, the Certificate of Vote and Order of Conditions shall be added to the Site Plans.

**FORM S:
APPLICATION FOR SCENIC ROAD WORK PERMIT**

1. Name and Address of Petitioner: Ryan + Margaret Camire
274 Prospect Street
Franklin MA 02038

Telephone Number: 617-388-6973 -
 (Please contact Lisa Muccicrone @ Burckon Design)

2. Check off the reason(s) which require a Work Permit under MGL Ch. 40, Section 15C (Scenic Roads Act): S08-198-0038

- Cutting or removal of trees within a designated Scenic Road
- Tearing down or destruction of stone walls or portions thereof within a Scenic Road

Describe the work proposed and any changes to tree(s) and/or stone wall(s) within the designated Scenic Road:

New driveway per attached plot plan

3. Location of Tree(s) and/or Stone Wall(s): (attach street map with locations marked)

Name of Scenic Road: Prospect Street
 Verified by: Michael Maglio

Tree(s) or Stone Wall(s) to be removed: stone wall

Specimen of Tree	Diameter of Tree	Length of Stone Wall	Height of Stone Wall
<i>Example: Oak</i>	<i>14"</i>	<i>75' across from pole 72/3 on south side of road</i>	

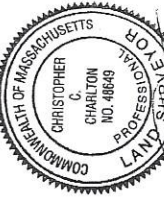
JOB# 16-0168

No. 278 PROSPECT ST.

N/F

LINDSEY TUFTS

I CERTIFY THAT THIS PLAN AS
CREATED BY AN INSTRUMENT SURVEY
ON THE GROUND AND THAT ALL
STRUCTURES ARE LOCATED AS
SHOWN HEREON.
I FURTHER CERTIFY THAT THE
LOT SHOWN AND THE IMPROVEMENTS
THEREON DO NOT LIE IN A WATER
RESOURCE DISTRICT.



CHRISTOPHER C. CHARLTON, PLS

PREPARED FOR:
RYAN CAMIRE
274 PROSPECT STREET
FRANKLIN, MA 02038
REFERENCES:
DEED: BOOK 32404, PAGE 82
PLAN: BOOK 3913, PAGE 381

CERTIFIED PLOT PLAN

LOCATED AT
274 PROSPECT STREET
ASSESSOR'S PARCEL # 309-011-000

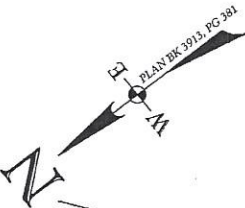
FRANKLIN, MA
NORFOLK COUNTY

CONTINENTAL LAND SURVEY, LLC
105 BEAVER STREET, FRANKLIN, MA
(508) 528-2528

SCALE: 1"=30' MAY 7, 2017



ZONING DISTRICT: RRI



PLAN BY 3913, PG 381

No. 2 PEPPERMILL LN.

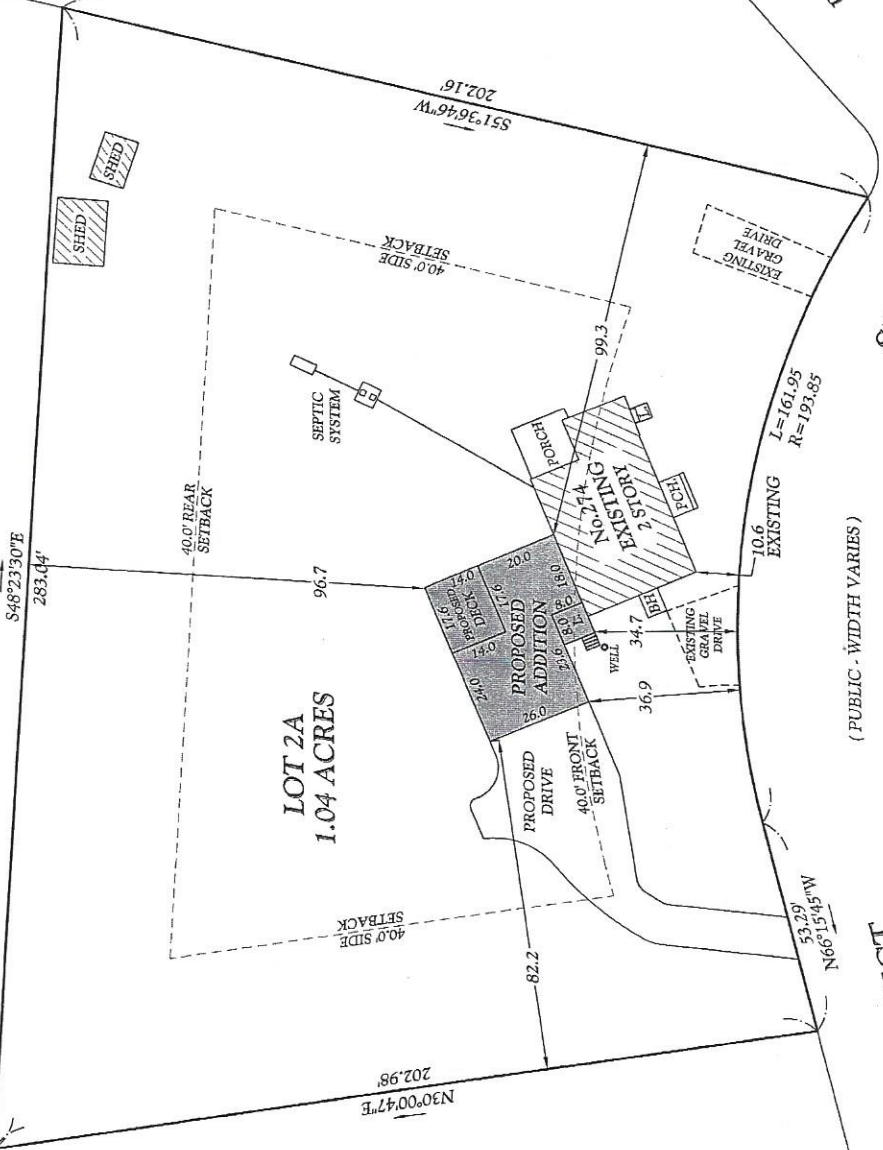
N/F

MARK LAPAGE

PEPPERTREE WAY

STREET

PROSPECT



LOT 2A
1.04 ACRES

No. 278 PROSPECT ST.

N/F

LINDSEY TUFTS

(PUBLIC - WIDTH VARIES)









**FRANKLIN PLANNING & COMMUNITY
DEVELOPMENT**

355 EAST CENTRAL STREET, ROOM 120
FRANKLIN, MA 02038-1352
TELEPHONE: 508-520-4907

MEMORANDUM

DATE: September 24, 2020
TO: Franklin Planning Board
FROM: Department of Planning and Community Development
RE: 274 Prospect Street
Scenic Road Permit

The DPCD has reviewed the above referenced Scenic Road Permit application for the Monday, September 28, 2020 Planning Board meeting and offers the following commentary:

General:

1. §170-53 of the Town Code, requires that any Stone wall within a Scenic Road must seek a permit through the Planning Board.
2. The applicant is proposing to remove a small portion of a stone wall in front of 274 Prospect Street.

Comments:

The Applicant has not provided any restoration measures.

The Planning Board will need to take a vote is they are in agreement of issuing a scenic road permit to remove a portion of the stonewall in front of 274 Prospect Street.

GIACALONE JAMES B
GIACALONE DOREEN
304 PROSPECT ST
FRANKLIN, MA 02038

SHEA KENNETH & JOHANN TRS
SHEA HIGH POINT TRUST
627 HIGH POINT DRIVE
MOUNT DORA, FL 32757

KLOWAN THEODOR J
KLOWAN LAURA A
286 PROSPECT ST
FRANKLIN, MA 02038

SHARPE MICHELLE L TR
SEWELL IRREVOCABLE TRUST
279 PROSPECT ST
FRANKLIN, MA 02038

HENDERSON EVERETT
HENDERSON MELISSA C
282 PROSPECT ST
FRANKLIN, MA 02038

COHEN MARY A
10 PEPPERMILL LN
FRANKLIN, MA 02038

TUFTS LINDSEY B
278 PROSPECT ST
FRANKLIN, MA 02038

LONGOBARDI ROBERT P JR
LONGOBARDI DANIELLE N
270 PROSPECT ST
FRANKLIN, MA 02038

CAMIRE RYAN L
CAMIRE MARGARET L
274 PROSPECT ST
FRANKLIN, MA 02038

LEPAGE MARK C
LEPAGE ALEXANDRA M
2 PEPPERMILL LN
FRANKLIN, MA 02038

TODESCA JOSEPH A
TODESCA MICHELE A
3 PEPPERMILL LN
FRANKLIN, MA 02038

FITZGERALD MICHAEL
FITZGERALD KATHLEEN L
5 PEPPERMILL LN
FRANKLIN, MA 02038

DUBLER JESSE
DUBLER BROOKE ANN
283 PROSPECT ST
FRANKLIN, MA 02038

LALIBERTE CAROLYN TREEN
LALIBERTE ANDRE M
267 PROSPECT ST
FRANKLIN, MA 02038

Town of Franklin



Planning Board

The following notice will be published in the Milford Daily Newspaper once on Monday, September 14, 2020 and again on September 21, 2020

FRANKLIN PLANNING BOARD PUBLIC HEARING NOTICE

In accordance with the Town of Franklin Zoning By-Laws, the Franklin Planning Board will hold a Remote public hearing on **Monday, September 28, 2020 at 7:10 PM**, for a Scenic Road application prepared by Bukkon Design, and submitted to the Department of Planning & Community Development by Ryan and Margaret Camire, Franklin, MA 02038. The site is located at 274 Prospect Street. The applicant is proposing to remove part of the stonewall located on Prospect Street.

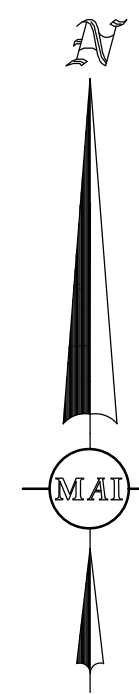
Please note: This will be your only written notice of this public hearing. All future Public Hearings on this matter, will be posted on the Planning Board's website under Agendas.

This meeting will be done remotely via "ZOOM" platform. Residents can view the Town Website and click on the Town Calendar for up to date information on access to the meeting.

Please contact the Department of Planning & Community Development at (508) 520-4907 if you require further information or if you need to make arrangements to provide translation services for the hearing impaired, or for persons with language barriers.

For copies of the plan and supporting documentation, please contact Department of Planning & Community Development via email alove@franklinma.gov.

Anthony Padula, Chairman



PERMIT SITE DEVELOPMENT PLANS

(TO ACCOMPANY A SITE PLAN REVIEW APPLICATION, ZONING APPROVAL APPLICATION & NOTICE OF INTENT)

164 GROVE STREET

(MAP: 306 LOT: 4)

LOCATED IN

FRANKLIN, MASSACHUSETTS

DATE: MAY 18, 2020

REVISED: AUGUST 20, 2020

REVISED: AUGUST 28, 2020

REVISED: SEPTEMBER 16, 2020

OWNER/APPLICANT:

NLCP 164 GROVE STREET MA, LLC

C/O NEWLAKE CAPITAL

549 W. RANDOLPH, SUITE 200
CHICAGO, IL 60661

PREPARED BY:



500 CUMMINGS CENTER SUITE 5950 BEVERLY, MASSACHUSETTS 01915
69 MILK STREET, SUITE 302 WESTBOROUGH, MASSACHUSETTS 01581
TELEPHONE: (978) 299-0447 TELEPHONE: (508) 871-7030
WWW.MERIDIANASSOC.COM

DRAWING INDEX:

- C0.0 COVER SHEET
- C1.0 RECORD CONDITIONS & DEMOLITION PLAN
- C2.0 LAYOUT, GRADING & EROSION CONTROL PLAN
- C3.0 UTILITY PLAN
- C4.0 LANDSCAPING PLAN
- C5.0 SITE DETAILS
- C5.1 SITE DETAILS
- C5.2 SITE DETAILS

REFERENCE DRAWINGS:

- VM1.0 VEHICLE MOVEMENT PLAN
- 1 of 1 SITE LIGHTING PLAN

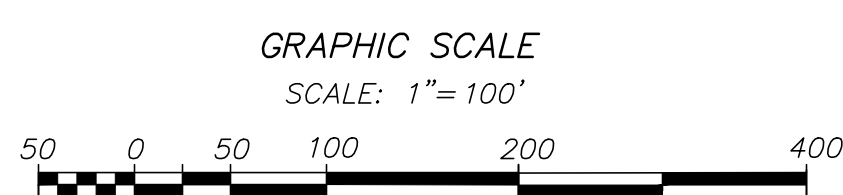
WAIVER REQUESTS:

A WAIVER IS BEING REQUESTED FROM (§300- 11.B.(2)(A)) TO ALLOW FOR HDPE STORM DRAIN PIPE IN LIEU OF CLASS V RCP.

APPROVED BY PLANNING BOARD

 DATE: _____

LOCUS CONTEXT MAP



MERIDIAN ASSOCIATES
 500 CUMMINGS CENTER, SUITE 5950
 BEVERLY, MASSACHUSETTS 01915
 TELEPHONE: (978) 299-0447
 69 MILK STREET, SUITE 302
 WESTBOROUGH, MASSACHUSETTS 01581
 TELEPHONE: (508) 871-7030
 WWW.MERIDIANASSOC.COM

INTERFORM
 ARCHITECTURE + DESIGN
 19 SOUTH LASALLE STREET
 SUITE 300 CHICAGO, IL 60603
 312/933.2701

PHARMACANN
 NEW CONSTRUCTION OF
 RETAIL CANNABIS
 DISPENSARY
 164 GROVE STREET
 FRANKLIN, MA 02038

ISSUED FOR PERMITTING ONLY NOT FOR CONSTRUCTION

NO.	DESCRIPTION	DATE
1	PLANNING BOARD REVIEW COMMENTS	08/20/2020
2	CONSERVATION COMMISSION FILING	08/28/2020
3	PLANNING BOARD REVIEW COMMENTS	09/16/2020

DATE	05/08/2020
SCALE	AS INDICATED
DRAWN	NB
CHECKED	DK
PROJECT NO.	6120-2

SEAL

COVER SHEET

C0.0

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UNOFFICIAL SOILS INFORMATION

DATE: OCTOBER 10, 2018
CONDUCTED BY: ANDREW RODRIGUEZ, SE (13890)

TEST PIT TP-1
ELEV.=252.0±
ASSUMED E.S.H.G.W. ELEV.=248.3±

0"-6" A HORIZON: SANDY LOAM
6"-13" B HORIZON: SANDY LOAM
13"-21" C LAYER: SAND AND GRAVEL, COBBLES
21"-32" C2 LAYER: FINE LOAMY SAND
32"-76" *C3 LAYER: COARSE SAND AND GRAVEL, COBBLES
REDOX @ 44"
WEEPING @ 46"
STANDING @ 74"
* POCKETS OF SANDY LOAM

TEST PIT TP-2
ELEV.=254.6±
ASSUMED E.S.H.G.W. ELEV.=251.5±

0"-15" HTM: SANDY LOAM
15"-23" B HORIZON: SANDY LOAM
23"-74" *C LAYER: COARSE SAND AND GRAVEL, COBBLES

LIGHT REDOX @ 37"
WEEPING @ 45"
STANDING @ 70"
* POCKETS OF SANDY LOAM

TEST PIT TP-3
ELEV.=257.6±
ASSUMED E.S.H.G.W. ELEV.=253.6±

0"-38" HTM: SANDY LOAM, BRICKS
38"-45" B HORIZON: SANDY LOAM
45"-68" *C LAYER: COARSE SAND AND GRAVEL, COBBLES

REDOX @ 48"
WEEPING @ 56"
STANDING @ 65"
* POCKETS OF SANDY LOAM

TEST PIT TP-4
ELEV.=260.2±
ASSUMED E.S.H.G.W. ELEV.=254.2±

0"-39" HTM: SANDY LOAM
39"-43" Ab HORIZON: SANDY LOAM
43"-60" B HORIZON: LOAMY SAND
60"-71" *C LAYER: COARSE SAND AND GRAVEL, COBBLES

WEEPING @ 48"
STANDING @ 69"
* POCKETS OF SANDY LOAM

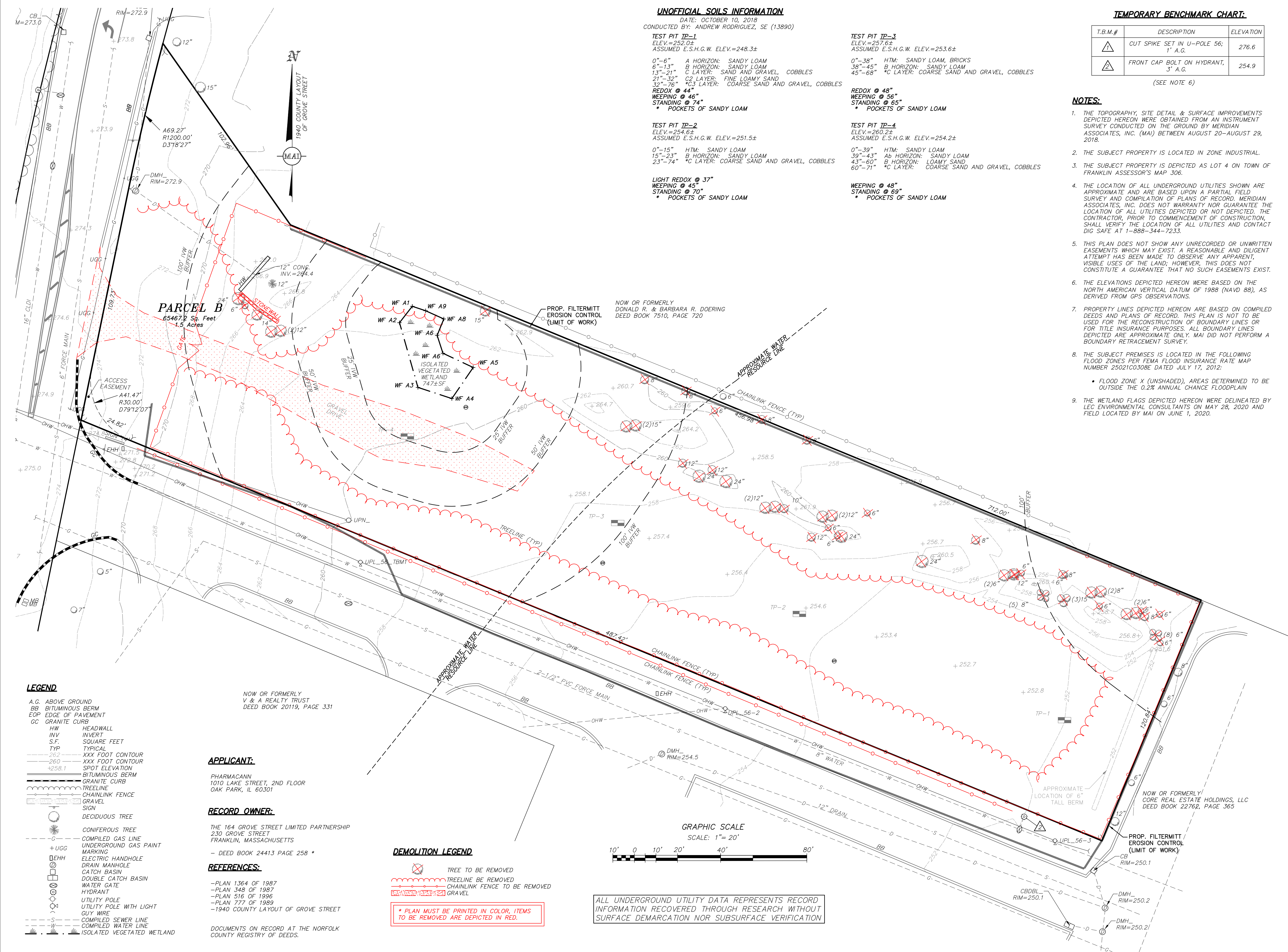
TEMPORARY BENCHMARK CHART:

Table with 3 columns: T.B.M.#, DESCRIPTION, ELEVATION. Contains data for CUT SPIKE SET IN U-POLE 56 and FRONT CAP BOLT ON HYDRANT.

(SEE NOTE 6)

NOTES:

- 1. THE TOPOGRAPHY, SITE DETAIL & SURFACE IMPROVEMENTS...
2. THE SUBJECT PROPERTY IS LOCATED IN ZONE INDUSTRIAL.
3. THE SUBJECT PROPERTY IS DEPICTED AS LOT 4 ON TOWN OF FRANKLIN ASSESSOR'S MAP 306.
4. THE LOCATION OF ALL UNDERGROUND UTILITIES SHOWN ARE APPROXIMATE AND ARE BASED UPON A PARTIAL FIELD SURVEY...
5. THIS PLAN DOES NOT SHOW ANY UNRECORDED OR UNWRITTEN EASEMENTS WHICH MAY EXIST...
6. THE ELEVATIONS DEPICTED HEREON WERE BASED ON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88)...
7. PROPERTY LINES DEPICTED HEREON ARE BASED ON COMPILED DEEDS AND PLANS OF RECORD...
8. THE SUBJECT PREMISES IS LOCATED IN THE FOLLOWING FLOOD ZONES PER FEMA FLOOD INSURANCE RATE MAP NUMBER 25021C0308E DATED JULY 17, 2012:
• FLOOD ZONE X (UNSHADED), AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN
9. THE WETLAND FLAGS DEPICTED HEREON WERE DELINEATED BY LEC ENVIRONMENTAL CONSULTANTS ON MAY 28, 2020 AND FIELD LOCATED BY MAI ON JUNE 1, 2020.



- LEGEND: A.G. ABOVE GROUND, BB BITUMINOUS BERM, EOP EDGE OF PAVEMENT, GC GRANITE CURB, HW HEADWALL, INV INVERT, S.F. SQUARE FEET, TYP TYPICAL, etc.

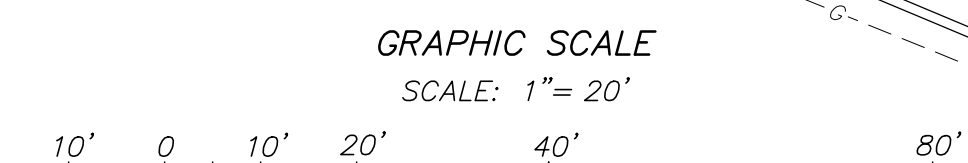
NOW OR FORMERLY V & A REALTY TRUST DEED BOOK 20119, PAGE 331

APPLICANT: PHARMACANN 1010 LAKE STREET, 2ND FLOOR OAK PARK, IL 60301

RECORD OWNER: THE 164 GROVE STREET LIMITED PARTNERSHIP 230 GROVE STREET FRANKLIN, MASSACHUSETTS DEED BOOK 24413 PAGE 258

REFERENCES: -PLAN 1364 OF 1987 -PLAN 348 OF 1987 -PLAN 516 OF 1996 -PLAN 777 OF 1989 -1940 COUNTY LAYOUT OF GROVE STREET DOCUMENTS ON RECORD AT THE NORFOLK COUNTY REGISTRY OF DEEDS.

- DEMOLITION LEGEND: TREE TO BE REMOVED, TREELINE TO BE REMOVED, CHAINLINK FENCE TO BE REMOVED, GRAVEL. * PLAN MUST BE PRINTED IN COLOR, ITEMS TO BE REMOVED ARE DEPICTED IN RED.



ALL UNDERGROUND UTILITY DATA REPRESENTS RECORD INFORMATION RECOVERED THROUGH RESEARCH WITHOUT SURFACE DEMARCATION NOR SUBSURFACE VERIFICATION

MERIDIAN ASSOCIATES 500 CUMMINGS CENTER, SUITE 5950 BEVERLY, MASSACHUSETTS 01915 TELEPHONE: (978) 299-0447

INTERFORM ARCHITECTURE + DESIGN 19 SOUTH LASALLE STREET SUITE 300 CHICAGO, IL 60603 312/933.2701

PHARMACANN

NEW CONSTRUCTION OF RETAIL CANNABIS DISPENSARY 164 GROVE STREET FRANKLIN, MA 02038

ISSUED FOR PERMITTING ONLY NOT FOR CONSTRUCTION

Table with 3 columns: NO., DESCRIPTION, DATE. Contains permit review comments.

Table with 2 columns: DATE, AS INDICATED. Contains drawing metadata.

SEAL: DAVID S. KELLEY CIVIL ENGINEER NO. 49389 09/18/2020

RECORD CONDITIONS & DEMOLITION PLAN

C1.0

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SCHEDULE OF DIMENSIONAL CONTROLS

ZONING DISTRICT: INDUSTRIAL (MARIJUANA OVERLAY DISTRICT)

Table with 4 columns: REQUIREMENT, MINIMUM, EXISTING, PROPOSED. Rows include LOT AREA, FRONTAGE, LOT DEPTH, LOT WIDTH, FRONT YARD, SIDE YARD, REAR YARD, BUILDING LOT COVERAGE, TOTAL IMPERVIOUS COVERAGE, BUILDING HEIGHT.

PARKING TABLE:

Table with 3 columns: REQUIRED, PROPOSED. Rows include RETAIL BUILDINGS (1 SPACE PER 200 SQUARE FEET OF GROSS FLOOR AREA), BUILDING GROSS FLOOR AREA, STANDARD PARKING SPACES, ACCESSIBLE PARKING SPACES.

GRADING NOTES:

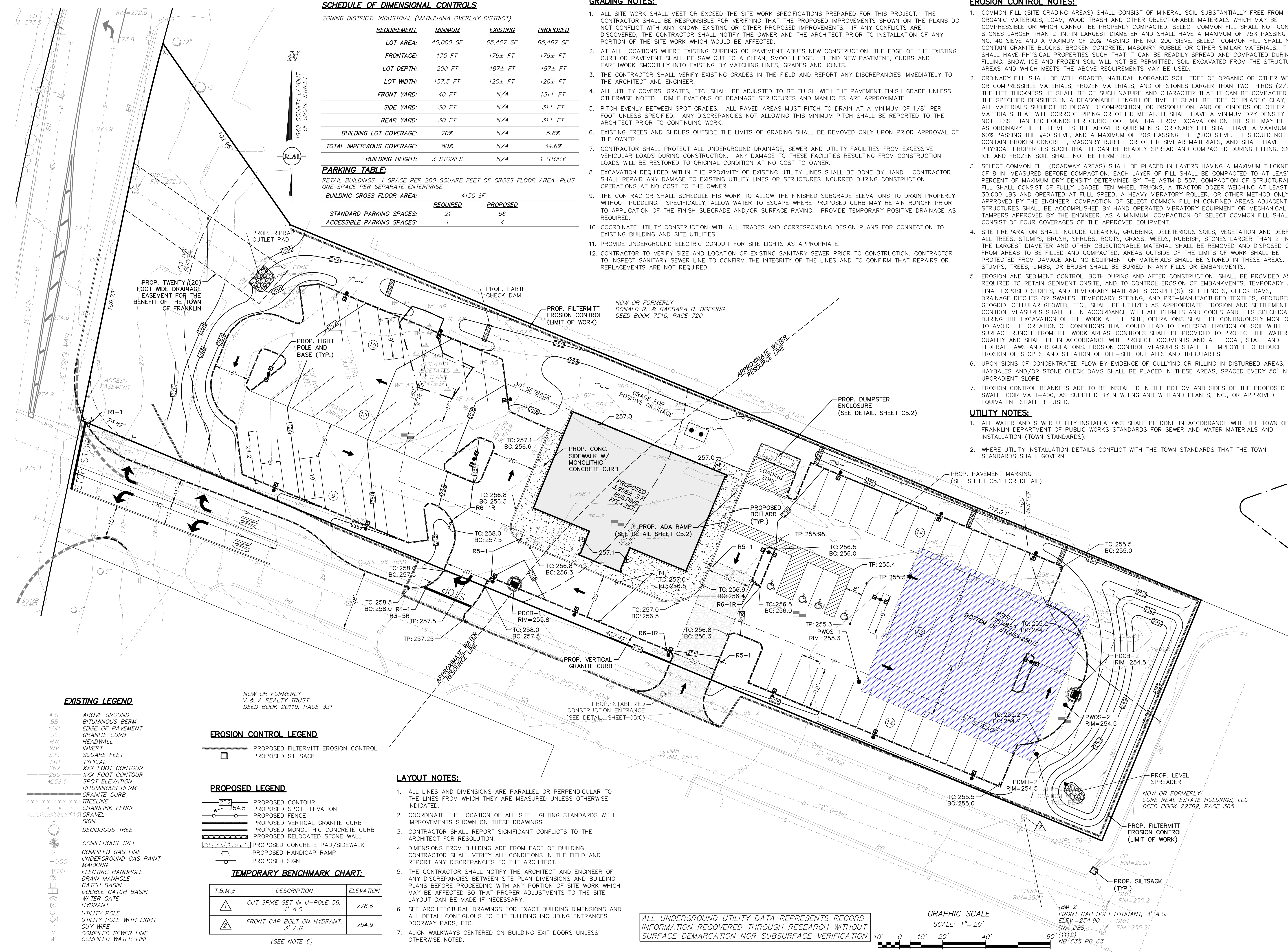
- 1. ALL SITE WORK SHALL MEET OR EXCEED THE SITE WORK SPECIFICATIONS PREPARED FOR THIS PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THAT THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS DO NOT CONFLICT WITH ANY KNOWN EXISTING OR OTHER PROPOSED IMPROVEMENTS.
2. AT ALL LOCATIONS WHERE EXISTING CURBING OR PAVEMENT ABUTS NEW CONSTRUCTION, THE EDGE OF THE EXISTING CURB OR PAVEMENT SHALL BE SAW CUT TO A CLEAN, SMOOTH EDGE.
3. THE CONTRACTOR SHALL VERIFY EXISTING GRADES IN THE FIELD AND REPORT ANY DISCREPANCIES IMMEDIATELY TO THE ARCHITECT AND ENGINEER.

EROSION CONTROL NOTES:

- 1. COMMON FILL (SITE GRADING AREAS) SHALL CONSIST OF MINERAL SOIL SUBSTANTIALLY FREE FROM ORGANIC MATERIALS, LOAM, WOOD TRASH AND OTHER OBJECTIONABLE MATERIALS WHICH MAY BE COMPRESSIBLE OR WHICH CANNOT BE PROPERLY COMPACTED.
2. ORDINARY FILL SHALL BE WELL GRADED, NATURAL INORGANIC SOIL, FREE OF ORGANIC OR OTHER WEAK OR COMPRESSIBLE MATERIALS, FROZEN MATERIALS, AND OF STONES LARGER THAN TWO THIRDS (2/3) THE LIFT THICKNESS.
3. SELECT COMMON FILL (ROADWAY AREAS) SHALL BE PLACED IN LAYERS HAVING A MAXIMUM THICKNESS OF 8 IN. MEASURED BEFORE COMPACTION.

UTILITY NOTES:

- 1. ALL WATER AND SEWER UTILITY INSTALLATIONS SHALL BE DONE IN ACCORDANCE WITH THE TOWN OF FRANKLIN DEPARTMENT OF PUBLIC WORKS STANDARDS FOR SEWER AND WATER MATERIALS AND INSTALLATION (TOWN STANDARDS).
2. WHERE UTILITY INSTALLATION DETAILS CONFLICT WITH THE TOWN STANDARDS THAT THE TOWN STANDARDS SHALL GOVERN.



EXISTING LEGEND

- A.G. ABOVE GROUND
BB BITUMINOUS BERM
EOP EDGE OF PAVEMENT
GC GRANITE CURB
HW HEADWALL
INV INVERT
S.F. SQUARE FEET
TYP TYPICAL
-262 XXX FOOT CONTOUR
-260 XXX FOOT CONTOUR
+258.1 SPOT ELEVATION
BITUMINOUS BERM
GRANITE CURB
TREELINE
CHAINLINK FENCE
GRAVEL SIGN
DECIDUOUS TREE
CONIFEROUS TREE
COMPILED GAS LINE
UNDERGROUND GAS PAINT MARKING
ELECTRIC HANDHOLE
DRAIN MANHOLE
CATCH BASIN
DOUBLE CATCH BASIN
WATER GATE
HYDRANT
UTILITY POLE
UTILITY POLE WITH LIGHT
GUY WIRE
COMPILED SEWER LINE
COMPILED WATER LINE

EROSION CONTROL LEGEND

- PROPOSED FILTERMITT EROSION CONTROL
PROPOSED SILTSACK

PROPOSED LEGEND

- PROPOSED CONTOUR
PROPOSED SPOT ELEVATION
PROPOSED FENCE
PROPOSED VERTICAL GRANITE CURB
PROPOSED MONOLITHIC CONCRETE CURB
PROPOSED RELOCATED STONE WALL
PROPOSED CONCRETE PAD/SIDEWALK
PROPOSED HANDICAP RAMP
PROPOSED SIGN

TEMPORARY BENCHMARK CHART:

Table with 3 columns: T.B.M.#, DESCRIPTION, ELEVATION. Rows include CUT SPIKE SET IN U-POLE 56; and FRONT CAP BOLT ON HYDRANT, 3' A.G.

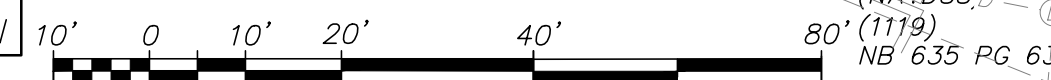
LAYOUT NOTES:

- 1. ALL LINES AND DIMENSIONS ARE PARALLEL OR PERPENDICULAR TO THE LINES FROM WHICH THEY ARE MEASURED UNLESS OTHERWISE INDICATED.
2. COORDINATE THE LOCATION OF ALL SITE LIGHTING STANDARDS WITH IMPROVEMENTS SHOWN ON THESE DRAWINGS.
3. CONTRACTOR SHALL REPORT SIGNIFICANT CONFLICTS TO THE ARCHITECT FOR RESOLUTION.

ALL UNDERGROUND UTILITY DATA REPRESENTS RECORD INFORMATION RECOVERED THROUGH RESEARCH WITHOUT SURFACE DEMARCATON NOR SUBSURFACE VERIFICATION

GRAPHIC SCALE

SCALE: 1" = 20'



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PHARMACANN

NEW CONSTRUCTION OF RETAIL CANNABIS DISPENSARY

164 GROVE STREET
FRANKLIN, MA 02038

ISSUED FOR PERMITTING ONLY NOT FOR CONSTRUCTION

Table with 3 columns: NO., DESCRIPTION, DATE. Rows include PLANNING BOARD REVIEW COMMENTS, CONSERVATION COMMISSION FILING, PLANNING BOARD REVIEW COMMENTS.

Table with 3 columns: DATE, SCALE, DRAWN, CHECKED, PROJECT NO. Values include 05/08/2020, AS INDICATED, NB, DK, 6120-2.

SEAL



LAYOUT, GRADING & EROSION CONTROL PLAN

C2.0

TEMPORARY BENCHMARK CHART:

T.B.M.#	DESCRIPTION	ELEVATION
1	CUT SPIKE SET IN U-POLE 56; 1' A.G.	276.6
2	FRONT CAP BOLT ON HYDRANT, 3' A.G.	254.9

(SEE NOTE 6)

Wetland Replication Area Planting Plan

Plant Species (Common Name)	Plant Species (Latin Name)	Spacing	Size	Quantity
Lurid Sedge	Carex lurida	1-2' O.C., Clustered	2" plug	144
Woolgrass	Scirpus cyperinus	1-2' O.C., Clustered	2" plug	144
Three Square Bulrush	Schoenoplectus pungens	1-2' O.C., Clustered	2" plug	144
Joe-Pyeweed	Eupatorium maculatum/purpureum	1-2' O.C., Clustered	2" plug	144
New England Aster	Aster novae-angliae	1-2' O.C., Clustered	2" plug	144
Soft Rush	Juncus effusus	1-2' O.C., Clustered	2" plug	144
Great Blue Lobelia	Lobelia siphilitica	1-2' O.C., Clustered	2" plug	144
Cardinal Flower	Lobelia cardinalis	1-2' O.C., Clustered	2" plug	144
Total				1,152

Wetland Replication Area Seed Mix: PA New England Province FACW Mix, Ernst Seeds, ernstseed.com, or equivalent, seed rate applied at 20lbs per acre.

LANDSCAPE TABLE:

PRIVATE PARKING LOTS: 1 TREE (2" DBH) PER 10 PARKING SPACES

PARKING SPACES = 70

	REQUIRED	PROPOSED
2" DBH TREES:	7	10

PLANT SCHEDULE

QTY	SYM	LATIN NAME	COMMON NAME	SIZE	NOTES
TREES					
10	AC	Amelanchier canadensis	Shadblow Serviceberry	6'-8' Ht. B&B	BR N ST White Birds Showy Edible Fruit Fall Color April-May
4	CC	Crataegus crus-galli var. inermis	Thornless Cockspur Hawthorn	3"-3.5" Cal. B&B	DT ST Thornless Variety White Flowers Red Fruit
3	JV	Juniperus virginiana	Eastern Red Cedar	6'-8' Ht. B&B	BR DR DT N ST Blueish/Black Fruit Wildlife Evergreen
1	PG	Picea glauca	White Spruce	6'-8' Ht. B&B	DR DT N ST Wildlife Evergreen
2	PP	Picea pungens	Colorado Blue Spruce	6'-8' Ht. B&B	DR DT ST Blueish Showy Evergreen
6	PV	Prunus virginiana	Chokecherry	2"-3" Cal. B&B	DT N ST Showy Red Fruit Color Wildlife
SHRUBS					
12	LB	Lindera benzoin	Northern Spicebush	24"-30" Ht. B&B	BR DR DT N ST 36" OC Yellow Birds Fall Color
25	IG	Ilex glabra 'Shamrock'	Shamrock inkberry	24"-30" Ht. B&B	BR DR DT N ST 36" OC Greenish-White Birds Evergreen May-June
22	MP	Myrica pensylvanica	Bayberry	36"-48" Ht. B&B	BR DT N ST 48" OC Birds Yellowish-green Winter Interest May
PERENNIALS & GROUND COVER					
300	RA	Rhus aromatica 'Gro-Low'	'Gro-Low' Sumac	#1 Pot	DR DT N 18" OC Low Growing May-September

BR = BIORETENTION | DR = DEER RESISTANT | DT = DROUGHT TOLERANT | N = NATIVE | ST = SALT TOLERANT | OC = ON-CENTER | B&B = BALLED AND BURLAPPED
NOTE: ALL TREE SPECIES SHALL BE FROM THE TOWN OF FRANKLIN BEST DEVELOPMENT PRACTICES GUIDEBOOK.

WETLAND REPLICATION AREA CONSTRUCTION SEQUENCE

- A QUALIFIED WETLAND SCIENTIST SHALL SUPERVISE ALL ASPECTS OF THE PROPOSED WETLAND REPLICATION AREA DURING CONSTRUCTION; EG., EROSION CONTROLS, SITE PREPARATORY, GRADING, BACKFILLING, PLANTING AND SEEDING.
- FLAG OR STAKE LIMITS OF WETLAND REPLICATION AREA. ONCE THE WETLAND REPLICATION AREA AND ADJACENT SIDE SLOPE HAVE BEEN GRADED, AN EROSION CONTROL BARRIER (I.E., STRAW WATTLES) MAY BE REQUIRED AT THE TOE OF SLOPE TO PROTECT THE WETLAND REPLICATION AREA.
- DURING CONSTRUCTION OF THE WETLAND REPLICATION AREA, THE SUPERVISORY WETLAND SCIENTIST SHALL OVERSEE THE PROPOSED GRADING AND PLANTING SCHEME. DURING CONSTRUCTION, THE WETLAND REPLICATION AREA SHALL BE EXCAVATED TO SIX INCHES BELOW THE FINAL DESIGN GRADE TO FACILITATE THE PLACEMENT OF APPROVED CLEAN WETLAND SOIL (12% ORGANIC CARBON CONTENT (OR 20% ORGANIC MATTER CONTENT) AND PH OF 6.2-6.8) AS A SUITABLE SUBSTRATE FOR THE ESTABLISHMENT OF WETLAND VEGETATION. WETLAND SOILS BE MANUFACTURED BY COMBINING TOPSOIL WITH A SOIL TEXTURAL CLASS OF SANDY LOAM, FINE SANDY LOAM, OR SILT LOAM WITH DECOMPOSED LEAF LITTER AT A 1:1 RATIO. THE WETLAND SOIL SHALL BE FREE OF STUMPS, ROOTS, HEAVY OR STIFF CLAY, STONES, COARSE SAND, NOXIOUS WEEDS, WEED SEEDS OR OTHER LITTER. SETTLING OF SOILS SHALL BE TAKEN INTO CONSIDERATION FOR FINAL ELEVATIONS.
- APPLY WETLAND SEED MIX (ERNST SEEDS, PA NEW ENGLAND PROVINCE FACW MIX, OR EQUIVALENT) AT A RATE OF 20 LBS PER ACRE OR HIGHER IF APPLIED AT THE END OF THE GROWING SEASON AND LIGHTLY RAKE TO INSURE SEED-TO-SOIL CONTACT. THERE SHALL BE NO SEEDING IN AREAS OF STANDING WATER.
- INSTALL BIODEGRADABLE COIR MAT FOLLOWED BY INSTALLATION OF 2" HERBACEOUS PLUGS.
- PLANTING: TIME OF WETLAND PLANTING SHALL BE BETWEEN THE START OF THE USDA GROWING SEASON THROUGH MAY 15 OR BETWEEN SEPTEMBER 1 AND THE END OF THE USDA GROWING SEASON (EARLY OCTOBER). PLANTING FROM MAY 16 TO AUGUST 31 MAY OCCUR UNDER SUITABLE ENVIRONMENTAL CONDITIONS AND THROUGH APPROVAL FROM THE SUPERVISORY WETLAND SCIENTIST. SUPPLEMENTAL WATERING MAY BE REQUIRED.
- APPLY CONSERVATION SEED MIX TO SIDE SLOPES AT A RATE SPECIFIED BY THE SUPPLIER AND LIGHTLY RAKE TO INSURE SEED-TO-SOIL CONTACT.
- APPLY A LIGHT MULCH OF CLEAN WEED FREE STRAW TO SIDE SLOPES.
- THE SUPERVISORY WETLAND SCIENTIST SHALL RESERVE THE RIGHT, DEPENDING ON WEATHER CONDITIONS, TO REQUIRE SUPPLEMENTAL WATERING OF WETLAND PLANTINGS.
- REMOVE EROSION CONTROL BARRIERS UPON STABILIZATION OF THE SIDE SLOPE AND WETLAND REPLICATION AREA.

PHARMACANN

NEW CONSTRUCTION OF RETAIL CANNABIS DISPENSARY

164 GROVE STREET
FRANKLIN, MA 02038

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NO.	DESCRIPTION	DATE
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2	CONSERVATION COMMISSION FILING	08/28/2020
3	PLANNING BOARD REVIEW COMMENTS	09/16/2020

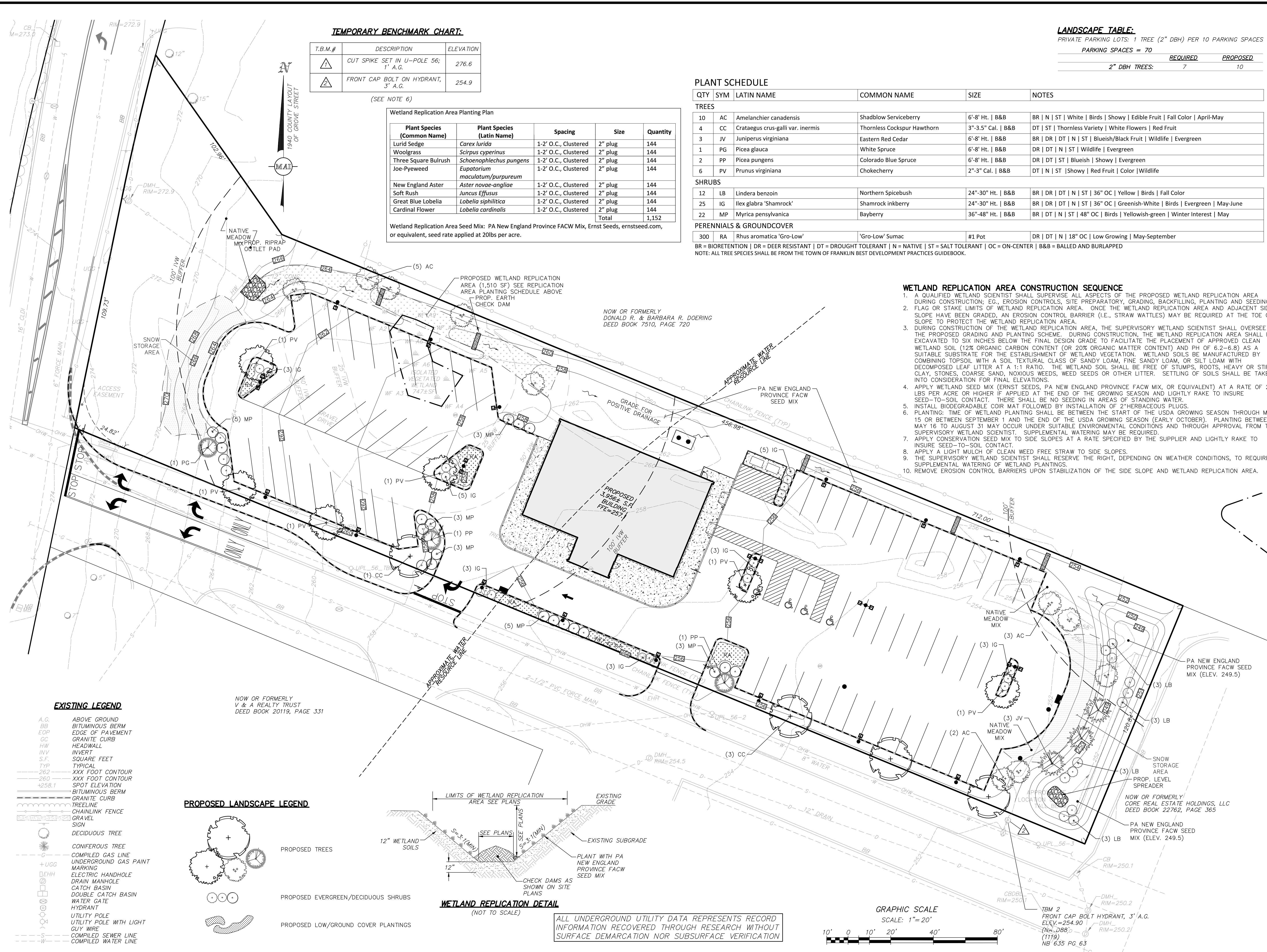
NO.	DESCRIPTION	DATE
DATE	05/08/2020	
SCALE	AS INDICATED	
DRAWN	NB	
CHECKED	DK	
PROJECT NO.	6120-2	

SEAL



LANDSCAPE PLAN

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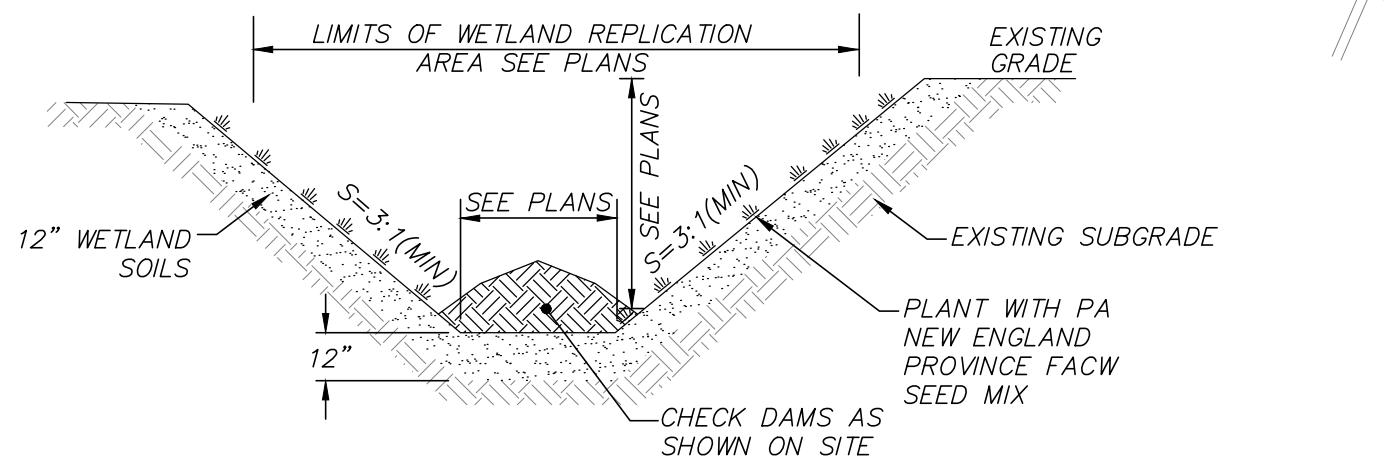
EXISTING LEGEND

- A.G. ABOVE GROUND
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- EOP EDGE OF PAVEMENT
- GC GRANITE CURB
- HW HEADWALL
- HW INVERT
- S.F. SQUARE FEET
- TYP TYPICAL
- 262 XXX FOOT CONTOUR
- 260 XXX FOOT CONTOUR
- +258.1 SPOT ELEVATION
- BITUMINOUS BERM
- GRANITE CURB
- TREELINE
- CHAINLINK FENCE
- GRAVEL
- SIGN
- DECIDUOUS TREE
- CONIFEROUS TREE
- CG COMPILED GAS LINE
- UGG UNDERGROUND GAS PAINT MARKING
- EHH ELECTRIC HANDHOLE
- DMH DRAIN MANHOLE
- CB CATCH BASIN
- DBH DOUBLE CATCH BASIN
- WATER GATE
- HYDRANT
- UTILITY POLE
- UTILITY POLE WITH LIGHT
- GUY WIRE
- CG COMPILED SEWER LINE
- W COMPILED WATER LINE

PROPOSED LANDSCAPE LEGEND

- PROPOSED TREES
- PROPOSED EVERGREEN/DECIDUOUS SHRUBS
- PROPOSED LOW/GROUND COVER PLANTINGS

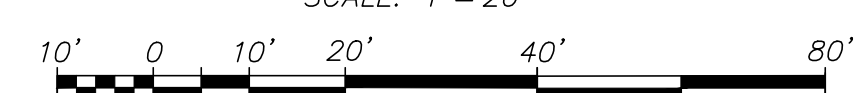
WETLAND REPLICATION DETAIL



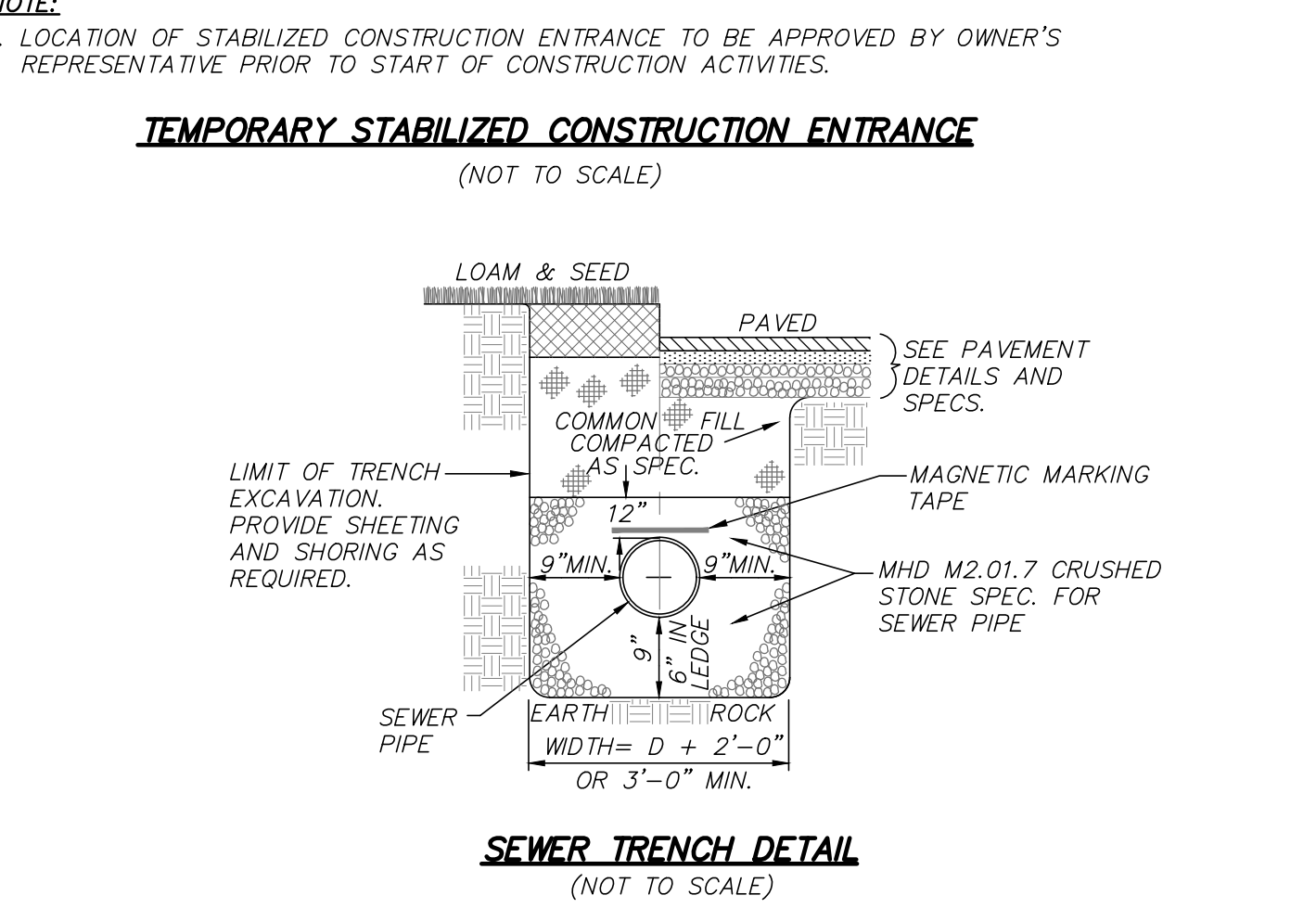
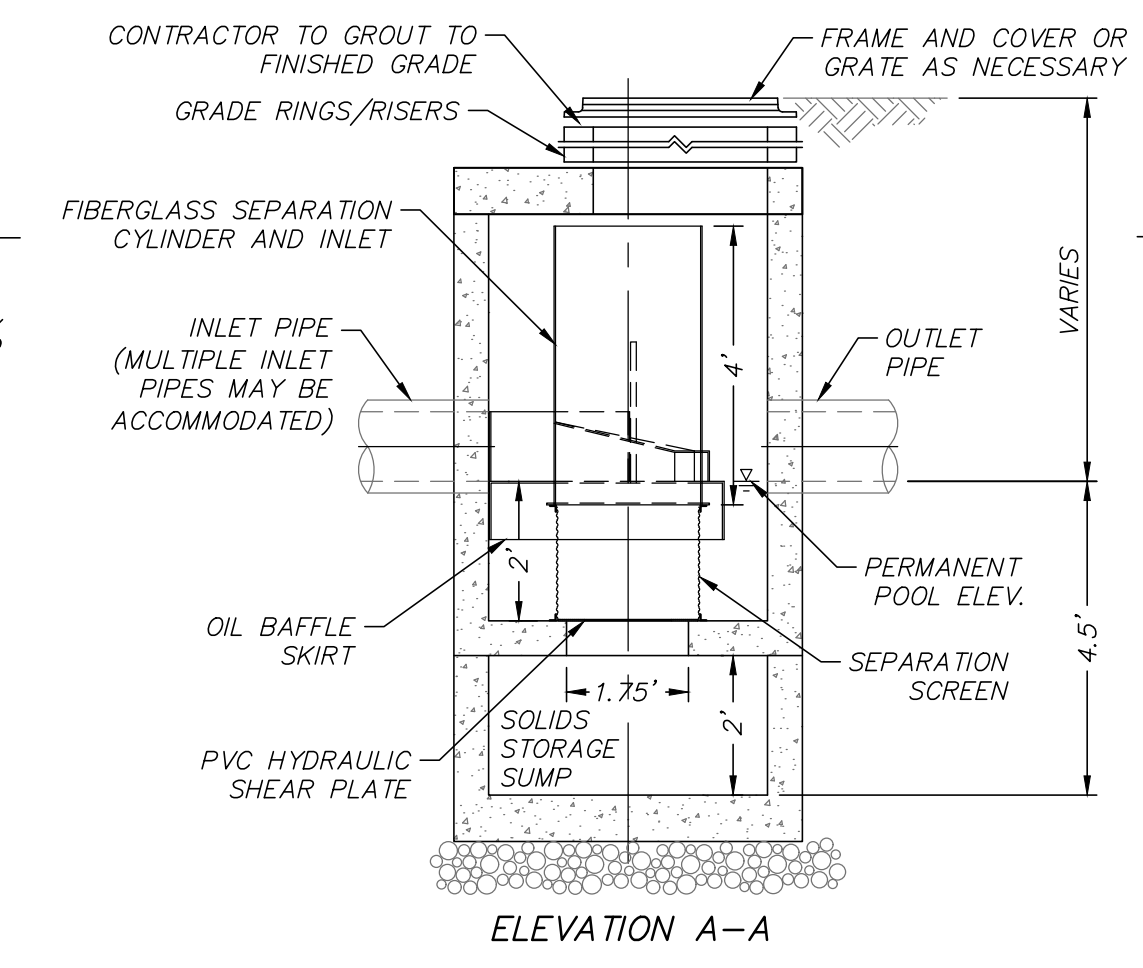
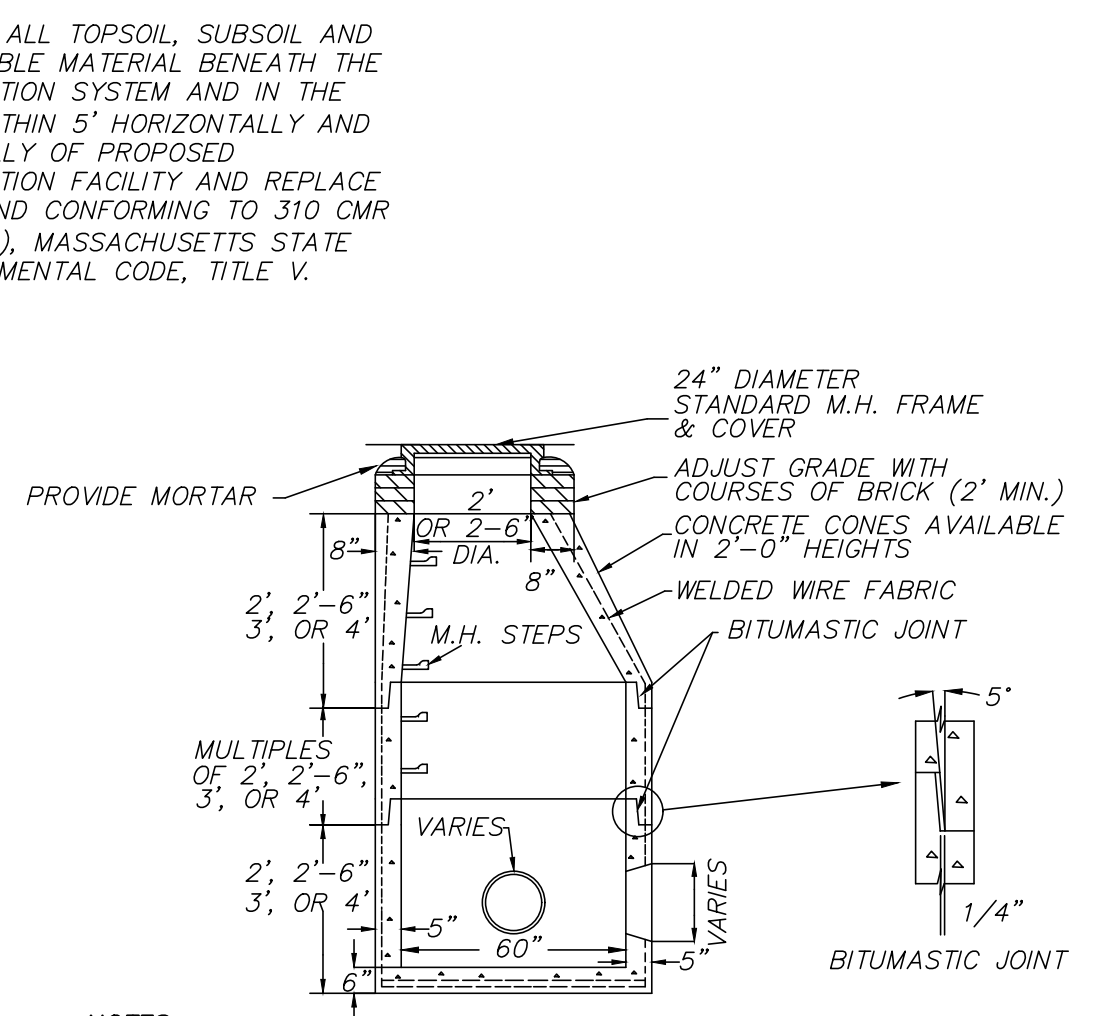
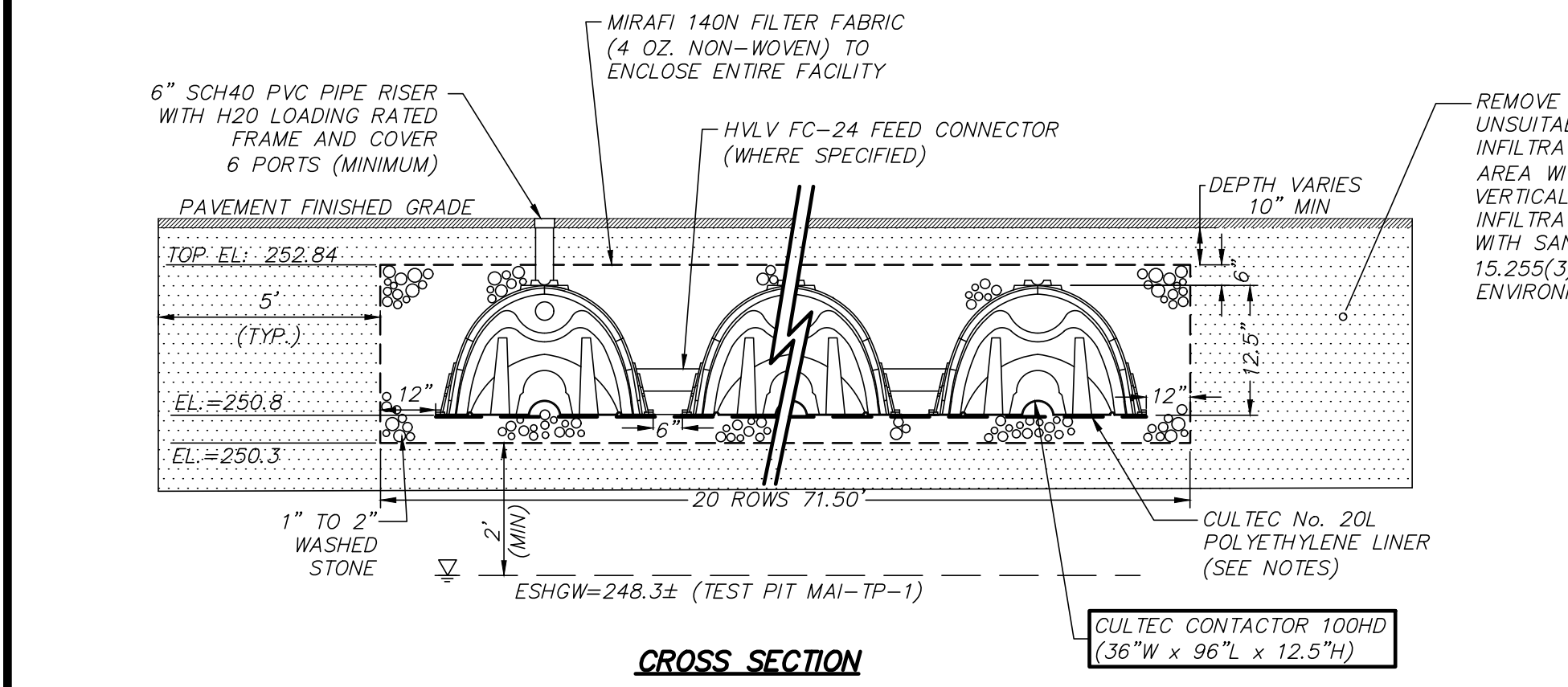
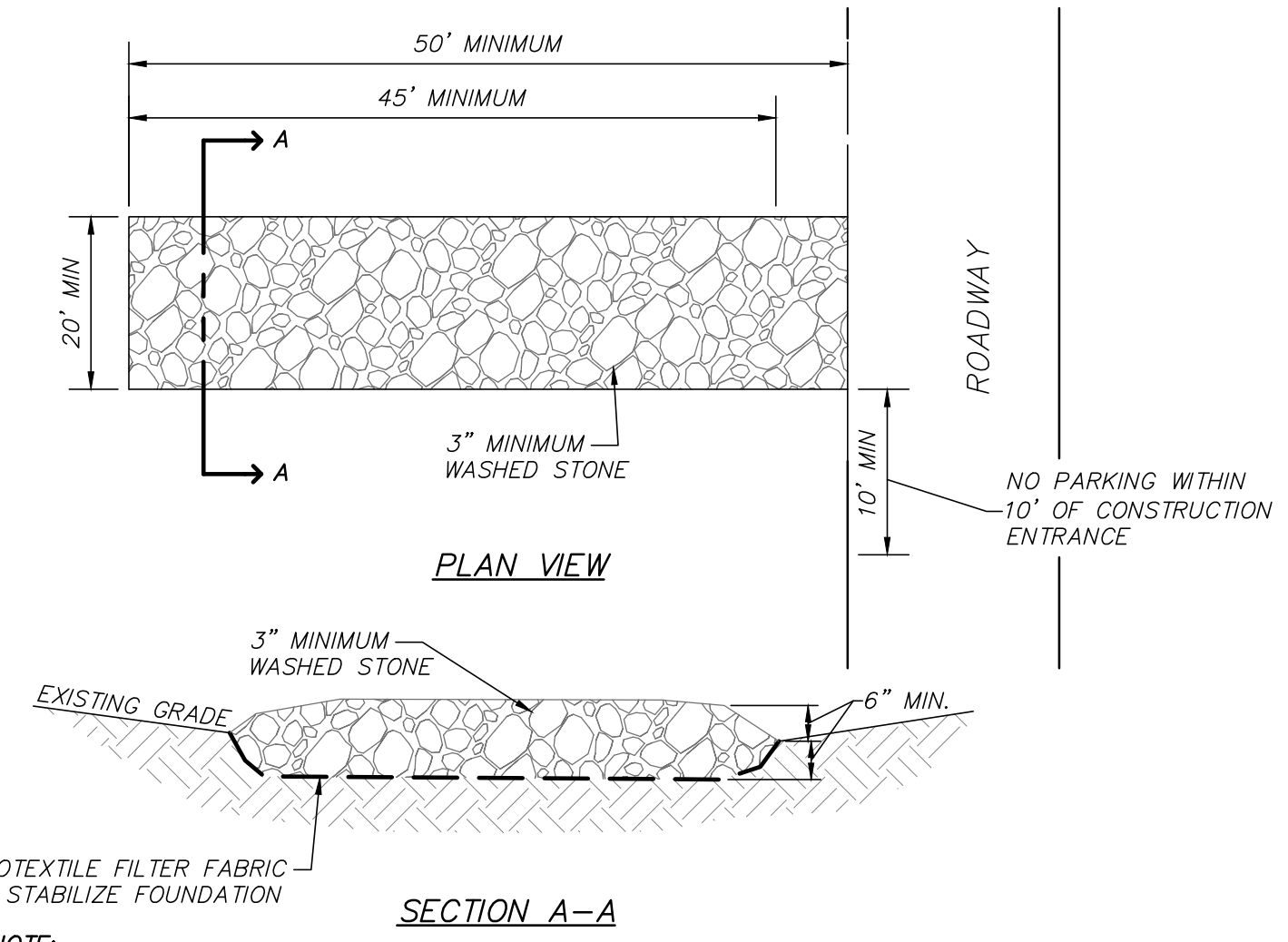
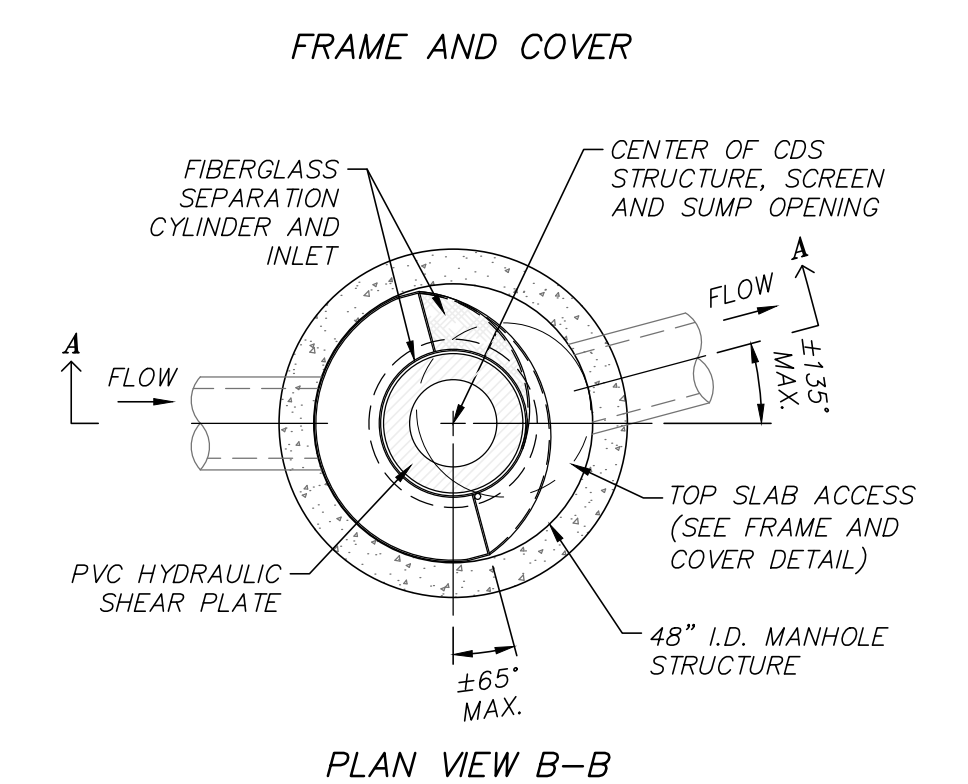
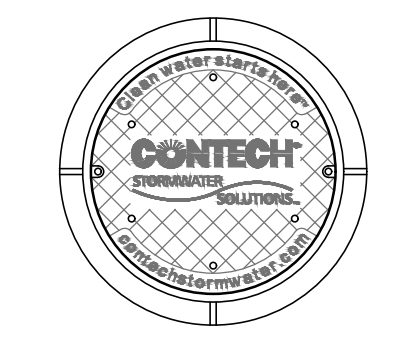
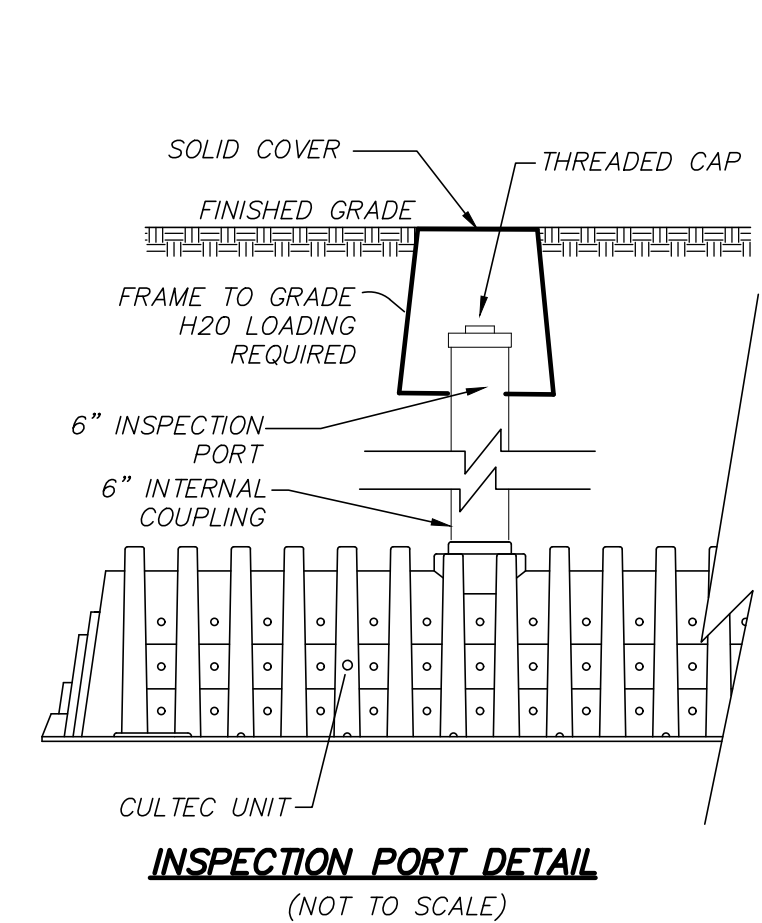
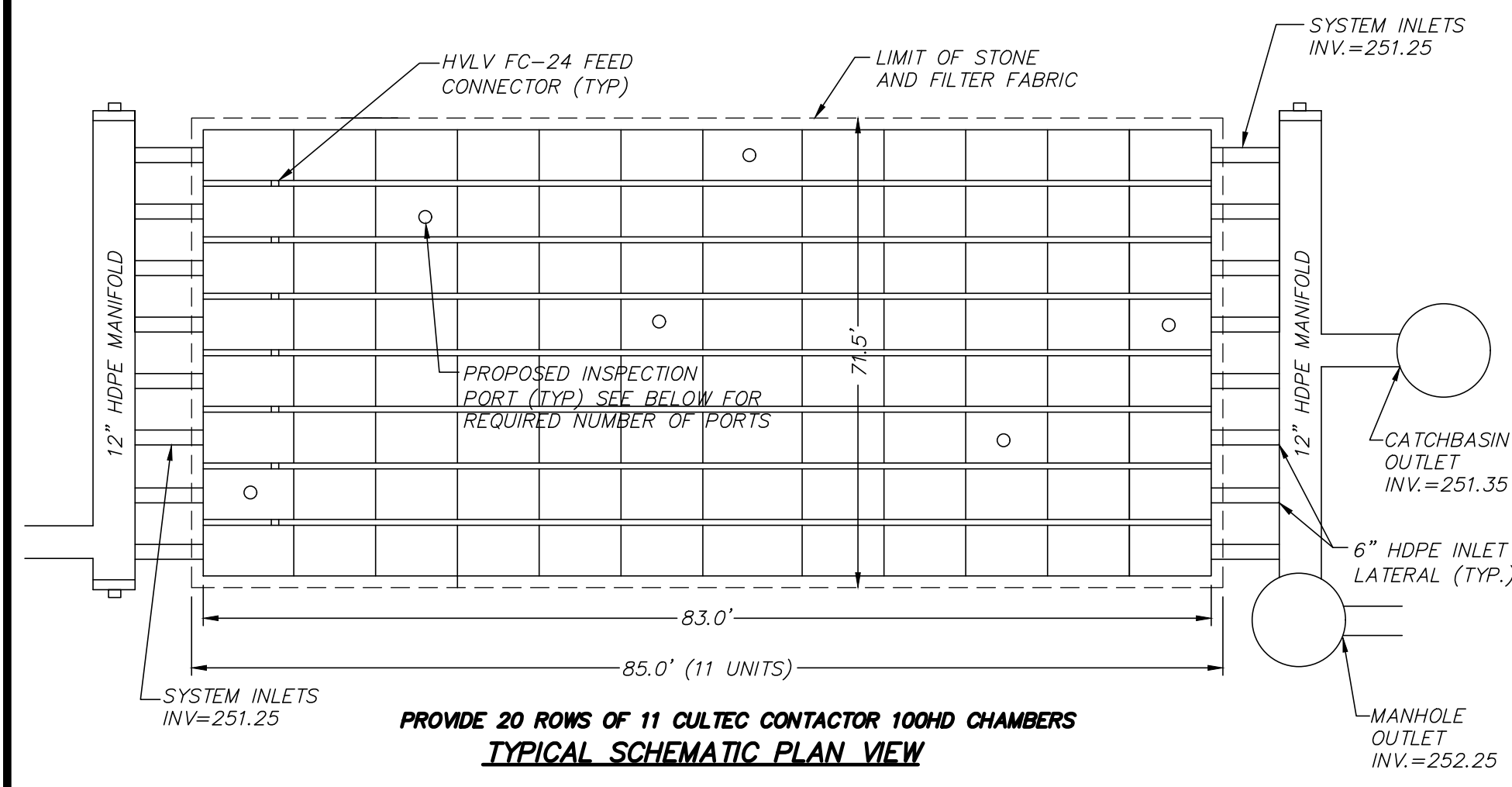
ALL UNDERGROUND UTILITY DATA REPRESENTS RECORD INFORMATION RECOVERED THROUGH RESEARCH WITHOUT SURFACE DEMARICATION NOR SUBSURFACE VERIFICATION

GRAPHIC SCALE

SCALE: 1" = 20'



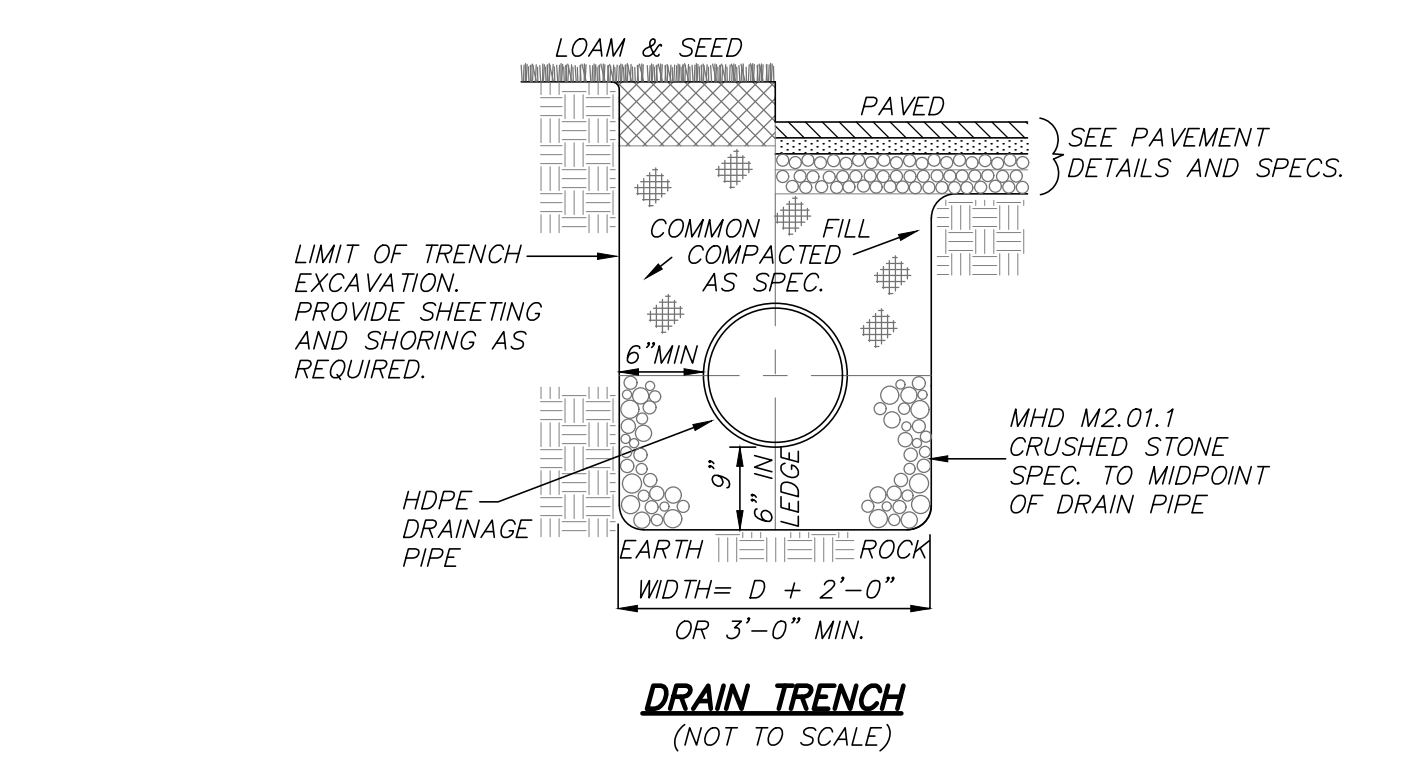
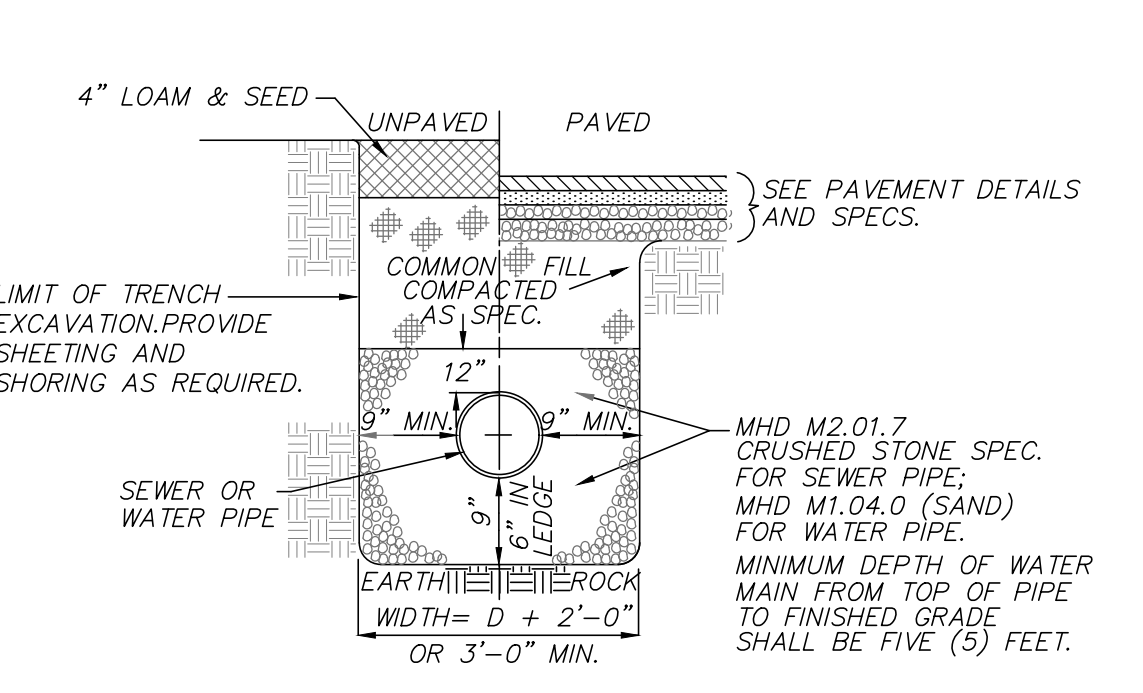
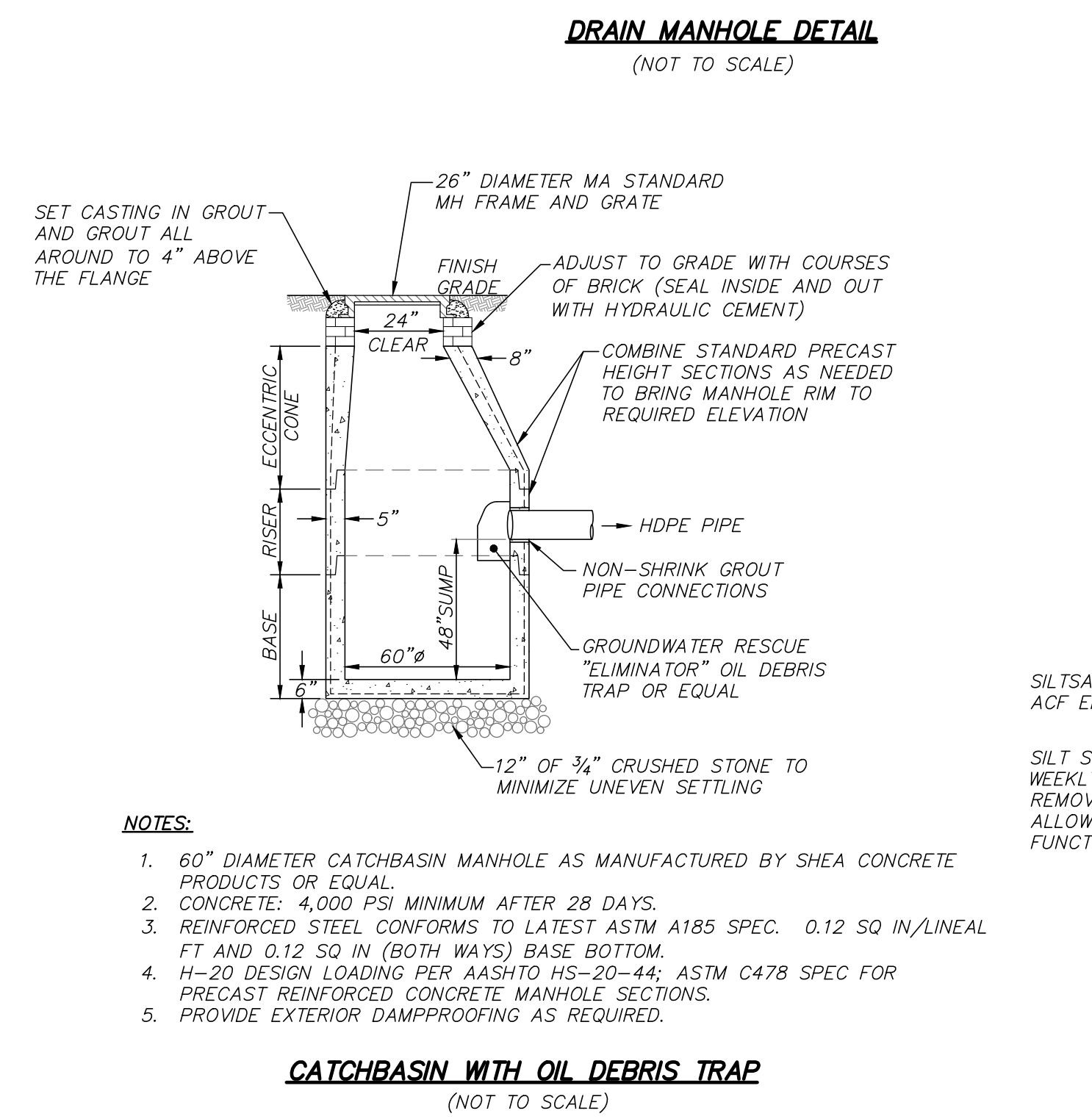
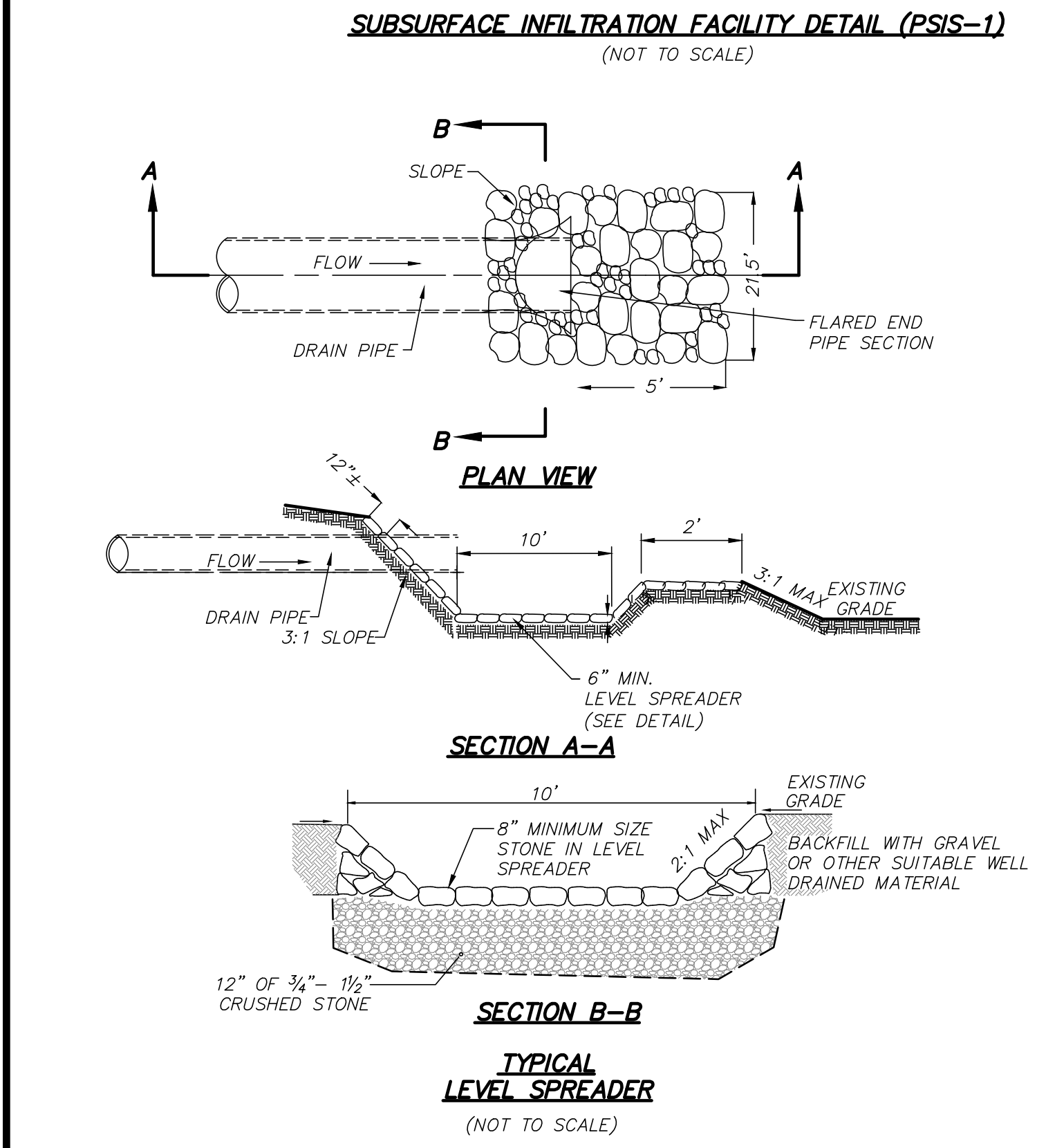
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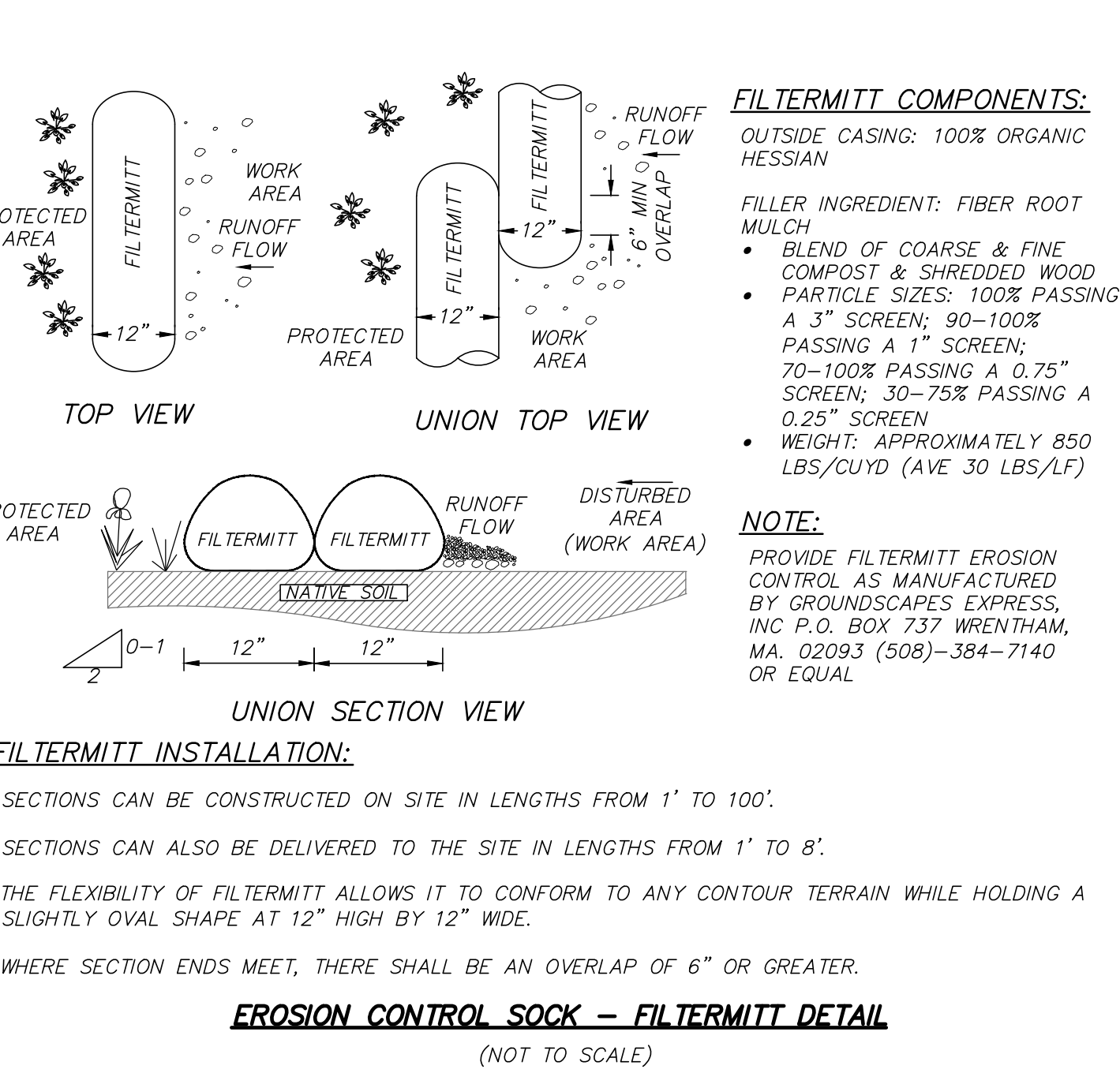
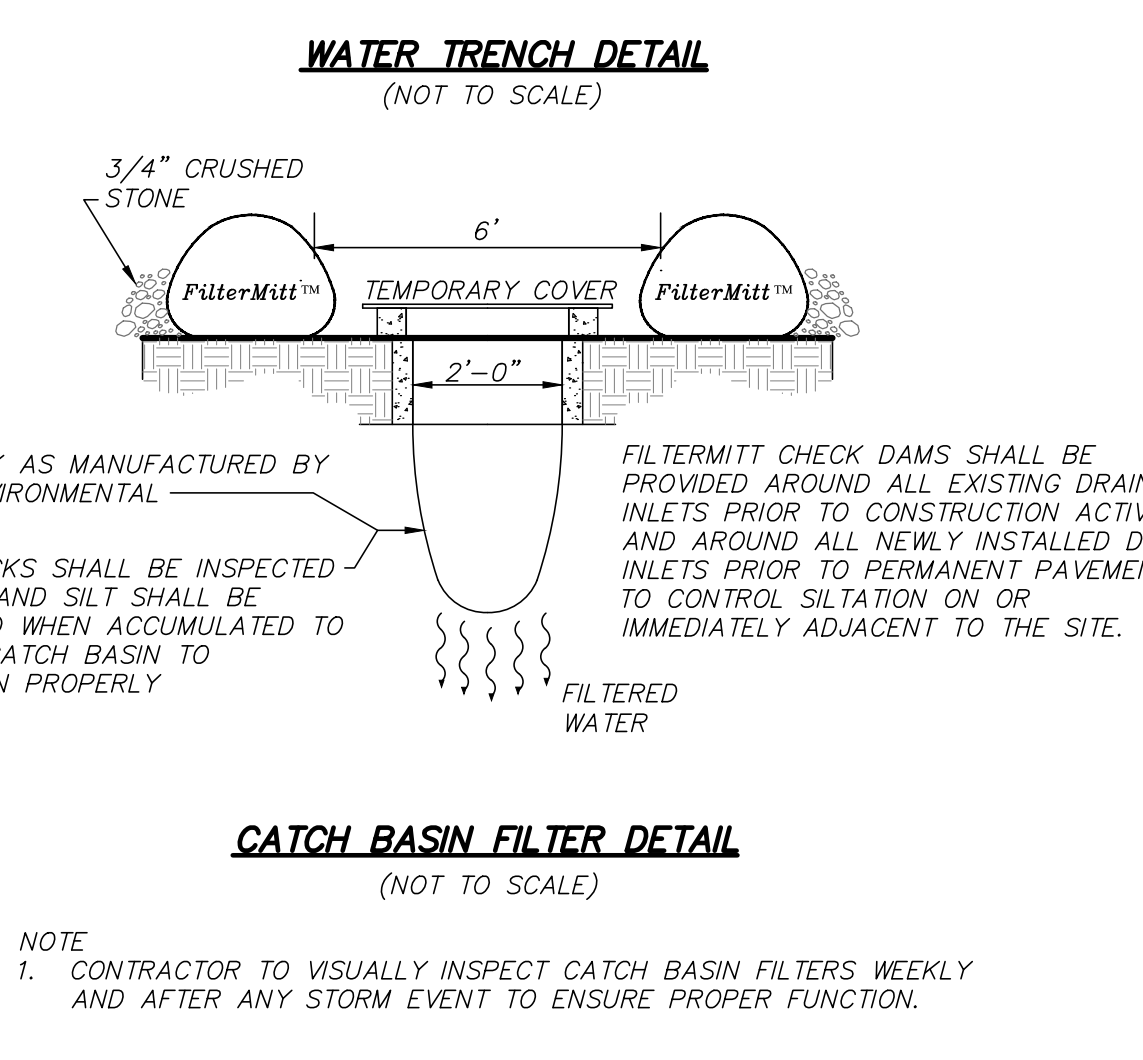
- NOTES:**
1. PROVIDE MINIMUM 2 FEET SEPARATION BETWEEN BOTTOM OF THE 6 INCH DOUBLE WASHED STONE LAYER AND SEASONAL HIGH GROUNDWATER TABLE.
 2. REMOVE ALL TOPSOIL, SUBSOIL AND UNSUITABLE MATERIAL BENEATH THE INFILTRATION SYSTEM AND WITHIN 5' HORIZONTALLY AND VERTICALLY OF PROPOSED INFILTRATION FACILITIES AND REPLACE WITH SAND CONFORMING TO 310 CMR 15.255(3), MASSACHUSETTS STATE ENVIRONMENTAL CODE, TITLE V.
 3. CULTEC No. 20L POLYETHYLENE LINER TO BE PLACED BENEATH CHAMBERS UTILIZING INTERNAL MANIFOLD ONLY.
 4. A BOTTOM OF BED INSPECTION SHALL BE CONDUCTED PRIOR TO INSTALLATION OF FILTER FABRIC, STONE AND DRAINAGE CHAMBERS. THIS INSPECTION SHALL BE CONDUCTED BY THE DESIGN ENGINEER AND SOILS EVALUATOR, AND SHALL BE WITNESSED BY THE TOWN OF FRANKLIN.

- NOTES:**
1. MANHOLE DESIGN TO LATEST ASTM C478.
 2. REINFORCING STEEL CONFORMS TO LATEST ASTM A 185.
 3. CONCRETE COMPRESSIVE STRENGTH-4,000 PSI @ 28 DAYS.
 4. ONE POUR MONOLITHIC BASE.
 5. WHEN SPECIFIED, MANHOLES WATERPROOF COATED.
 6. STEPS-STEEL REINFORCED COPOLYMER POLYPROPYLENE PLASTIC (P52-PFSL M.A. INDUSTRIES, INC. CONFORMS TO LATEST ASTM C478 PARA-12.
 7. BITUMASTIC BUTYL RUBBER FOR JOINTS CONFORM TO LATEST ASTM C443 AND FED SS-5-201A SPEC.

- NOTES:**
1. PROVIDE CDS 2015-4-C AS MANUFACTURED BY CONTECH ENGINEERED SOLUTIONS OR EQUAL.
 2. UNIT CAN BE CONFIGURED WITH GRATED COVER AS NECESSARY.



- NOTES:**
1. 60" DIAMETER CATCHBASIN MANHOLE AS MANUFACTURED BY SHEA CONCRETE PRODUCTS OR EQUAL.
 2. CONCRETE: 4,000 PSI MINIMUM AFTER 28 DAYS.
 3. REINFORCED STEEL CONFORMS TO LATEST ASTM A185 SPEC. 0.12 SQ IN./LINEAL FT AND 0.12 SQ IN. (BOTH WAYS) BASE BOTTOM.
 4. H-20 DESIGN LOADING PER AASHTO HS-20-44; ASTM C478 SPEC FOR PRECAST REINFORCED CONCRETE MANHOLE SECTIONS.
 5. PROVIDE EXTERIOR DAMPPROOFING AS REQUIRED.



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SUITE 300 CHICAGO, IL 60603
312/933.2701

PHARMACANN
NEW CONSTRUCTION OF
RETAIL CANNABIS
DISPENSARY
164 GROVE STREET
FRANKLIN, MA 02038

ISSUED FOR PERMITTING
ONLY NOT FOR
CONSTRUCTION

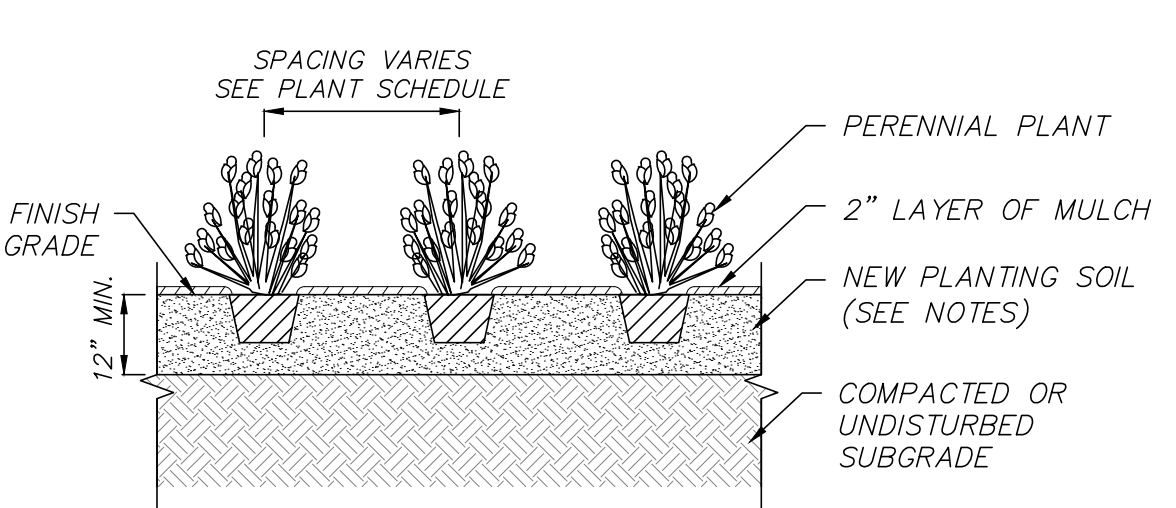
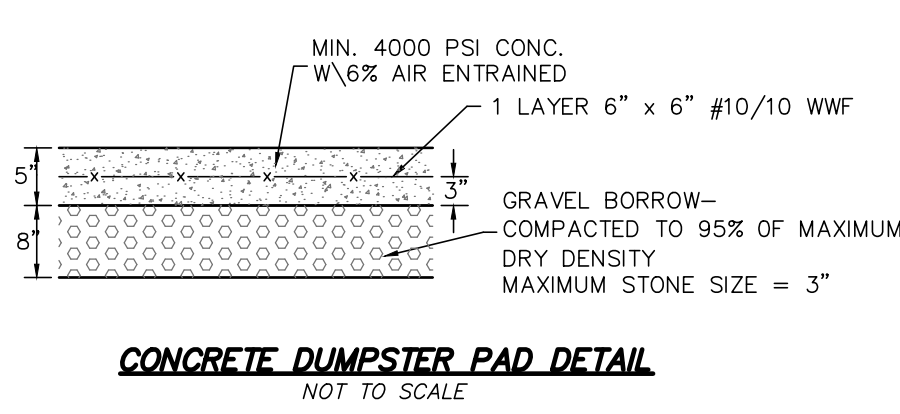
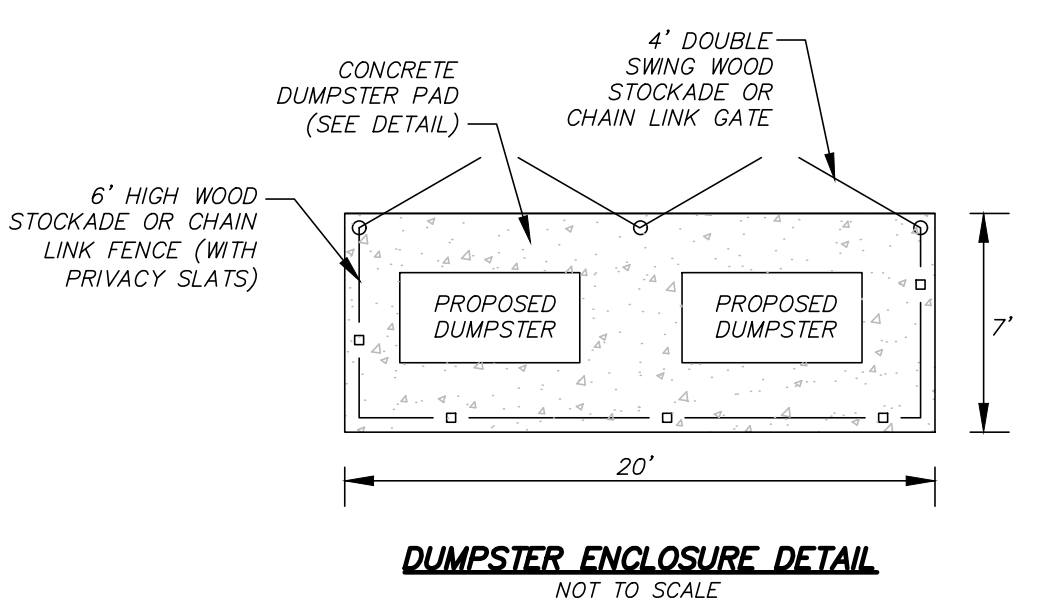
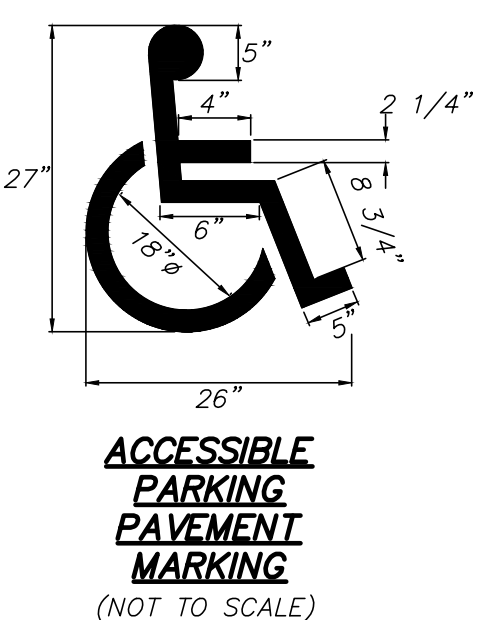
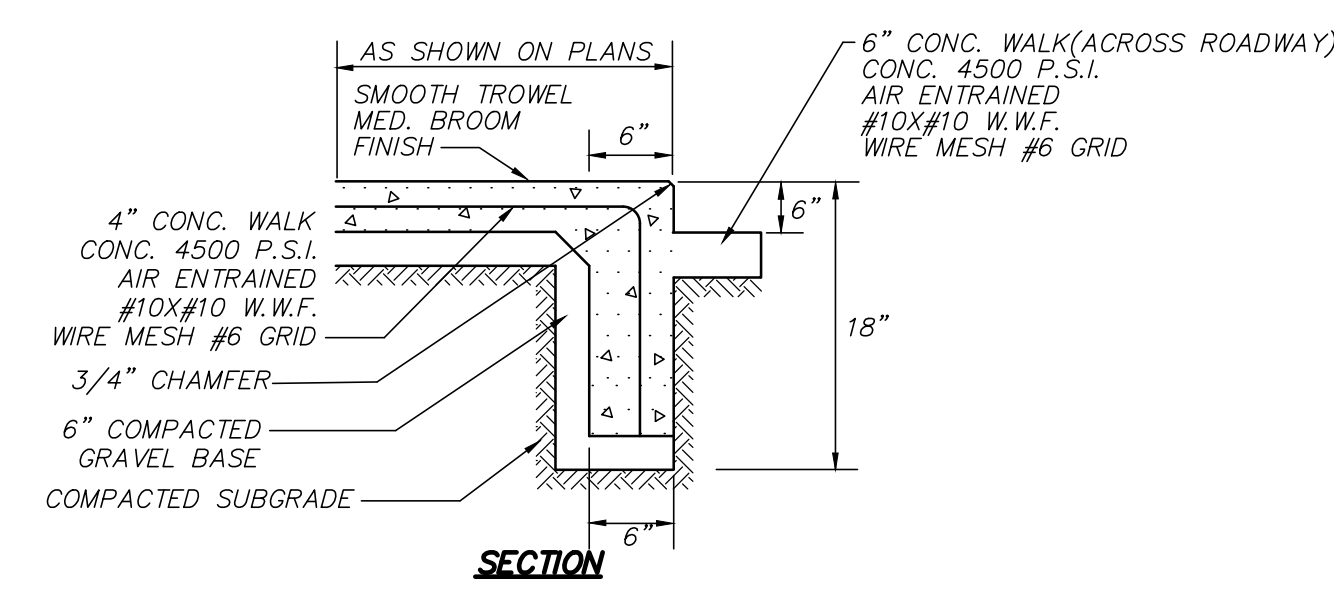
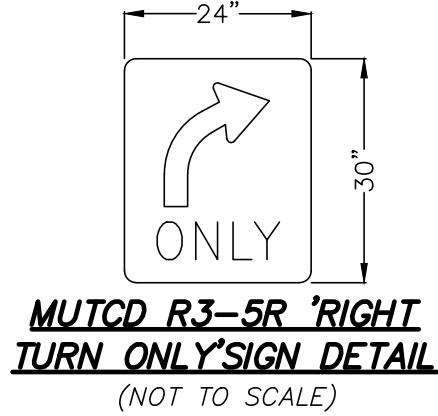
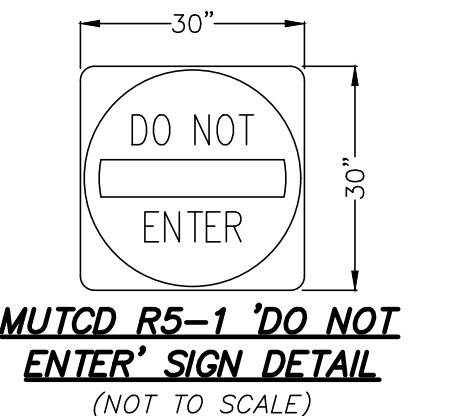
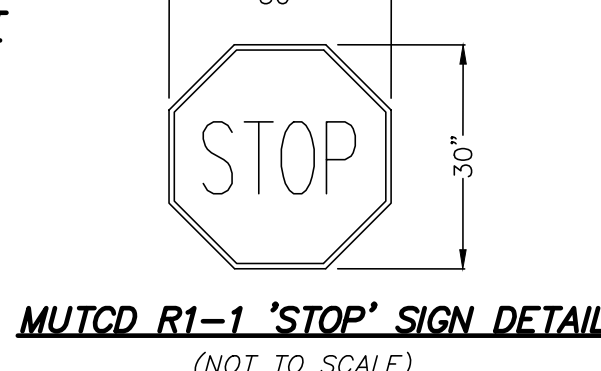
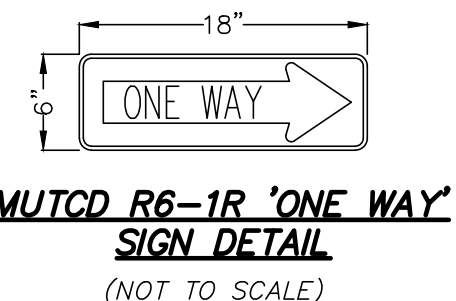
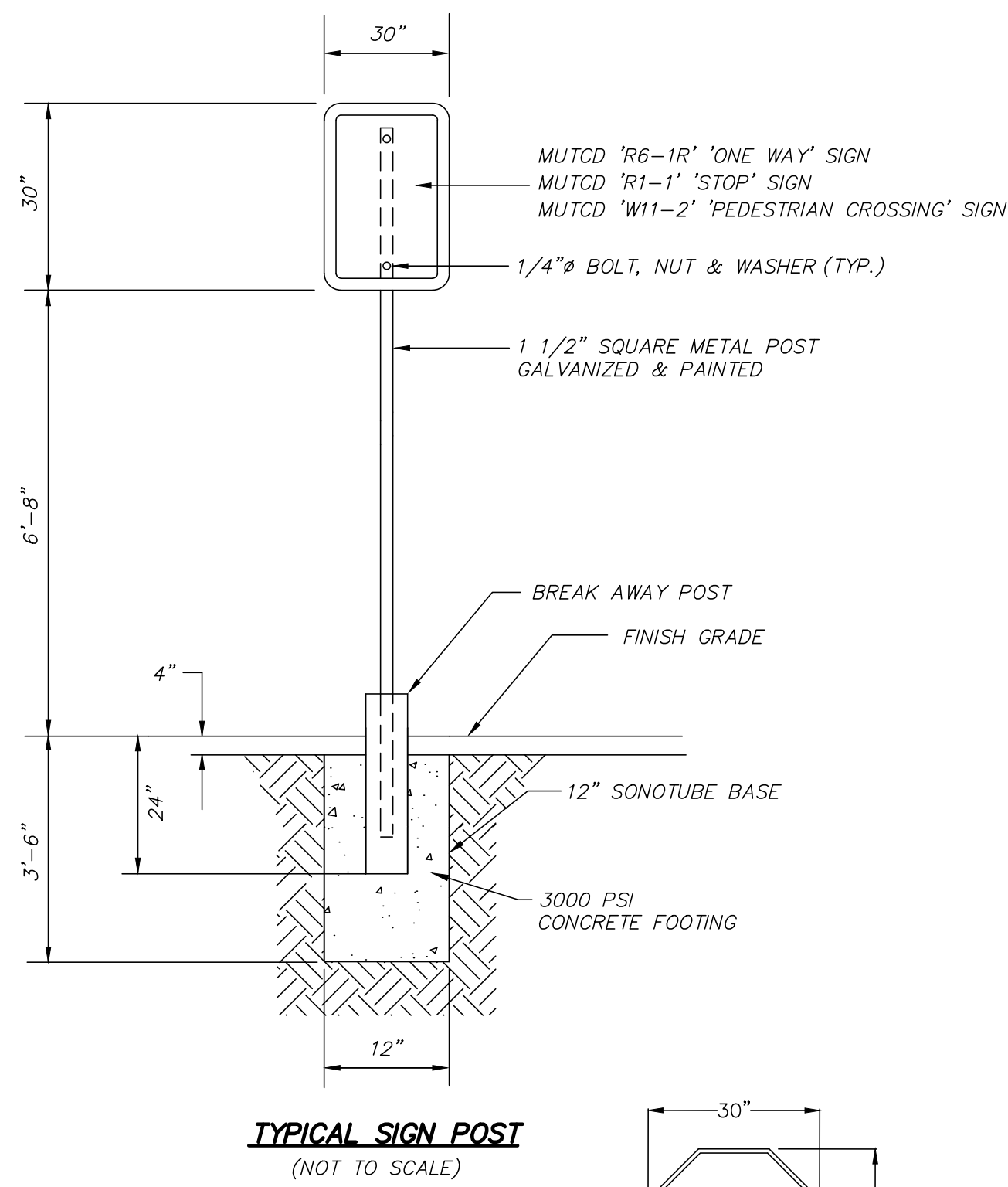
NO.	DESCRIPTION	DATE
1	PLANNING BOARD REVIEW COMMENTS	08/20/2020
2	CONSERVATION COMMISSION FILING	08/28/2020
3	PLANNING BOARD REVIEW COMMENTS	09/16/2020

DATE	DESCRIPTION	DATE
05/08/2020	AS INDICATED	
	NB	
	DK	
	6120-2	

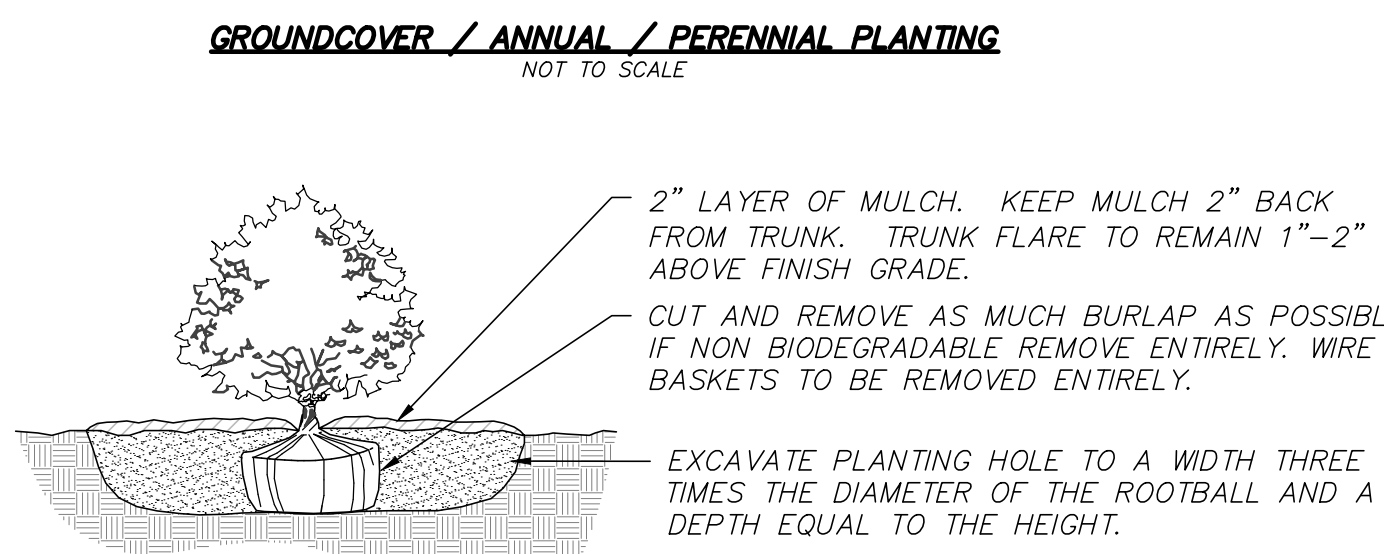
SEAL
DAVID S. KELLEY
CIVIL
NO. 49389
09/18/2020

SITE DETAILS
C5.0
DWG. No. 6120-DET.DWG © 2020 INTERFORM ARCHITECTURE + DESIGN

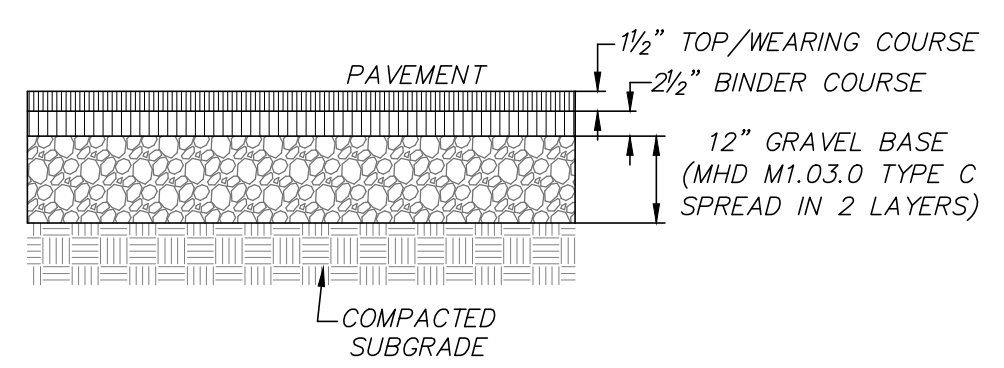
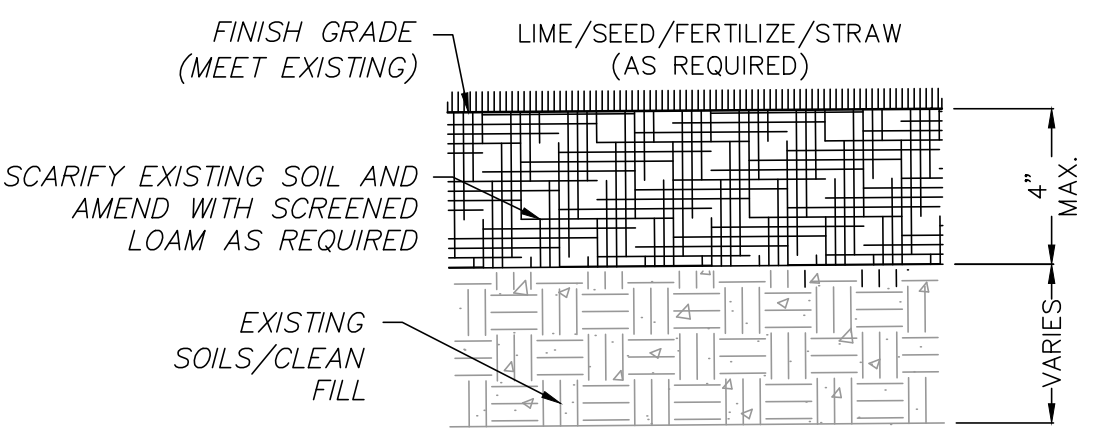
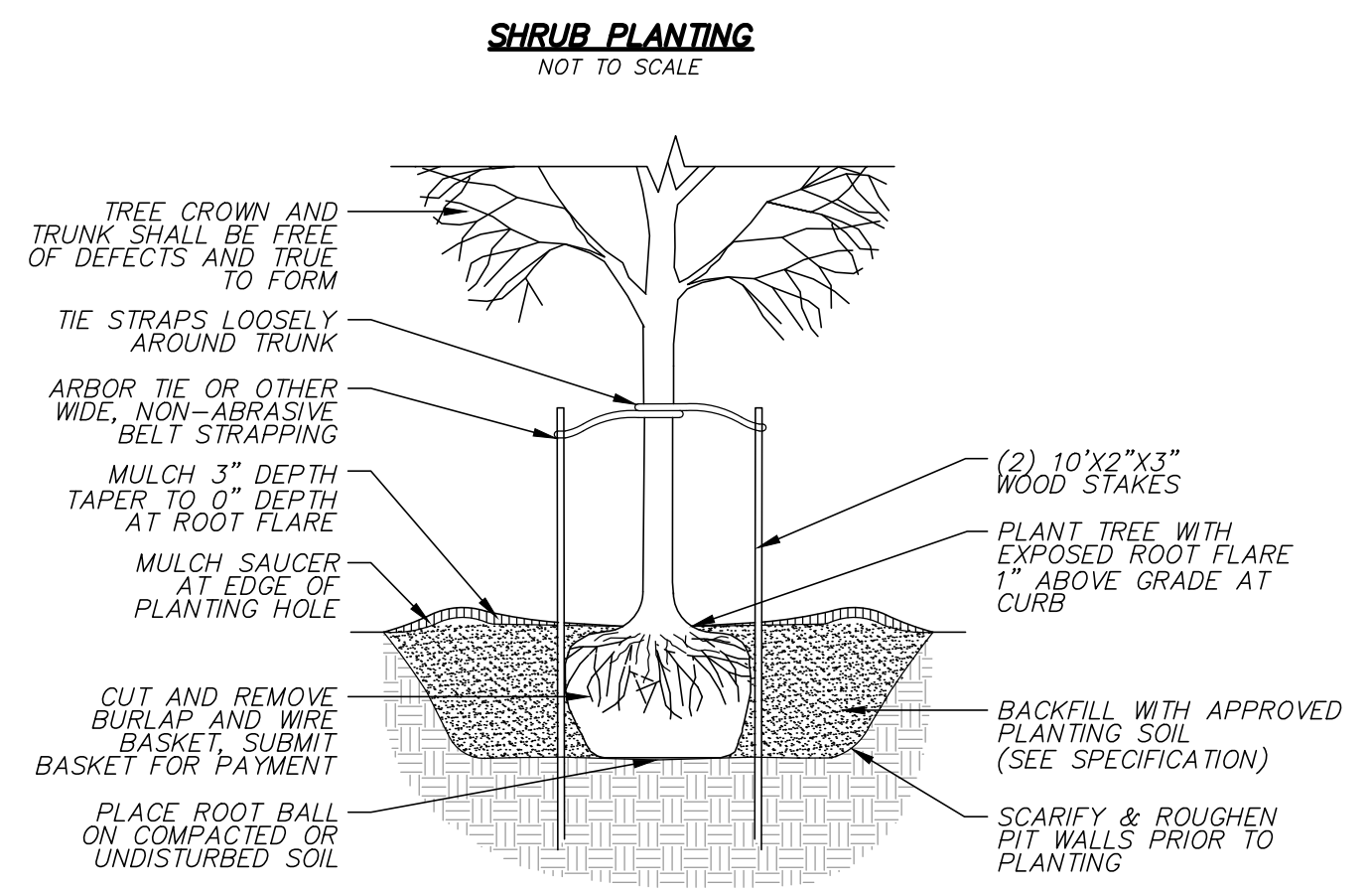
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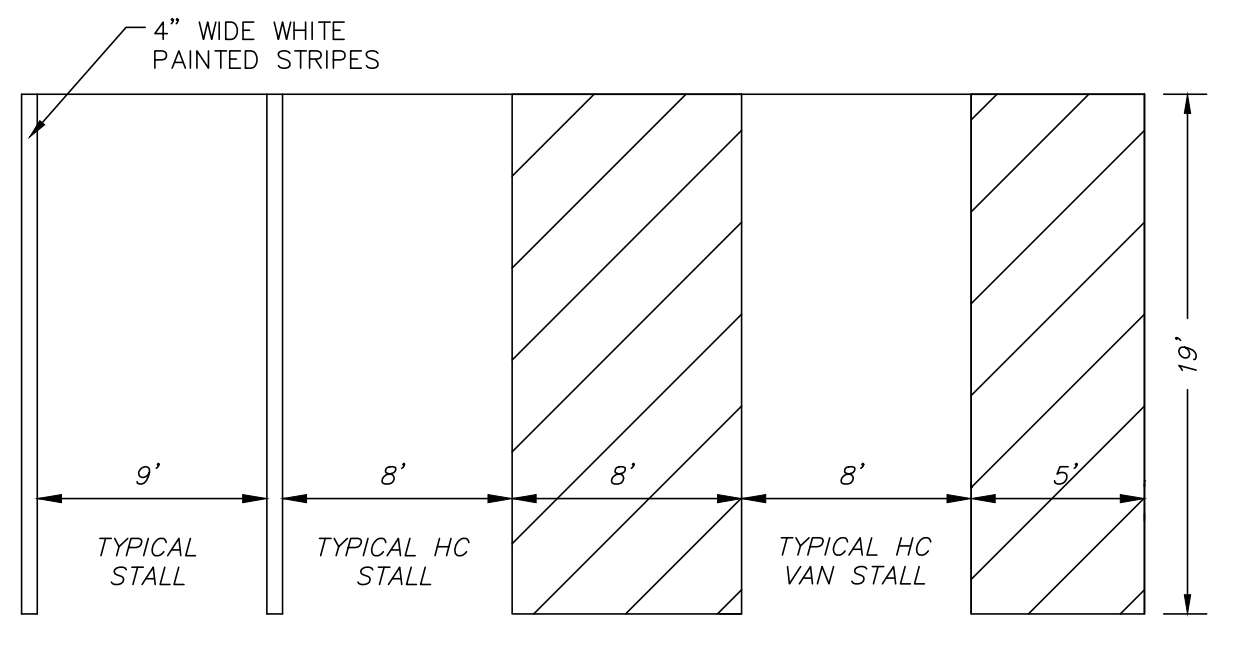
- NOTE:
1. SEE LANDSCAPE NOTES FOR ADDITIONAL PLANTING REQUIREMENTS.
2. SPACE PLANTS EQUALLY TO PROVIDE CONSISTENT COVER OVER INDICATED PLANTING BED.
3. ALL ROOT BALLS TO BE SCARIFIED PRIOR TO BACKFILLING.



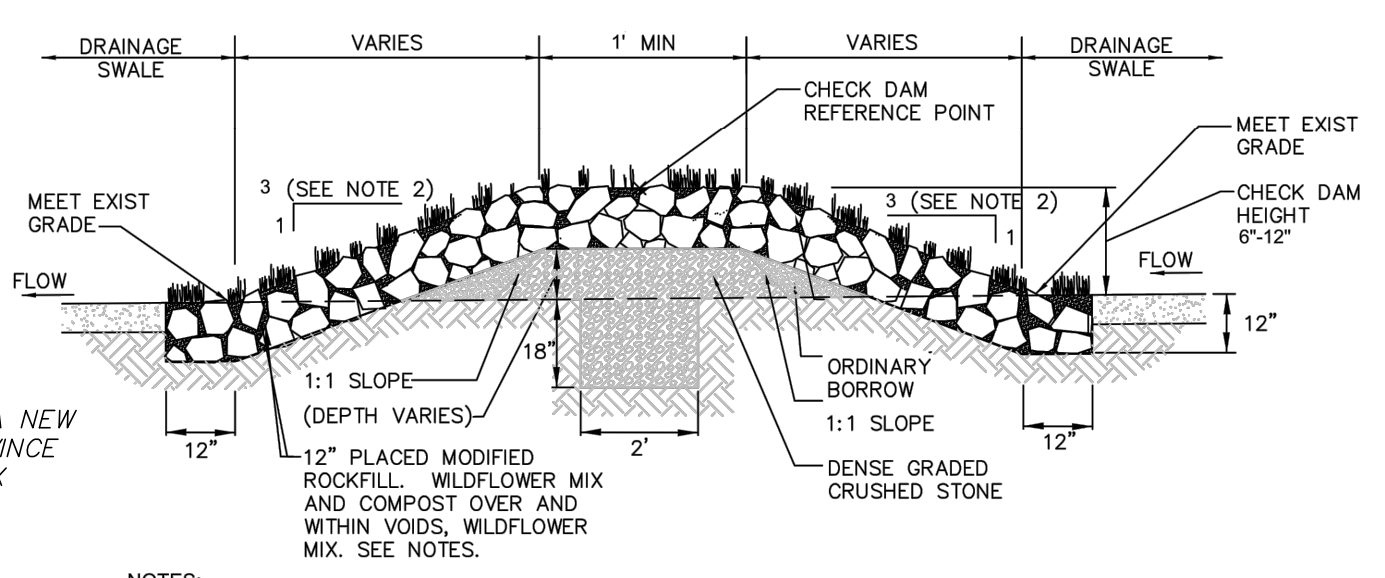
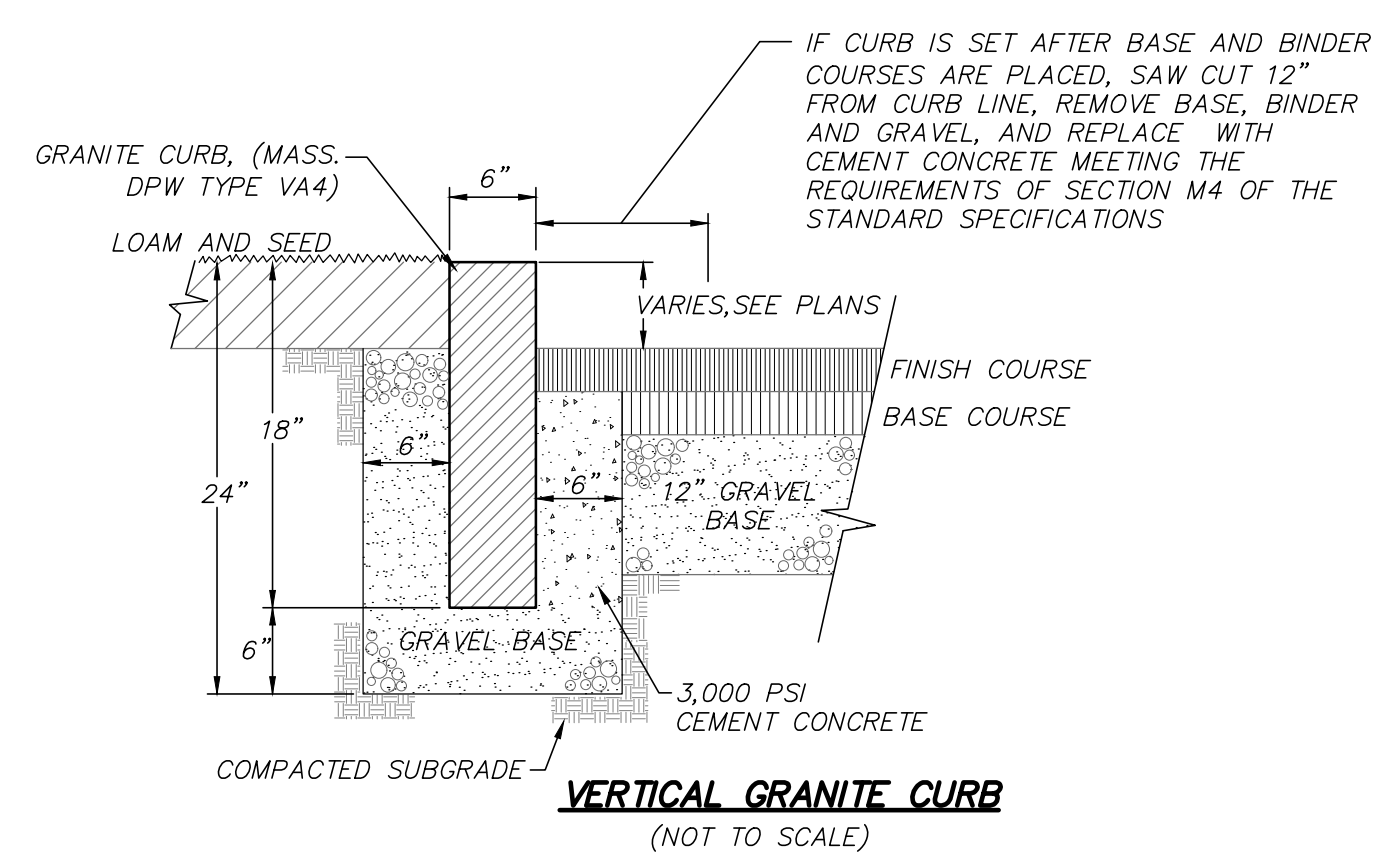
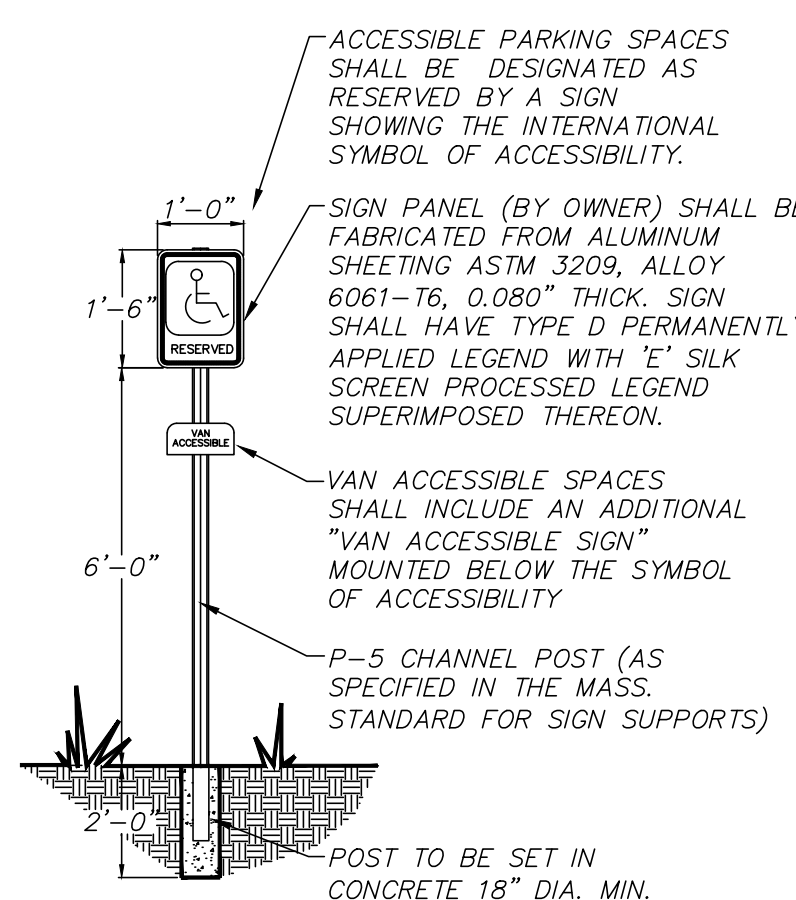
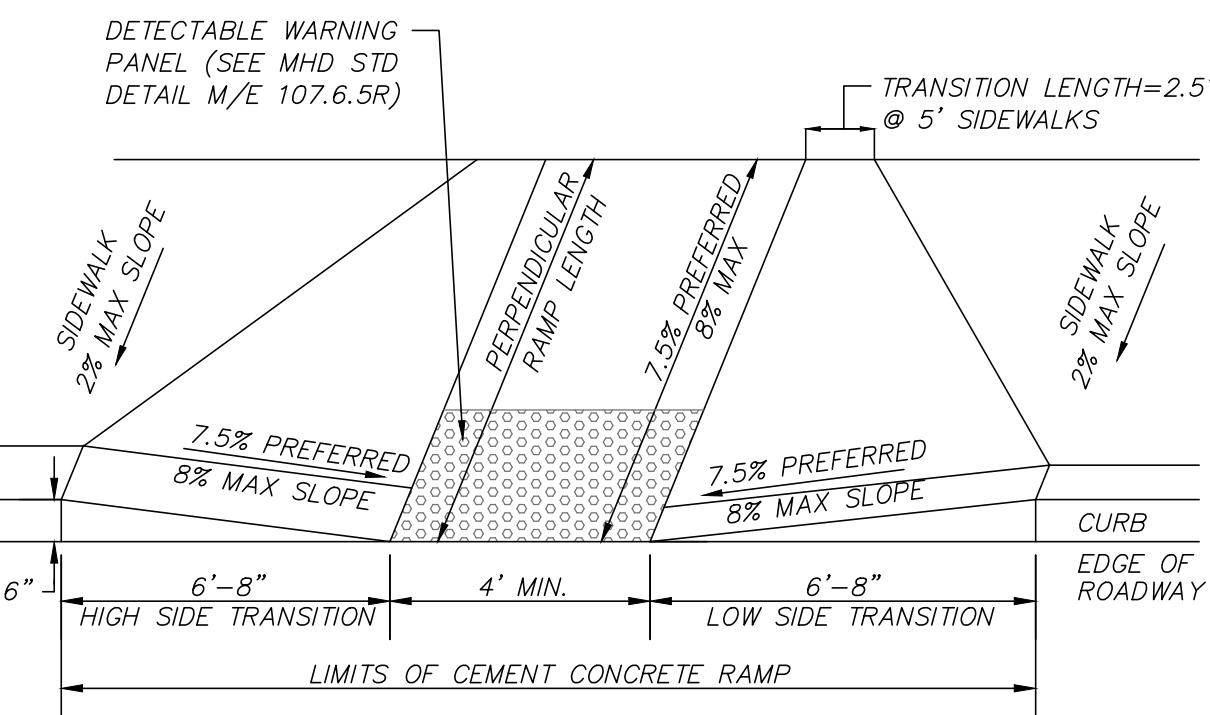
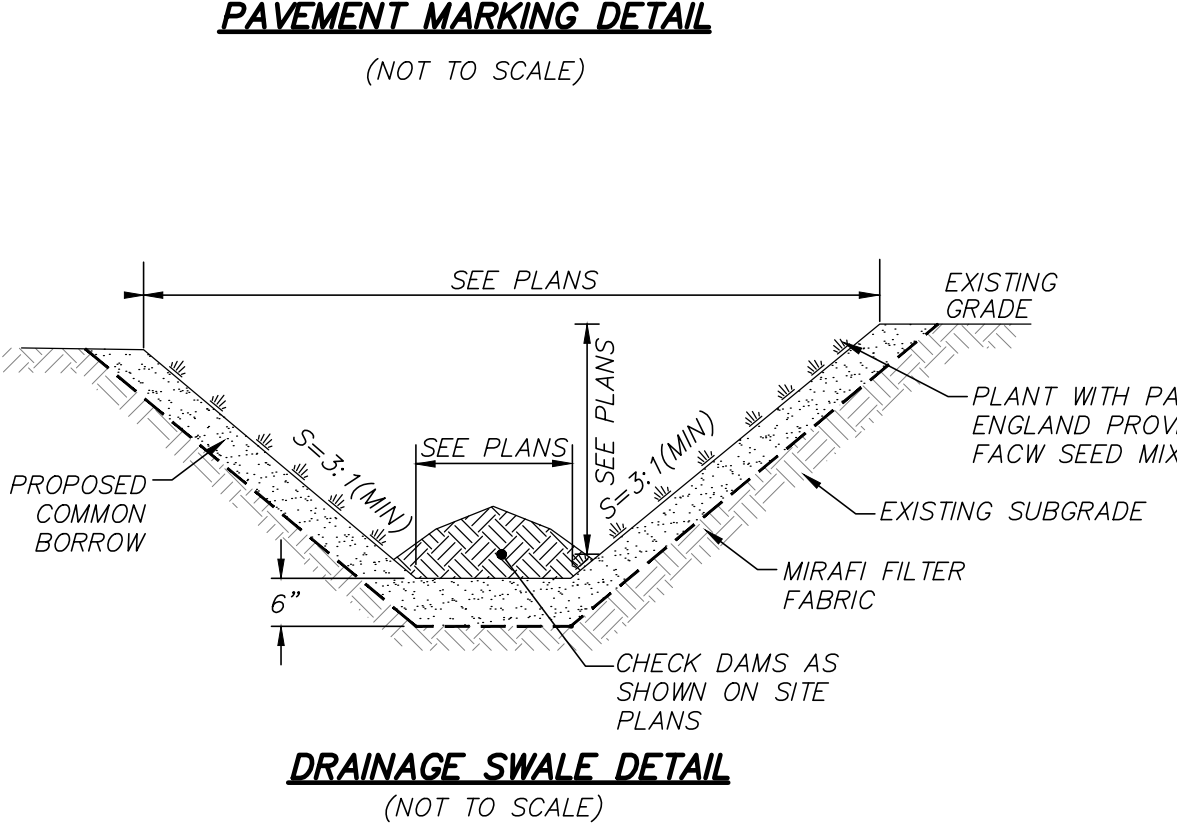
- NOTES:
1. BACKFILL PLANTING HOLE WITH NEW PLANTING SOIL.
2. BACKFILL HALF THE SOIL AND WATER TO SETTLE OUT AIR POCKETS, COMPLETE BACKFILLING AND REPEAT WATERING.
3. IF ROOTS ARE CIRCLING THE ROOTBALL EXTERIOR, CUT ROOTS VERTICALLY IN SEVERAL PLACES PRIOR TO PLANTING.



NOTE:
THE WEARING AND BINDER COURSES SHALL CONSIST OF CLASS I TYPE I-1 BITUMINOUS CONCRETE (HOT MIX ASPHALT)



NOTE: FOR LOCATION AND DIMENSIONS OF ALL PAVEMENT STRIPING, SEE SITE PLAN



- NOTES:
1. CONTRACTOR SHALL FIELD VERIFY THAT THE DIFFERENCE BETWEEN CHECK DAM REFERENCE POINT ELEVATION AND ADJACENT ROADWAY ELEVATION IS NOT LESS THAN 1-FOOT. IF FIELD CONDITIONS DO NOT RESULT IN 1-FOOT ELEVATION DIFFERENCE BETWEEN PROVIDED CHECK DAM REFERENCE POINT AND ADJACENT ROADWAY ELEVATION AT EDGE OF PAVEMENT, CONTRACTOR SHALL NOTIFY ENGINEER.
2. 6:1 SLOPE. STEEPER TRAVERSE SLOPES ARE PERMISSIBLE (3:1 MAXIMUM) WITH LONGITUDINAL ROADSIDE BARRIERS, AREAS OUTSIDE THE CLEAR ZONE, OR ON LOW SPEED FACILITIES.
3. APPLY COMPOST MATERIAL OVER AND WITHIN THE MODIFIED ROCKFILL. MATERIAL SHOULD BE PLACED SO THAT SETTLED MATERIAL IS AT OR SLIGHTLY BELOW SURFACE PLANE OF STONE AND SHALL BE WORKED INTO THE VOIDS OF THE MODIFIED ROCK FILL. COMPOST SHALL BE RAKED BY HAND.
4. SEED OVER COMPOST SHOULD BE PA NEW ENGLAND PROVINCE FACW SEED MIX.

MERIDIAN ASSOCIATES
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69 MILK STREET, SUITE 302
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DATE 05/08/2020
SCALE AS INDICATED
DRAWN NB
CHECKED DK
PROJECT NO. 6120-2




SEAL
COMMONWEALTH OF MASSACHUSETTS
DAVID S. KELLEY
NO. 49389
REGISTERED PROFESSIONAL ENGINEER
09/18/2020

SITE DETAILS

C5.1

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Schedule

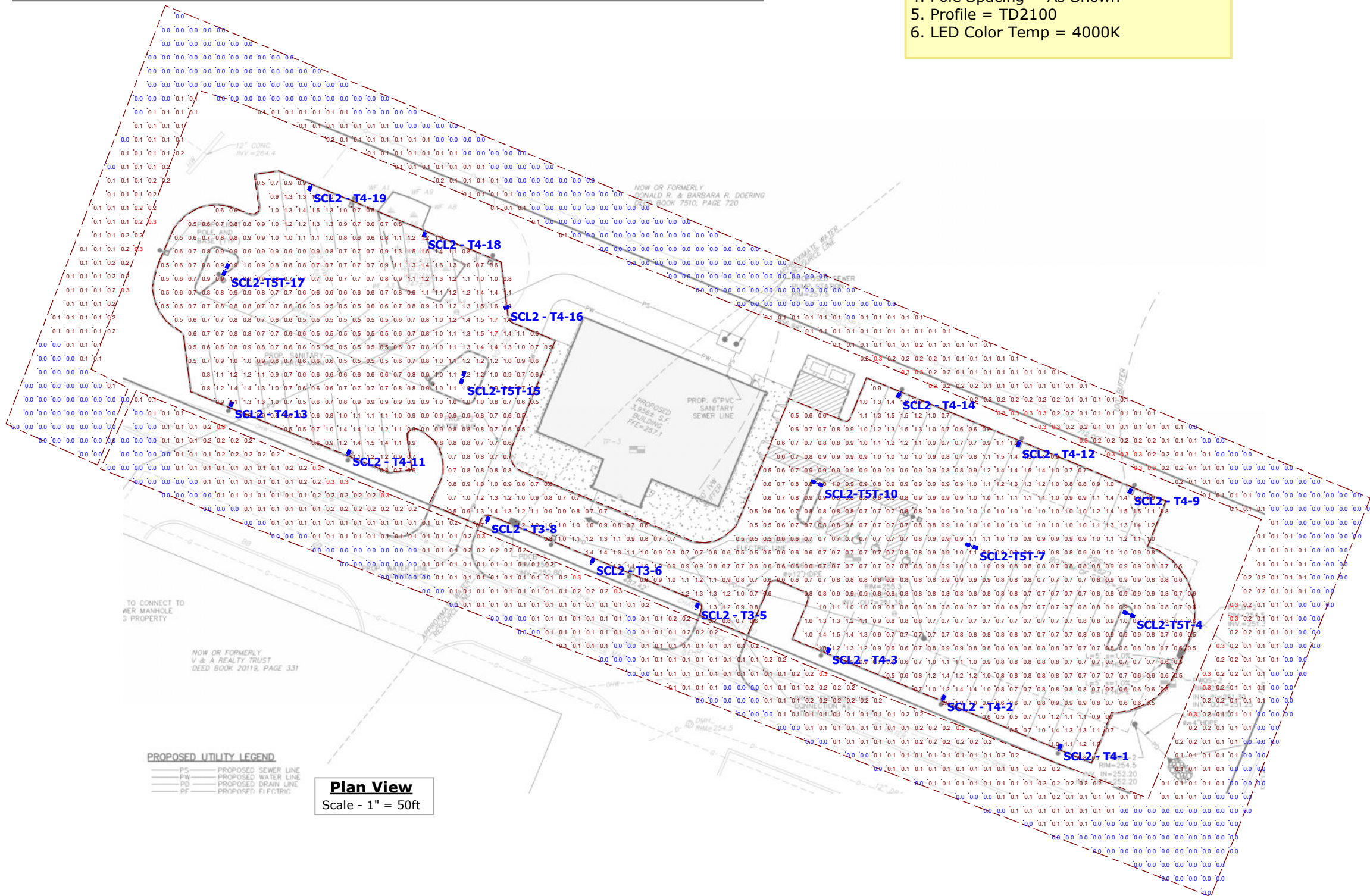
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	SCL2 - T4	11	First Light Technologies	0.9	2175	0
	SCL2 - T3	3	First Light Technologies	0.9	2150	0
	SCL2-T5T	5	First Light Technologies	0.9	2325	0

Statistics

Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Parking Lot	+	0.9 fc	1.7 fc	0.5 fc	3.4:1	1.8:1
Property Line	+	0.1 fc	0.3 fc	0.0 fc	N/A	N/A

Note

1. Mounting Height = 20 ft
2. Calculation zone = Ground
3. Grid Spacing = 6ft
4. Pole Spacing = As Shown
5. Profile = TD2100
6. LED Color Temp = 4000K



**Pharmacann Dispensary
SCL2 Series Lights
First Light Technologies Ltd.**

Designer

I Gillies

Date

2020-08-20

Scale

Not to Scale

Drawing No.

PD082020

Approved by

Carla M. Moynihan
617.646.2043
cmmoynihan@sherin.com
029783.00002

September 18, 2020

VIA EMAIL AND OVERNIGHT MAIL DELIVERY

Franklin Planning Board
c/o Department of Public Works Building
257 Fisher Street
Franklin, MA 02038
Attention: Anthony Padula, Chair

Re: Supplemental Filings for Pending Special Permits/Site Plan Application
164 Grove Street, Franklin, MA

Chair Padula,

My office represents NLCP 164 Grove Street MA LLC, a Massachusetts limited liability company (“**Owner**”), the property owner of the approximately 1.5 acre parcel of vacant land located at 164 Grove Street, Franklin, Massachusetts 02038 (Map 306, Lot 4) (the “**Property**”) and PharmaCannis Massachusetts Inc., a Massachusetts corporation (“**Licensee**”). This letter is a follow up to our prior letters with submission materials to the Town of Franklin Planning Board (the “**Board**”) dated June 29, 2020 and August 21, 2020 regarding certain special permits and site plan approval pertaining to the proposed co-location of the Non-Medical Marijuana Establishment and Medical Marijuana Treatment Facility at the Property, the use of a common driveway for access from Grove Street for more than 2 lots and special considerations as to impervious surface coverage due to the Property’s location within the Water Resource Overlay District. The public hearing was opened on July 27, 2020 during which the Applicant provided an initial presentation, continued to August 24, 2020 at which the Applicant requested a further continuance without presentation, continued to September 14, 2020 during which the Applicant provided a substantive presentation responding to Board, staff and peer review comments and has been continued to the Board’s next meeting on September 28, 2020.

The following documents are provided in support of the Proposed Project as submitted in electronic form as well as hard copy:

1. Site Plans, prepared by Meridian Associates, dated May 8, 2020, as revised August 20, 2020, as revised August 28, 2020, as revised September 16, 2020 consisting of 10 sheets
 - a. 5 sets of 11” x 17” prints of site plan set
 - b. 2 sets of 24” x 36” prints of site plan set (folded)
2. Stormwater Analysis and Calculations Report, prepared by Meridian Associates, dated May 8, 2020, as revised August 20, 2020, as further revised September 16, 2020 – 1 copy

Franklin Planning Board
September 18, 2020
Page - 2 -

3. Response Letter to BETA prepared by Meridian Associates, dated August 20, 2020, as updated September 16, 2020 – 1 copy
4. Response Letter to BETA prepared by GTI, dated September 17, 2020 – 1 copy
5. Updated Operational Plan – 5 color copies

In further response to Planning Board members' comments, with respect to the updated Operational Plan included with this filing, we have (i) clarified our Operating Hours along with Peak days/times as well as noting Non-Peak times; (ii) indicated 20-parking spots for employees; and (iii) committed to operating by reservation only for the initial thirty (30) after which we would seek review of same at the next scheduled Planning Board meeting.

For the above reasons, together with the prior letters, submission materials and testimony provided during public hearings, the Board should grant Owner and Licensee's requests for the Site Plan and the Special Permits for the Proposed Project. Please do not hesitate to reach out should you have any questions.

Sincerely



Carla M. Moynihan

Enclosures

cc: Bryan Taberner, Director Planning & Community Development (btaberner@franklinma.gov)
Amy Love, Town Planner (alove@franklinma.gov)
Matthew Crowley, P.E., Town Project Manager (MCrowley@BETA-Inc.com & overnight)
Jaklyn Centracchio, BETA Inc. (JCentracchio@BETA-Inc.com)
Andrew Bradford, PharmaCann LLC
Shelley Stormo, PharmaCann LLC



September 16, 2020

Mr. Anthony Padula, Chairman
Franklin Planning Board
355 East Central Street
Franklin, MA 02038

**Re: 164 Grove Street
Site Plan Peer Review**

Dear Mr. Padula & Members of the Board:

Meridian Associates, Inc. (MAI) has received the peer review prepared by BETA Group, Inc. dated September 3, 2020 regarding the Site Plan Review submission and offer the following responses:

GENERAL COMMENTS

G1. Provide detail for proposed dumpster pad and enclosure (with screening). *MAI: A detail for the dumpster pad and enclosure has been added to the plan set, see Sheet C 5.1.* BETA2: Details provided. BETA recommends that slats are provided for the chain link option, which is typically required by the Board.

MAI Response: Privacy slats have been added to the Dumpster Enclosure Detail.

G2. Confirm access rights and utility easements are being acquired from the adjacent property to the south. *MAI: Yes. We are in active discussions and negotiations with owner representative for Core Real Estate Holdings of 166 Grove Street as to mutually acceptable business terms and conditions to acquire the access rights and utility easements for the 164 Grove Street Project including the ability to address any improvements required to the access way by the Planning Board in connection with its review an consideration of the Special Permit for Shared Common Driveway. Attached are copies of the Deed into Core Real Estate Holdings as well as the existing Easement Agreement and plan between the owners of 166 Grove Street and 168 Grove Street concerning similar access and utility easements.* BETA2: Information provided. BETA defers to the preference of the Board to require rights/easements as a condition of approval.

MAI Response: MAI concurs, we are requesting that the Board require rights/easements as a condition of approval. To date, the Applicant has reached agreement on business terms and conditions for the grant of easements for the shared common driveway and utility connections from the 166 Grove Street and 168 Grove Street property owners.

G3. Clarify the disposition of the existing fences and gate surrounding the property. *MAI: The existing fence around the perimeter of the site, that is located within the property lines, is to be removed. Refer to Sheet C 1.0.* BETA2: Clarification provided. It is anticipated that any fence removal outside of the property line will be coordinate with the ongoing access and easement negotiations – issue resolved.



- G4. Recommend revising snow storage areas to maintain clear flow path within swale along the northerly property line. Consider providing additional snow storage along the southerly curb line. *MAI: The snow storage locations have been adjusted accordingly, refer to Sheet C 4.0.*
BETA2: Snow storage area revised – issue resolved.

MAI Response: No response required.

- G5. Provide a note to indicate that tree species shall be from the Town of Franklin Best Development Practices Guidebook. Also confirm the proposed plantings meet this requirement.
BETA2: No response provided – issue remains outstanding.

MAI Response: A note has been added to the landscaping plan. Additionally, the tree species have been updated and now specify trees that are listed in the Town of Franklin Best Development Practices Guidebook.

ZONING

The Site is located within the Industrial (I) Zoning District and the Marijuana Use Overlay District. The proposed use of the Site is identified as Non-Medical Marijuana Retail Establishment. The proposed uses are allowed in the District via a Special Permit from the Planning Board.

Schedule of Lot, Area, Frontage, Yard and Height Requirements (§185 Attachment 9)

The project site will meet the requirements for lot area, frontage, lot depth, yards, height, and impervious coverage. The project does not meet the requirements for lot width; however, per §185-3 Lot Width C.(2) any lot shown on a recorded plan prior to May 21, 1998 is exempt from this definition. The Quitclaim Deed provided as part of the submission documents indicates the subject parcel is depicted on a plan of land recorded in the Norfolk Registry of Deeds, dated August 25, 1987 and is therefore exempt.

Parking, Loading and Driveway Requirements (§185-21)

The existing Site includes one access driveway from Grove Street to the west. The project proposes to remove this access route and construct two new paved access driveways (1 entrance, 1 exit) from the 166 Grove Street site to the south.

Section §185-21.B.(3) describes the number of parking spaces required for residential and nonresidential buildings in the Industrial Zoning District. The required parking for a retail use is one space per 200 sq. ft. of gross floor area plus one space per separate enterprise. For the proposed 4,150 sq. ft. building, the required parking is thus 21 spaces and a total of 66 spaces are proposed. With the understanding that retail marijuana uses have specific parking demands, additional commentary will be provided as part of the Traffic Review, to be provided under separate cover.

Proposed 90° parking spaces are depicted as 19' long and 9' wide. Proposed angled (60°) parking spaces are 18' long (usable stall) and 9' wide. Access route widths vary between 16 ft. and 24 ft, and all driveways are designated to be one-way. In accordance with Massachusetts Architectural Access Board



(MAAB) requirements, four parking spaces have been designed to be handicap accessible, two of which are also van accessible.

In compliance with §185-21.C.(5), one tree must border the parking lot per every 10 parking spaces. A total of 31 trees, supplemented by shrubs, are proposed in the vicinity of the parking lot.

- P1. The angled parking layout conforms to industry standards; however, the usable stall length is only 18 feet. Revise the usable stall length to be 19 feet §185-21.C.(9)(a). *MAI: The length of the angled parking spaces has been revised accordingly, refer to Sheet C 2.0.* BETA2: Stall length revised – issue resolved.

MAI Response: No response required.

- P2. The accessible route is located within the 24' driveway aisle and vehicles backing out of spaces will encroach into the striped walkway. Evaluate alternatives to eliminate pedestrian/vehicle conflicts. *MAI: The location of the accessible route from the parking spaces to the building was chosen as it provides the most visibility for drivers while circulating through the parking lot. Additionally, the drive aisle width in this location is twenty-four (24) feet wide thus providing a nineteen (19) foot wide aisle for vehicles in which to safely travel throughout the parking lot.* BETA2: BETA notes that while the location of the accessible route is not ideal, there does not appear to be a practicable solution that does not require significant redesign of the site.

MAI Response: No response required.

- P3. Clarify if additional parking/site layouts have been evaluated, such as relocating the proposed building to the west end of the site and providing a continuous parking area. The current layout requires vehicles to circulate in a “figure 8” pattern with a number of vehicle conflict points. *MAI: Many layouts for the site were considered. Ultimately the layout selected was preferred to move any potential traffic congestion away from Grove Street. Parking count was maximized beyond the minimum requirements to help avoid customers waiting for parking spots, and it was preferable to avoid one large parking lot with long walks for store customers. In addition, the entrance and exits are aligned with the existing curb cuts on the southern side of the access drive.* BETA2: Information provided – refer to comment P4.

MAI Response: No response required.

- P4. Provide turning movements on Site Plan to demonstrate that passenger, delivery, and waste collection vehicles can safely maneuver throughout the site. It is anticipated that the Fire Chief will review turning movements for fire apparatus throughout the site. *MAI: A turning monument sketch has been provided and is submitted as a part of this comment response letter.* BETA2: Also provide a turning movement for the passenger vehicle making a right-hand turn into and around the easterly parking area to demonstrate there will be no conflicts with the other passenger vehicle movements at the entrance. BETA also recommends to evaluate if the waste collection vehicle can make turns to use the site exit instead of backing into the common driveway.



MAI Response: The additional passenger vehicle turning movement has been added to the Vehicle Movement Plan. It should be noted that the dumpsters use will be small roll away dumpsters and they can be moved to reduce the movement of the truck used to remove the dumpsters.

- P5. Confirm the number of trees provided in the Plant Schedule (31) vs. the Landscape Table (10). *MAI: The number of trees and shrubs depicted on the plans and listed in the plant schedule are consistent.* BETA2: The number of trees provided is adequate – issue dismissed.

Sidewalks (§185-28)

The project is located within the Industrial Zoning District and is not required to provide sidewalks along the street frontage. There are no existing sidewalks on Grove Street in proximity to the project.

Curbing (§185-29)

The project proposes the use of vertical granite curbing along paved areas.

- S11. Clarify limits of vertical granite curb as it relates to the concrete walkway. The Concrete Walkway Detail depicts monolithic concrete curb. *MAI: The limits of the types of curbing have been clarified, refer to Sheet C 2.0.* BETA2: Clarification provided – issue resolved.

Site Plan Review (§185-31)

The proposed development is subject to Site Plan Review and must comply with the requirements of this section.

- S1. Include abutting land uses and zoning information on the Locus Map (§185-31.C.(3)(d)). *MAI: The abutting land uses have been added to the plan set, refer to sheet C0.0.* BETA2: Abutting land uses provided and it is understood that all abutting parcels are zoned as Industrial – issue resolved.
- S2. Provide photometric plan (§185-31.C.(3)(l)). *MAI: A photometric plan has been added to the plan set, refer to sheet 6.0.* BETA2: Plan provided indicating adequate illumination will be provided for safety and security. Expand limits of analysis to demonstrate there will be no nuisance or excessive light spillage onto adjacent properties in accordance with site plan and special permit review criteria.

MAI Response: The photometric plan has been revised to expand the limits of the analysis to demonstrate there is no nuisance or excessive light spillage onto adjacent properties.

- S3. Depict proposed limits of clearing on the plans, as applicable, including areas of existing vegetation to be retained (§185-31.C.(3)(u)). *MAI: The limit of clearing / limit of work is shown on the Site Plan, refer to Sheet C 2.0 of the plan set. It has also been added to Sheet C 1.0.* BETA2: Information provided – issue resolved.



Screening (§185-35)

The project proposes outdoor parking for 10 or more cars, which must be screened from adjacent residential districts or uses from which they would otherwise be visible. The Site is surrounded by lots zoned as Industrial, and it does not appear that the project will be visible from any residential use; therefore, screening is likely unnecessary.

Water Resources District (§185-40)

The Site is partially located within the Water Resources District due to the presence of a Zone II Wellhead Protection Area. This portion of the Site includes the eastern parking lot and the majority of the proposed building.

- WR1. Clarify if the proposed sewer force main will connect to an off-site sewage disposal system or Town Sewer. If necessary, confirm the estimated sewage flow for the existing sewage disposal system will not exceed 110 gallons per 10,000 sq. ft. of lot area if located within the Water Resources District (§185-40.D.(1)(i)). *MAI: The proposed wastewater will be directed to the Town of Franklin public sewer. Per Massachusetts Department of Environmental Protection, Title V design standards, a retail store will produce approximately two hundred (200) gallons of wastewater per day. This assumes that public restrooms are available, however, at this site, the restrooms will not be available to the public so the flows should be far less.* BETA2: Connection to Town sewer confirmed – issue dismissed.
- WR2. Section §185-40.D.(1)(l)(ii) requires that the proposed groundwater recharge efforts must be approved by a hydrogeologist; however, provided that the stormwater management system is revised to fully comply with the Massachusetts Stormwater Management Standards no adverse impacts to groundwater are anticipated as a result of the project. BETA defers to the preference of the Board to require approval by a hydrogeologist. *MAI: BETA2: No further comment.*
- WR3. Note that any fill placed in quantity greater than 15 yards must be certified in accordance with §185-40.E.(5). *MAI: MAI concurs with the above statement.* BETA2: No further comment.
- WR4. In conjunction with comment SW12, it is anticipated that minimal flow is directed from the project site to the paved area in proximity to DP2. BETA notes that to fully comply with (§185-40.E.(4)), all stormwater runoff from impervious surfaces must be recharged unless following consultation with, and approval from the Conservation Commission and the Building Inspector that recharge is determined to be infeasible. *MAI: This project will be submitted to the Conservation Commission for review and approval. Runoff from the impervious area that connects the site to the existing access road is di minimus in scale and should not have any adverse impacts to the adjacent properties. This is reflected in the stormwater calculations. Note that runoff from all of the other impervious surfaces is directed to an infiltration system that provides ground water recharge.* BETA2: Information provided – issue dismissed.

Utilities

Proposed utilities include drainage, electric, sanitary sewer, and domestic water services. Detailed review of water and sewer utilities is anticipated to be provided by the DPW and Fire Chief (e.g. for fire hydrants), as applicable.



- U1. Provide a note that all water and sewer utility installations shall be done in accordance with the Town of Franklin Department of Public Works Standards for Sewer and Water Materials and Installation (Town Standards). Also note that where utility installation details conflict with the Town Standards that the Town Standards shall govern. *MAI: The above requested note has been added to the plan set, refer to Sheets C 2.0 and C 3.0. Notes have been added that show where utility installation details conflict with the Town Standards that the Town Standards shall govern.* BETA2: Note provided – issue resolved.
- U2. Provide size and material information for proposed sewer force main and water line(s). *MAI: The size and materials of the sewer and water lines have been added to the plan set, refer to Sheet C 3.0.* BETA2: Information provided. In accordance with Town Specifications, revise material of water service line to copper if length is 100 feet or less (corporation stop to curb stop and curb stop to building) and HDPE otherwise.

MAI Response: The water line has been revised to be copper.

- U3. Indicate how water for fire protection will be supplied, if at all. *MAI: There is no Automated Fire Sprinkler system. Per applicable State & Local Codes (IBC 2015 and CMR 780-9-903 local amendment, Automated Fire Sprinklers are not required for Group M and B occupancy under 12,000 sf and under 3 stories. Proposed building area is 3,930 sf and this is a one-story building.* BETA2: Information provided – issue dismissed.
- U4. Confirm the proposed solar lighting is capable of providing adequate illumination for the site throughout the night during adverse conditions (e.g. multiple cloudy/rainy days). *MAI: The solar area lights have an electronic smart controller that stores energy and adjusts light output for optimal performance up to 14 days. Light levels will be maintained per IES recommendations as shown on the attached photometric plan.* BETA2: Information provided – issue resolved.

STORMWATER MANAGEMENT

The project proposes to direct runoff from impervious areas into a new subsurface infiltration system via catch basin connections and proprietary water quality units (Contech CDS). Overflows from the proposed infiltration system will be directed into a low-lying basin area on the eastern side of the lot.

General

- SW1. As part of the MS4 regulations, the Town is proposing revisions to Chapter 153, Stormwater Management. Once the revisions are approved (date not yet determined) they will be applicable to any project that is subject to the Bylaw and has not yet been approved. BETA recommends the designer review the proposed Bylaw revisions to evaluate if additional stormwater provisions or treatment may be required. *MAI: MAI has reviewed the proposed bylaw revisions and has made changes to the design as required.* BETA2: Information provided to demonstrate compliance with future requirements – issue resolved.
- SW2. Provide a stamped Stormwater Management Checklist. *MAI: A stamped Stormwater Management Checklist has been provided in the stormwater report.* BETA2: Checklist provided. Clarify reference to project being covered by the NPDES Multi-Sector General Permit, as the



proposed use is not an industrial activity. The checklist should also reference that the project is located in a watershed with a TMDL (Charles River), has soils with rapid infiltration rates, and involves runoff from land uses with higher potential pollutant loads (>1,000 trips per traffic report).

MAI Response: The checklist has been revised accordingly.

SW3. Revise proposed HDPE pipe to be RCP. Where cover is less than 42" provide Class V RCP (§300-11.B.(2)(a)). BETA notes that with a waiver request, the Board may consider allowing the use of the 4" HDPE overflow from the subsurface infiltration system. *MAI: A waiver has been requested from (§300- 11.B.(2)(a)) to allow for a HDPE pipe, refer to Sheet C 0.0. HDPE is used industry wide where cover over the pipe is in excess of twenty-four (24) inches.* BETA2: Waiver request provided; however, BETA notes that to date the Board has not granted this waiver on previous projects except for short connections directly to subsurface infiltration systems.

MAI Response: We will continue to request the waiver. We note that should the waiver not be granted, then the pipe will be constructed of RCP.

SW4. In coordination with the Town, provide an easement for the existing outfall at the northwest end of the site. *MAI: An easement for the town at the headwall has been depicted graphically on the plan set, refer to Sheet C 2.0.* BETA2: Easement provided. BETA defers any additional comment to the DPW.

SW5. Revise the diameter of the proposed catch basins to a minimum of 5 feet to accommodate the proposed double grates. *MAI: The diameter of the catch basins have been revised accordingly, refer to Sheet C 5.0.* BETA2: Diameter revised – issue resolved.

SW6. Consider providing periodic check dams in the northerly swale to minimize flow velocities and promote infiltration. *MAI: Check dams have been added to the plan set, refer to Sheet C 2.0.* BETA2: Check dams provided – issue resolved.

SW7. Clarify where the Typical Level Spreader is proposed. *MAI: The location of the level spreader has been added to the plan set, refer to Sheet 2.0.* BETA2: Clarification provided – issue resolved.

Massachusetts Stormwater Management Standards:

The proposed development will disturb greater than one acre and is subject to Chapter 153: Stormwater Management of the Town of Franklin Bylaws and MassDEP Stormwater Management Standards.

No untreated stormwater (Standard Number 1): *No new stormwater conveyances (e.g., outfalls) may discharge untreated stormwater directly to or cause erosion in wetlands or waters of the Commonwealth.* The project does not propose any new untreated stormwater discharges to wetlands. An outfall is proposed from the subsurface infiltration system which discharges to a low-lying area. A riprap apron is proposed for erosion control.

SW8. Although the existing outfall at the northwest corner of the site is not the responsibility of the project proponent, it is recommended to provide a rip rap pad at the outlet. *MAI: A rip rap pad*



has been added to the existing outfall pipe, refer to Sheet C 2.0. BETA2: Rip rap pad provided – issue resolved.

Post-development peak discharge rates (Standard Number 2): *Stormwater management systems must be designed so that post-development peak discharge rates do not exceed pre-development peak discharge rates.* The project proposes an increase in impervious area and will use subsurface infiltration systems to mitigate increases in post-development peak discharge rates and total runoff volumes.

SW9. Provide summary table comparing pre-development and post-development runoff volumes. Runoff volumes may not increase per §300-11.A.(3) and the Best Development Practices Guidebook. *MAI: A summary table comparing pre-development and post-develop runoff volumes has been added to the stormwater management report.* BETA2: Table provided indicating a reduction in peak runoff volume – issue resolved.

SW10. Revise HydroCAD model to include subwatershed SC100, as depicted on the Post-Development Drainage Plan, and show the boundary between Watershed SC100 and SC200. *MAI: The HydroCAD model has been revised to exclude subwatershed SC100 and instead shows the eastern and western parking lots as subcatchment 200, which flows to the subsurface infiltration basin. Subwatershed SC101 is the runoff that is directed to Design Point #1.* BETA2: Information provided – issue resolved.

SW11. Label the Post-Development subwatershed located in the south-central portion of the Site. *MAI: The Post-Development subwatershed located in the south-central portion of the site has been added on the drainage maps.* BETA2: Information provided – issue resolved.

SW12. Based on a review of the site there appears to be a low-lying area on the east of the site in proximity to DP2. Additional spot grades from the initial survey should be provided on the plan to clarify this topography and if the low area is confirmed it should be included in the HydroCAD model as a pond. *MAI: The above referenced low-lying area is actually an elevated mound, not a depression, therefore there was no need to modify the HydroCAD model.* BETA2: BETA revisited the site and confirmed that the referenced mound (approx. 6" to 1' high near the abutting Planet Fitness property line - refer to attached sketch) is likely to impound water and will minimize any flow directed to the adjacent site – issue remains outstanding.

MAI Response: The existing earth berm near the Planet Fitness property line has been added to the plans and has been modeled in HydroCAD. The calculations show that this berm does retain and reduce the runoff onto Planet Fitness. In Proposed conditions, a depression is proposed to mimic the functionality of the earthen berm. With that said, the HydroCAD calculations have been revised accordingly and the calculations still show a reduction in the peak rate of runoff as well as a reduction in volume from existing conditions to proposed conditions.

SW13. Recommend including the proposed infiltration overflow area in the HydroCAD model as an additional infiltration area. *MAI: This area is likely to be used as a wetland replication area and vegetated with wetland species. It is anticipated that this area will provide infiltration, but it is not being modeled as such, therefore revisions to the HydroCAD model have not been made.* BETA2: Information provided. In conjunction with comment SW12, the designer should



demonstrate that the proposed overflow area provides an equivalent or greater storage volume than the existing impoundment, as the flow from the Town system is not included in the stormwater model.

MAI Response: The existing earth berm near the Planet Fitness property line has been added to the plans and has been modeled in HydroCAD. The calculations show that this berm does retain and reduce the runoff onto Planet Fitness. In Proposed conditions, a depression is proposed to mimic the functionality of the earthen berm. With that said, the HydroCAD calculations have been revised accordingly and the calculations still show a reduction in the peak rate of runoff as well as a reduction in volume from existing conditions to proposed conditions.

SW14. Revise limits of watershed SC101. Based on the proposed grading, the majority of this area will drain to the western parking area (Design Point 2) instead of Design Point 1. *MAI: The limits of watershed SC101 have been revised accordingly.* BETA2: Watershed limits revised – issue resolved.

SW15. Clarify how roof runoff will be conveyed. Consider providing a direct connection from the roof leaders to the subsurface infiltration system. *MAI: Downspouts will be directed to a closed underground piping system that will connect directly to the 12" manifold at the subsurface infiltration basin.* BETA2: Direction connection provided – issue resolved.

Recharge to groundwater (Standard Number 3): *Loss of annual recharge to groundwater should be minimized through the use of infiltration measures to maximum extent practicable.*

NRCS maps indicate the presence of Sudbury fine sandy loam, rated in hydrologic soil group (HSG) B, primarily at the site. A small area of Merrimac fine sandy loam (HSG A) is depicted along the west side of the site near Grove Street. The infiltration systems have been designed to provide a recharge volume in excess of that required.

SW16. Clarify the Schematic Plan View of the Subsurface Infiltration Facility Details to indicate it is a typical layout and the dimensions are 20 rows of 11 chambers. Revise detail name, as necessary, to reflect the number of systems proposed. *MAI: The details of the Subsurface Infiltration Facility details have been revised accordingly, refer to Sheet C 5.0.* BETA2: Details revised – issue resolved.

SW17. The proposed bottom of the infiltration system is at elevation 250.30 and will not provide the required 2' minimum separation to groundwater based upon the soils analysis for Test Pit 2 (ESHGW @ 251.5). *MAI: The bottom elevation of the infiltration basin is two (2) feet above the groundwater encountered in Test Pit #1 (248.3), which is located adjacent to the infiltration system.* BETA2: Information provided which indicates the eastern side of the proposed infiltration system has the required 2' separation to groundwater; however, the groundwater profile created by the additional test pit information cannot be discounted for the remainder of the system. Either revise the system to provide the required 2' separation throughout the system based on the groundwater profile or provide an additional test pit at the western side of the proposed system to demonstrate a consistent groundwater elevation.



MAI Response: A confirmatory test pit can be dug in the western portion of the infiltration system prior to construction to confirm the ground water elevations. If that test pit depicts a higher than anticipated groundwater elevation, modifications to the drainage system will be made at such time.

SW18. Revise the top elevation of the stone in the infiltration system on the Cross Section detail to be consistent with other elevations. *MAI: The top elevation of the stone in the infiltration system has been revised accordingly, refer to Sheet C 5.0.* BETA2: Elevation revised – issue resolved.

SW19. Provide mounding analysis for proposed infiltration systems as separation to groundwater is less than 4 feet. *MAI: Mounding calculations have been provided in the stormwater management report.* BETA2: Analysis provided – issue resolved.

SW20. Test pit data indicates pockets of sandy loam within the C layer of coarse sand and gravel, which are more restrictive than the design exfiltration rate of 8.27 in/hr. Provide additional clarification to justify the design exfiltration rate or lower the rate, if appropriate. *MAI: Per the Subsurface Infiltration Detail on sheet C 5.0, there is a note that states that all unsuitable materials are to be removed five (5) feet in all directions from around the proposed infiltration system, this includes the sandy loam.* BETA2: Information provided – issue resolved.

80% TSS Removal (Standard Number 4): *For new development, stormwater management systems must be designed to remove 80% of the annual load of Total Suspended Solids.*

The project proposes to direct runoff from new impervious areas to a treatment train consisting of deep sump catch basins with hoods, proprietary water quality units (Contech CDS), and a subsurface infiltration system. Calculations are provided that demonstrate the required 80% TSS removal and 1” Water Quality Volume can be provided with the deep sump catch basin and infiltration basin treatment train.

Higher Potential Pollutant Loads (Standard Number 5): *Stormwater discharges from Land Uses with Higher Potential Pollutant Loads require the use of specific stormwater management BMPs.*

SW21. Provide the total number of estimated trips per day for the site. If the number exceeds 1,000 the site is considered a high-intensity-use parking area and is therefore LUHPPL. *MAI: The site will generate, on average 800 - 1,000 trips per day and is therefore is not considered a LUHPPL.* BETA2: The traffic report indicates the daily trips are 1,050; therefore, the site is considered a LUHPPL. BETA notes this classification is not anticipated to require any stormwater modifications.

MAI Response: MAI concurs with the above statement.

Critical Areas (Standard Number 6): *Stormwater discharges to critical areas must utilize certain stormwater management BMPs approved for critical areas.*

The project includes discharges to a Zone II Wellhead Protection Area, a critical area, and 44% pretreatment is required prior to infiltration. The proposed treatment trains are consistent with the recommendations of MassDEP for discharges to Zone II wellhead protection areas.



SW22. Revise narrative to correctly indicate the presence of a critical area. *MAI: The narrative has been revised accordingly.* BETA2: Narrative revised – issue resolved.

SW23. Provide calculation based upon MassDEP’s “Standard Method to Convert Required Water Quality Volume to a Discharge Rate for Sizing Flow Based Manufactured Proprietary Stormwater Treatment Practices” to demonstrate the Contech Structures are capable of treating the calculated discharge rate and will remove a minimum of 44% TSS prior to infiltration. *MAI: MAI has reached out to Contech to obtain the documentation required that demonstrates that the Contech structures are capable of treating the calculated discharge rate and will remove a minimum of 44% TSS prior to infiltration. That documentation can be found in the Appendix of this report.* BETA2: The provided information does not appear to show the DEP calculated water quality flow rate compared to the maximum treatment rate provided by the Contech unit – issue remains outstanding.

MAI Response: DEP calculated water quality flow rates compared to the maximum treatment rate provided by the Contech unit have been provided.

Redevelopment (Standard Number 7): *Redevelopment of previously developed sites must meet the Stormwater Management Standards to the maximum extent practicable.*

The project does not qualify as redevelopment – not applicable.

SW24. Revise narrative to remove references to “70 Frank Mossberg Drive” and that the project qualifies as a redevelopment. *MAI: The narrative has been revised accordingly.* BETA2: Narrative revised – issue resolved.

Construction Period Erosion and Sediment Controls (Standard Number 8): *Erosion and sediment controls must be implemented to prevent impacts during construction or land disturbance activities.*

The project as currently depicted will disturb greater than one acre of land; therefore, a Notice of Intent with EPA and a Stormwater Pollution Prevention Plan (SWPPP) is required. The project plans indicate the use of a stabilized construction entrance, silt sacks, and perimeter erosion controls (Filtermitt).

SW25. Provide perimeter controls along the southwestern border of the Site (e.g. where existing flows are directed to DP1). *MAI: Perimeter erosion controls have been added to the plan set, refer to Sheets C 1.0 and C 2.0.* BETA2: Perimeter controls provided – issue resolved.

SW26. Revise Temporary Stabilized Construction Entrance Detail to be a continuous width of 20 feet as depicted on the Layout, Grading, and Erosion Control Plan. *MAI: The temporary Stabilized Construction Entrance Detail has been revised to be a continuous width of 20 feet.* BETA2: Detail revised – issue resolved.

Operations/maintenance plan (Standard Number 9): *A Long-Term Operation and Maintenance Plan shall be developed and implemented to ensure that stormwater management systems function as designed.*

A Long-Term Operation and Maintenance (O&M) Plan has been provided.



SW27. Provide long-term maintenance measures for catch basins and Contech water quality units. *MAI: The Operation and Maintenance Plan has been revised accordingly.* BETA2: Information provided – issue resolved.

SW28. Provide a plan that shows the location of all stormwater BMPs as part of the O&M Plan. *MAI: A plan that depicts the stormwater BMP's has been added to the O&M Plan.* BETA2: Plan provided – issue resolved.

SW29. Provide an estimated O&M budget. *MAI: An estimated O&M Budget will be provided prior to construction.* BETA2: To avoid a condition of approval that would require this information to be provided in the future, it is recommended to estimate the O&M budget at this time with the understanding that it can be modified prior to construction, if necessary.

MAI Response: An estimated annual budget of \$90,000 – \$95,000 has been added to the O & M Plan.

Illicit Discharges (Standard Number 10): All illicit discharges to the stormwater management systems are prohibited.

The Stormwater Management Report indicates that no illicit discharges are proposed, and a signed Illicit Discharge Compliance Statement will be provided prior to construction.

SW30. Provide a signature on the Illicit Discharge Compliance Statement. *MAI: A signature has been added to the Illicit Discharge Compliance Statement.* BETA2: Signature provided – issue resolved.

New MAI Comment:

The proposed widening of the Planet Fitness access drive will add approximately 1,000 square feet (0.023 Acres). The drainage area for the Planet Fitness drainage system is approximately 4.5 Acres. We feel as though the increase of 0.02 Acres, this is a 0.4 percent increase on the existing drainage area and as such should not have a negative impact on the existing drainage system. Note that the existing drainage system is presently comprised of deep sump catch basins, sediment forebays and detention basins which should be able to adequately treat the minor increase in the paved area.

Please feel free to call with any questions.

Sincerely,

MERIDIAN ASSOCIATES, INC.

David S. Kelley, P.E.
Senior Project Manager

September 24, 2020

Mr. Anthony Padula, Chairman
Franklin Planning Board
355 East Central Street
Franklin, MA 02038

**Re: 164 Grove Street
Site Plan Peer Review Update**

Dear Mr. Padula:

BETA Group, Inc. has reviewed revised documents for the proposed Site Plan Approval application, "**Permit Site Development Plans - 164 Grove Street, Franklin, Massachusetts.**" This letter is provided to update findings, comments, and recommendations.

BASIS OF REVIEW

BETA received the following items:

- **Site Plan & Special Permit Application**, including the following:
 - *Cover Letter*
 - *Application for Approval of a Site Plan and Special Permits*
 - *Exhibit 5: Special Permit Findings*
 - *Form P*
 - *Certificate of Ownership*
 - *Filing Fees*
- Plans (10 Sheets) entitled **Permit Site Development Plans** dated May 5, 2020, revised September 16, 2020 and prepared by Meridian Associates of Beverly, MA.
- **Stormwater Analysis and Calculations**, dated May 8, 2020, revised September 16, 2020, and prepared by Meridian Associates of Beverly, MA.

Review by BETA will include the above items along with the following, as applicable:

- Site Visit
- **Zoning Chapter 185 From the Code of the Town of Franklin**, current through October 2019
- **Zoning Map of the Town of Franklin, Massachusetts**, attested to April 30, 2019
- **Stormwater Management Chapter 153 From the Code of the Town of Franklin**, Adopted May 2, 2007
- **Subdivision Regulations Chapter 300 From the Code of the Town of Franklin**, current through January 1, 2016
- **Wetlands Protection Chapter 181 From the Code of the Town of Franklin**, dated August 20, 1997
- **Town of Franklin Best Development Practices Guidebook**, dated September 2016

INTRODUCTION

The project site consists of 164 Grove Street, a vacant lot developed with a small cleared area and gravel driveway (the "Site"). The parcel contains an area of 1.5 Acres and is located along the eastern side of Grove Street. The Town of Franklin Assessor's Office identifies the parcel as Map 306 Lot 4. The Site and all surrounding properties are located within the Industrial Zoning District.

The existing Site includes a gravel driveway connecting to Grove Street which extends into the center of the Site. This central area is an undeveloped area surrounded by small trees. A bar gate located along the driveway restricts access into the Site. A chain link fence connects to this gate and surrounds the perimeter of the Site. Topography at the Site is generally sloped towards the east, and grades are typically 4% or flatter with the exception of several steeper areas (10% +/-) on the western side of the Site.

The Applicant proposes to remove the existing fence, driveway, and vegetation and construct a new 4,150 sq. ft. Non-Medical Marijuana Retail Establishment. Associated site developments will include two new paved parking lots, two driveway aprons connecting to the existing driveway to the south, grading, utilities (water, sewer, underground electric), lighting, and landscaping. Stormwater management is proposed through deep sump catch basins, water quality units, and a subsurface infiltration system.

A portion of the project is located within an approved wellhead protection area (Zone II) and therefore the Water Resource District. No wetland resource areas are depicted within the project limits; however, the northeastern portion of the site is shown to be within the 100-foot buffer zone. The project is not located within a FEMA mapped 100-year flood zone or a NHESP mapped estimated habitat area of rare or endangered species. NRCS maps primarily indicate the presence of Sudbury fine sandy loam, rated in hydrologic soil group (HSG) B, at the site. A small area of Merrimac fine sandy loam (HSG A) is depicted along the west side of the site near Grove Street.

FINDINGS, COMMENTS AND RECOMMENDATIONS

GENERAL COMMENTS

- G1. Provide detail for proposed dumpster pad and enclosure (with screening). *MAI: A detail for the dumpster pad and enclosure has been added to the plan set, see Sheet C 5.1. BETA2: Details provided. BETA recommends that slats are provided for the chain link option, which is typically required by the Board. MAI2: Privacy slats have been added to the Dumpster Enclosure Detail. BETA3: Slats provided – issue resolved.*
- G2. Confirm access rights and utility easements are being acquired from the adjacent property to the south. *MAI: Yes. We are in active discussions and negotiations with owner representative for Core Real Estate Holdings of 166 Grove Street as to mutually acceptable business terms and conditions to acquire the access rights and utility easements for the 164 Grove Street Project including the ability to address any improvements required to the access way by the Planning Board in connection with its review an consideration of the Special Permit for Shared Common Driveway. Attached are copies of the Deed into Core Real Estate Holdings as well as the existing Easement Agreement and plan between the owners of 166 Grove Street and 168 Grove Street concerning similar access and utility easements. BETA2: Information provided. BETA defers to the preference of the Board to require rights/easements as a condition of approval. MAI2: MAI concurs, we are requesting that the Board require rights/easements as a condition of approval. To*

date, the Applicant has reach agreement on business terms and conditions for the grant of easements for the shared common driveway and utility connections from the 166 Grove Street and 168 Grove Street property owners. BETA3: No further comment.

- G3. Clarify the disposition of the existing fences and gate surrounding the property. *MAI: The existing fence around the perimeter of the site, that is located within the property lines, is to be removed. Refer to Sheet C 1.0. BETA2: Clarification provided. It is anticipated that any fence removal outside of the property line will be coordinate with the ongoing access and easement negotiations – issue resolved.*
- G4. Recommend revising snow storage areas to maintain clear flow path within swale along the northerly property line. Consider providing additional snow storage along the southerly curb line. *MAI: The snow storage locations have been adjusted accordingly, refer to Sheet C 4.0. BETA2: Snow storage area revised – issue resolved.*
- G5. Provide a note to indicate that tree species shall be from the Town of Franklin Best Development Practices Guidebook. Also confirm the proposed plantings meet this requirement. **BETA2: No response provided – issue remains outstanding.** *MAI: A note has been added to the landscaping plan. Additionally, the tree species have been updated and now specify trees that are listed in the Town of Franklin Best Development Practices Guidebook. BETA2: Note provided – issue resolved.*

ZONING

The Site is located within the Industrial (I) Zoning District and the Marijuana Use Overlay District. The proposed use of the Site is identified as Non-Medical Marijuana Retail Establishment. The proposed uses are allowed in the District via a Special Permit from the Planning Board.

SCHEDULE OF LOT, AREA, FRONTAGE, YARD AND HEIGHT REQUIREMENTS (§185 ATTACHMENT 9)

The project site will meet the requirements for lot area, frontage, lot depth, yards, height, and impervious coverage. The project does not meet the requirements for lot width; however, per §185-3 Lot Width C.(2) any lot shown on a recorded plan prior to May 21, 1998 is exempt from this definition. The Quitclaim Deed provided as part of the submission documents indicates the subject parcel is depicted on a plan of land recorded in the Norfolk Registry of Deeds, dated August 25, 1987 and is therefore exempt.

PARKING, LOADING AND DRIVEWAY REQUIREMENTS (§185-21)

The existing Site includes one access driveway from Grove Street to the west. The project proposes to remove this access route and construct two new paved access driveways (1 entrance, 1 exit) from the 166 Grove Street site to the south.

Section §185-21.B.(3) describes the number of parking spaces required for residential and nonresidential buildings in the Industrial Zoning District. The required parking for a retail use is one space per 200 sq. ft. of gross floor area plus one space per separate enterprise. For the proposed 4,150 sq. ft. building, the required parking is thus 21 spaces and a total of 66 spaces are proposed. With the understanding that retail marijuana uses have specific parking demands, additional commentary will be provided as part of the Traffic Review, to be provided under separate cover.

Proposed 90° parking spaces are depicted as 19' long and 9' wide. Proposed angled (60°) parking spaces are 18' long (usable stall) and 9' wide. Access route widths vary between 16 ft. and 24 ft, and all driveways are designated to be one-way. In accordance with Massachusetts Architectural Access Board (MAAB)

requirements, four parking spaces have been designed to be handicap accessible, two of which are also van accessible.

In compliance with §185-21.C.(5), one tree must border the parking lot per every 10 parking spaces. A total of 31 trees, supplemented by shrubs, are proposed in the vicinity of the parking lot.

- P1. The angled parking layout conforms to industry standards; however, the usable stall length is only 18 feet. Revise the usable stall length to be 19 feet §185-21.C.(9)(a). *MAI: The length of the angled parking spaces has been revised accordingly, refer to Sheet C 2.0. BETA2: Stall length revised – issue resolved.*
- P2. The accessible route is located within the 24' driveway aisle and vehicles backing out of spaces will encroach into the striped walkway. Evaluate alternatives to eliminate pedestrian/vehicle conflicts. *MAI: The location of the accessible route from the parking spaces to the building was chosen as it provides the most visibility for drivers while circulating through the parking lot. Additionally, the drive aisle width in this location is twenty-four (24) feet wide thus providing a nineteen (19) foot wide aisle for vehicles in which to safely travel throughout the parking lot. BETA2: BETA notes that while the location of the accessible route is not ideal, there does not appear to be a practicable solution that does not require significant redesign of the site.*
- P3. Clarify if additional parking/site layouts have been evaluated, such as relocating the proposed building to the west end of the site and providing a continuous parking area. The current layout requires vehicles to circulate in a “figure 8” pattern with a number of vehicle conflict points. *MAI: Many layouts for the site were considered. Ultimately the layout selected was preferred to move any potential traffic congestion away from Grove Street. Parking count was maximized beyond the minimum requirements to help avoid customers waiting for parking spots, and it was preferable to avoid one large parking lot with long walks for store customers. In addition, the entrance and exits are aligned with the existing curb cuts on the southern side of the access drive. BETA2: Information provided – refer to comment P4.*
- P4. Provide turning movements on Site Plan to demonstrate that passenger, delivery, and waste collection vehicles can safely maneuver throughout the site. It is anticipated that the Fire Chief will review turning movements for fire apparatus throughout the site. *MAI: A turning monument sketch has been provided and is submitted as a part of this comment response letter. BETA2: Also provide a turning movement for the passenger vehicle making a right-hand turn into and around the easterly parking area to demonstrate there will be no conflicts with the other passenger vehicle movements at the entrance. BETA also recommends to evaluate if the waste collection vehicle can make turns to use the site exit instead of backing into the common driveway. MAI2: The additional passenger vehicle turning movement has been added to the Vehicle Movement Plan. It should be noted that the dumpsters use will be small roll away dumpsters and the can be moved to reduce the movement of the truck used to remove the dumpsters. BETA3: The turning movement plan indicates a conflict between vehicles and should be revised to show that the vehicles can safely move past each other. Consider increasing the radius on the northwest corner of the landscaped island at the site entrance to provide additional room for turning, if necessary. BETA notes that the waste collection vehicle will likely be required to back onto the private common driveway while exiting the site.*
- P5. Confirm the number of trees provided in the Plant Schedule (31) vs. the Landscape Table (10). *MAI: The number of trees and shrubs depicted on the plans and listed in the plant schedule are consistent. BETA2: The number of trees provided is adequate – issue dismissed.*

SIDEWALKS (§185-28)

The project is located within the Industrial Zoning District and is not required to provide sidewalks along the street frontage. There are no existing sidewalks on Grove Street in proximity to the project.

CURBING (§185-29)

The project proposes the use of vertical granite curbing along paved areas.

- S11. Clarify limits of vertical granite curb as it relates to the concrete walkway. The Concrete Walkway Detail depicts monolithic concrete curb. *MAI: The limits of the types of curbing have been clarified, refer to Sheet C 2.0.* **BETA2: Clarification provided – issue resolved.**

SITE PLAN REVIEW (§185-31)

The proposed development is subject to Site Plan Review and must comply with the requirements of this section.

- S1. Include abutting land uses and zoning information on the Locus Map (§185-31.C.(3)(d)). *MAI: The abutting land uses have been added to the plan set, refer to sheet C0.0.* **BETA2: Abutting land uses provided and it is understood that all abutting parcels are zoned as Industrial – issue resolved.**
- S2. Provide photometric plan (§185-31.C.(3)(l)). *MAI: A photometric plan has been added to the plan set, refer to sheet 6.0.* **BETA2: Plan provided indicating adequate illumination will be provided for safety and security. Expand limits of analysis to demonstrate there will be no nuisance or excessive light spillage onto adjacent properties in accordance with site plan and special permit review criteria.** *MAI2: The photometric plan has been revised to expand the limits of the analysis to demonstrate there is no nuisance or excessive light spillage onto adjacent properties.* **BETA3: The revised plan indicates minor spillage on the order of 0.01 to 0.02 footcandles, the equivalent of moonlight, along portions of the northerly property line**
- S3. Depict proposed limits of clearing on the plans, as applicable, including areas of existing vegetation to be retained (§185-31.C.(3)(u)). *MAI: The limit of clearing / limit of work is shown on the Site Plan, refer to Sheet C 2.0 of the plan set. It has also been added to Sheet C 1.0.* **BETA2: Information provided – issue resolved.**

SCREENING (§185-35)

The project proposes outdoor parking for 10 or more cars, which must be screened from adjacent residential districts or uses from which they would otherwise be visible. The Site is surrounded by lots zoned as Industrial, and it does not appear that the project will be visible from any residential use; therefore, screening is likely unnecessary.

WATER RESOURCES DISTRICT (§185-40)

The Site is partially located within the Water Resources District due to the presence of a Zone II Wellhead Protection Area. This portion of the Site includes the eastern parking lot and the majority of the proposed building.

- WR1. Clarify if the proposed sewer force main will connect to an off-site sewage disposal system or Town Sewer. If necessary, confirm the estimated sewage flow for the existing sewage disposal system will not exceed 110 gallons per 10,000 sq. ft. of lot area if located within the Water Resources District (§185-40.D.(1)(i)). *MAI: The proposed wastewater will be directed to the Town*

of Franklin public sewer. Per Massachusetts Department of Environmental Protection, Title V design standards, a retail store will produce approximately two hundred (200) gallons of wastewater per day. This assumes that public restrooms are available, however, at this site, the restrooms will not be available to the public so the flows should be far less. BETA2: Connection to Town sewer confirmed – issue dismissed.

- WR2. Section §185-40.D.(1)(I)(ii) requires that the proposed groundwater recharge efforts must be approved by a hydrogeologist; however, provided that the stormwater management system is revised to fully comply with the Massachusetts Stormwater Management Standards no adverse impacts to groundwater are anticipated as a result of the project. BETA defers to the preference of the Board to require approval by a hydrogeologist. MAI: **BETA2: No further comment.**
- WR3. Note that any fill placed in quantity greater than 15 yards must be certified in accordance with §185-40.E.(5). MAI: *MAI concurs with the above statement.* **BETA2: No further comment.**
- WR4. In conjunction with comment SW12, it is anticipated that minimal flow is directed from the project site to the paved area in proximity to DP2. BETA notes that to fully comply with (§185-40.E.(4)), all stormwater runoff from impervious surfaces must be recharged unless following consultation with, and approval from the Conservation Commission and the Building Inspector that recharge is determined to be infeasible. MAI: *This project will be submitted to the Conservation Commission for review and approval. Runoff from the impervious area that connects the site to the existing access road is di minimus in scale and should not have any adverse impacts to the adjacent properties. This is reflected in the stormwater calculations. Note that runoff from all of the other impervious surfaces is directed to an infiltration system that provides ground water recharge.* **BETA2: Information provided – issue dismissed.**

UTILITIES

Proposed utilities include drainage, electric, sanitary sewer, and domestic water services. Detailed review of water and sewer utilities is anticipated to be provided by the DPW and Fire Chief (e.g. for fire hydrants), as applicable.

- U1. Provide a note that all water and sewer utility installations shall be done in accordance with the Town of Franklin Department of Public Works Standards for Sewer and Water Materials and Installation (Town Standards). Also note that where utility installation details conflict with the Town Standards that the Town Standards shall govern. MAI: *The above requested note has been added to the plan set, refer to Sheets C 2.0 and C 3.0. Notes have been added that show where utility installation details conflict with the Town Standards that the Town Standards shall govern.* **BETA2: Note provided – issue resolved.**
- U2. Provide size and material information for proposed sewer force main and water line(s). MAI: *The size and materials of the sewer and water lines have been added to the plan set, refer to Sheet C 3.0.* **BETA2: Information provided. In accordance with Town Specifications, revise material of water service line to copper if length is 100 feet or less (corporation stop to curb stop and curb stop to building) and HDPE otherwise.** MAI2: *The water line has been revised to be copper.* **BETA3: Material revised – issue resolved.**
- U3. Indicate how water for fire protection will be supplied, if at all. MAI: *There is no Automated Fire Sprinkler system. Per applicable State & Local Codes (IBC 2015 and CMR 780-9-903 local amendment, Automated Fire Sprinklers are not required for Group M and B occupancy under*

12,000 sf and under 3 stories. Proposed building area is 3,930 sf and this is a one-story building.
BETA2: Information provided – issue dismissed.

- U4. Confirm the proposed solar lighting is capable of providing adequate illumination for the site throughout the night during adverse conditions (e.g. multiple cloudy/rainy days). *MAI: The solar area lights have an electronic smart controller that stores energy and adjusts light output for optimal performance up to 14 days. Light levels will be maintained per IES recommendations as shown on the attached photometric plan.* **BETA2: Information provided – issue resolved.**

STORMWATER MANAGEMENT

The project proposes to direct runoff from impervious areas into a new subsurface infiltration system via catch basin connections and proprietary water quality units (Contech CDS). Overflows from the proposed infiltration system will be directed into a low-lying basin area on the eastern side of the lot.

GENERAL

- SW1. As part of the MS4 regulations, the Town is proposing revisions to Chapter 153, Stormwater Management. Once the revisions are approved (date not yet determined) they will be applicable to any project that is subject to the Bylaw and has not yet been approved. BETA recommends the designer review the proposed Bylaw revisions to evaluate if additional stormwater provisions or treatment may be required. *MAI: MAI has reviewed the proposed bylaw revisions and has made changes to the design as required.* **BETA2: Information provided to demonstrate compliance with future requirements – issue resolved.**
- SW2. Provide a stamped Stormwater Management Checklist. *MAI: A stamped Stormwater Management Checklist has been provided in the stormwater report.* **BETA2: Checklist provided. Clarify reference to project being covered by the NPDES Multi-Sector General Permit, as the proposed use is not an industrial activity. The checklist should also reference that the project is located in a watershed with a TMDL (Charles River), has soils with rapid infiltration rates, and involves runoff from land uses with higher potential pollutant loads (>1,000 trips per traffic report). *MAI2: The checklist has been revised accordingly.* **BETA3: Checklist revised – issue resolved.****
- SW3. Revise proposed HDPE pipe to be RCP. Where cover is less than 42" provide Class V RCP (§300-11.B.(2)(a)). BETA notes that with a waiver request, the Board may consider allowing the use of the 4" HDPE overflow from the subsurface infiltration system. *MAI: A waiver has been requested from (§300- 11.B.(2)(a)) to allow for a HDPE pipe, refer to Sheet C 0.0. HDPE is used industry wide where cover over the pipe is in excess of twenty-four (24) inches.* **BETA2: Waiver request provided; however, BETA notes that to date the Board has not granted this waiver on previous projects except for short connections directly to subsurface infiltration systems.** *MAI2: We will continue to request the waiver. We note that should the waiver not be granted, then the pipe will be constructed of RCP.* **BETA3: BETA recommends for the Board to discuss their preference for pipe material.**
- SW4. In coordination with the Town, provide an easement for the existing outfall at the northwest end of the site. *MAI: An easement for the town at the headwall has been depicted graphically on the plan set, refer to Sheet C 2.0.* **BETA2: Easement provided. BETA defers any additional comment to the DPW.**

- SW5. Revise the diameter of the proposed catch basins to a minimum of 5 feet to accommodate the proposed double grates. *MAI: The diameter of the catch basins have been revised accordingly, refer to Sheet C 5.0. BETA2: Diameter revised – issue resolved.*
- SW6. Consider providing periodic check dams in the northerly swale to minimize flow velocities and promote infiltration. *MAI: Check dams have been added to the plan set, refer to Sheet C 2.0. BETA2: Check dams provided – issue resolved.*
- SW7. Clarify where the Typical Level Spreader is proposed. *MAI: The location of the level spreader has been added to the plan set, refer to Sheet 2.0. BETA2: Clarification provided – issue resolved.*
- SW7A. Revise the infiltration system overflow size on the plan from 4” to 6” to match the current HydroCAD model.**

MASSACHUSETTS STORMWATER MANAGEMENT STANDARDS:

The proposed development will disturb greater than one acre and is subject to Chapter 153: Stormwater Management of the Town of Franklin Bylaws and MassDEP Stormwater Management Standards.

No untreated stormwater (Standard Number 1): *No new stormwater conveyances (e.g., outfalls) may discharge untreated stormwater directly to or cause erosion in wetlands or waters of the Commonwealth.*

The project does not propose any new untreated stormwater discharges to wetlands. An outfall is proposed from the subsurface infiltration system which discharges to a low-lying area. A riprap apron is proposed for erosion control.

- SW8. Although the existing outfall at the northwest corner of the site is not the responsibility of the project proponent, it is recommended to provide a rip rap pad at the outlet. *MAI: A rip rap pad has been added to the existing outfall pipe, refer to Sheet C 2.0. BETA2: Rip rap pad provided – issue resolved.*

Post-development peak discharge rates (Standard Number 2): *Stormwater management systems must be designed so that post-development peak discharge rates do not exceed pre-development peak discharge rates.*

The project proposes an increase in impervious area and will use subsurface infiltration systems to mitigate increases in post-development peak discharge rates and total runoff volumes.

- SW9. Provide summary table comparing pre-development and post-development runoff volumes. Runoff volumes may not increase per §300-11.A.(3) and the Best Development Practices Guidebook. *MAI: A summary table comparing pre-development and post-develop runoff volumes has been added to the stormwater management report. BETA2: Table provided indicating a reduction in peak runoff volume – issue resolved.*
- SW10. Revise HydroCAD model to include subwatershed SC100, as depicted on the Post-Development Drainage Plan, and show the boundary between Watershed SC100 and SC200. *MAI: The HydroCAD model has been revised to exclude subwatershed SC100 and instead shows the eastern and western parking lots as subcatchment 200, which flows to the subsurface infiltration basin. Subwatershed SC101 is the runoff that is directed to Design Point #1. BETA2: Information provided – issue resolved.*

- SW11. Label the Post-Development subwatershed located in the south-central portion of the Site. *MAI: The Post-Development subwatershed located in the south-central portion of the site has been added on the drainage maps.* **BETA2: Information provided – issue resolved.**
- SW12. Based on a review of the site there appears to be a low-lying area on the east of the site in proximity to DP2. Additional spot grades from the initial survey should be provided on the plan to clarify this topography and if the low area is confirmed it should be included in the HydroCAD model as a pond. *MAI: The above referenced low-lying area is actually an elevated mound, not a depression, therefore there was no need to modify the HydroCAD model.* **BETA2: BETA revisited the site and confirmed that the referenced mound (approx. 6” to 1’ high near the abutting Planet Fitness property line - refer to attached sketch) is likely to impound water and will minimize any flow directed to the adjacent site – issue remains outstanding.** *MAI2: The existing earth berm near the Planet Fitness has been modeled in HydroCAD. The calculations show that this berm does retain and reduce the runoff onto Planet Fitness. In Proposed conditions, a depression is proposed to mimic the functionality of the eatehrn berm. With that said, the HydroCAD calculations have been revised accordingly and the calculations still show a reduction in the peak rate of runoff as well as a reduction in volume from existing conditions to proposed conditions.* **BETA3: Existing impoundment included in HydroCAD model – issue resolved.**
- SW13. Recommend including the proposed infiltration overflow area in the HydroCAD model as an additional infiltration area. *MAI: This area is likely to be used as a wetland replication area and vegetated with wetland species. It is anticipated that this area will provide infiltration, but it is not being modeled as such, therefore revisions to the HydroCAD model have not been made.* **BETA2: Information provided. In conjunction with comment SW12, the designer should demonstrate that the proposed overflow area provides an equivalent or greater storage volume than the existing impoundment, as the flow from the Town system is not included in the stormwater model.** *MAI2: The existing earth berm near the Planet Fitness has been modeled in HydroCAD. The calculations show that this berm does retain and reduce the runoff onto Planet Fitness. In Proposed conditions, a depression is proposed to mimic the functionality of the eatehrn berm. With that said, the HydroCAD calculations have been revised accordingly and the calculations still show a reduction in the peak rate of runoff as well as a reduction in volume from existing conditions to proposed conditions.* **BETA3: BETA compared the volumes of the existing and proposed impoundments and notes that additional storage volume will be provided in the proposed conditions. Additionally, BETA compared the flow rates and volumes directed to the impoundments and found they will be reduced in the proposed conditions – issue resolved.**
- SW14. Revise limits of watershed SC101. Based on the proposed grading, the majority of this area will drain to the western parking area (Design Point 2) instead of Design Point 1. *MAI: The limits of watershed SC101 have been revised accordingly.* **BETA2: Watershed limits revised – issue resolved.**
- SW15. Clarify how roof runoff will be conveyed. Consider providing a direct connection from the roof leaders to the subsurface infiltration system. *MAI: Downspouts will be directed to a closed underground piping system that will connect directly to the 12” manifold at the subsurface infiltration basin.* **BETA2: Direction connection provided – issue resolved.**
- SW15A. The new impervious area associated with the widened driveway has not been included in the HydroCAD model and the designer has asserted that this flow is directed to treatment train consisting of deep sump catch basins, sediment forebays, and detention basins, which will**

provide the required treatment and attenuations. BETA requests that record plans of the existing drainage system as well as photographic evidence that the existing system is maintained and functioning as designed be provided.

Recharge to groundwater (Standard Number 3): *Loss of annual recharge to groundwater should be minimized through the use of infiltration measures to maximum extent practicable.*

NRCS maps indicate the presence of Sudbury fine sandy loam, rated in hydrologic soil group (HSG) B, primarily at the site. A small area of Merrimac fine sandy loam (HSG A) is depicted along the west side of the site near Grove Street. The infiltration systems have been designed to provide a recharge volume in excess of that required.

- SW16. Clarify the Schematic Plan View of the Subsurface Infiltration Facility Details to indicate it is a typical layout and the dimensions are 20 rows of 11 chambers. Revise detail name, as necessary, to reflect the number of systems proposed. *MAI: The details of the Subsurface Infiltration Facility details have been revised accordingly, refer to Sheet C 5.0. BETA2: Details revised – issue resolved.*
- SW17. The proposed bottom of the infiltration system is at elevation 250.30 and will not provide the required 2' minimum separation to groundwater based upon the soils analysis for Test Pit 2 (ESHGW @ 251.5). *MAI: The bottom elevation of the infiltration basin is two (2) feet above the groundwater encountered in Test Pit #1 (248.3), which is located adjacent to the infiltration system. BETA2: Information provided which indicates the eastern side of the proposed infiltration system has the required 2' separation to groundwater; however, the groundwater profile created by the additional test pit information cannot be discounted for the remainder of the system. Either revise the system to provide the required 2' separation throughout the system based on the groundwater profile or provide an additional test pit at the western side of the proposed system to demonstrate a consistent groundwater elevation. MAI2: A confirmatory test pit can be dug in the western portion of the infiltration system prior to construction to confirm the groundwater elevations. If that test pit depicts a higher than anticipated groundwater elevation, modifications to the drainage system will be made at such time. BETA3: In consideration that the entire stormwater system design is contingent on this subsurface infiltration system and that it is anticipated that additional test pit information will indicate a groundwater table within 2 feet of the infiltration system, BETA recommends for the issue to be resolved at this time.*
- SW18. Revise the top elevation of the stone in the infiltration system on the Cross Section detail to be consistent with other elevations. *MAI: The top elevation of the stone in the infiltration system has been revised accordingly, refer to Sheet C 5.0. BETA2: Elevation revised – issue resolved.*
- SW19. Provide mounding analysis for proposed infiltration systems as separation to groundwater is less than 4 feet. *MAI: Mounding calculations have been provided in the stormwater management report. BETA2: Analysis provided – issue resolved.*
- SW20. Test pit data indicates pockets of sandy loam within the C layer of coarse sand and gravel, which are more restrictive than the design exfiltration rate of 8.27 in/hr. Provide additional clarification to justify the design exfiltration rate or lower the rate, if appropriate. *MAI: Per the Subsurface Infiltration Detail on sheet C 5.0, there is a note that states that all unsuitable materials are to be removed five (5) feet in all directions from around the proposed infiltration system, this includes the sandy loam. BETA2: Information provided – issue resolved.*

80% TSS Removal (Standard Number 4): *For new development, stormwater management systems must be designed to remove 80% of the annual load of Total Suspended Solids.*

The project proposes to direct runoff from new impervious areas to a treatment train consisting of deep sump catch basins with hoods, proprietary water quality units (Contech CDS), and a subsurface infiltration system. Calculations are provided that demonstrate the required 80% TSS removal and 1" Water Quality Volume can be provided with the deep sump catch basin and infiltration basin treatment train.

Higher Potential Pollutant Loads (Standard Number 5): *Stormwater discharges from Land Uses with Higher Potential Pollutant Loads require the use of specific stormwater management BMPs.*

SW21. Provide the total number of estimated trips per day for the site. If the number exceeds 1,000 the site is considered a high-intensity-use parking area and is therefore LUHPPL. **MAI:** *The site will generate, on average 800 - 1,000 trips per day and is therefore is not considered a LUHPPL.* **BETA2:** **The traffic report indicates the daily trips are 1,050; therefore, the site is considered a LUHPPL. BETA notes this classification is not anticipated to require any stormwater modifications.** **MAI2:** *MAI concurs with the above statement.* **BETA3:** **No further comment.**

Critical Areas (Standard Number 6): *Stormwater discharges to critical areas must utilize certain stormwater management BMPs approved for critical areas.*

The project includes discharges to a Zone II Wellhead Protection Area, a critical area, and 44% pretreatment is required prior to infiltration. The proposed treatment trains are consistent with the recommendations of MassDEP for discharges to Zone II wellhead protection areas.

SW22. Revise narrative to correctly indicate the presence of a critical area. **MAI:** *The narrative has been revised accordingly.* **BETA2:** **Narrative revised – issue resolved.**

SW23. Provide calculation based upon MassDEP's "Standard Method to Convert Required Water Quality Volume to a Discharge Rate for Sizing Flow Based Manufactured Proprietary Stormwater Treatment Practices" to demonstrate the Contech Structures are capable of treating the calculated discharge rate and will remove a minimum of 44% TSS prior to infiltration. **MAI:** *MAI has reached out to Contech to obtain the documentation required that demonstrates that the Contech structures are capable of treating the calculated discharge rate and will remove a minimum of 44% TSS prior to infiltration. That documentation can be found in the Appendix of this report.* **BETA2:** **The provided information does not appear to show the DEP calculated water quality flow rate compared to the maximum treatment rate provided by the Contech unit – issue remains outstanding.** **MAI2:** *DEP calculated water quality flow rates compared to the maximum treatment rate provided by the Contech unit have been provided.* **BETA3:** **BETA calculated the required water quality flow rate per DEP guidance (0.98 cfs) and determined it is less than the provided treatment capacity of the Contech unit (1.4 cfs) – issue resolved.**

Redevelopment (Standard Number 7): *Redevelopment of previously developed sites must meet the Stormwater Management Standards to the maximum extent practicable.*

The project does not qualify as redevelopment – not applicable.

SW24. Revise narrative to remove references to "70 Frank Mossberg Drive" and that the project qualifies as a redevelopment. **MAI:** *The narrative has been revised accordingly.* **BETA2:** **Narrative revised – issue resolved.**

Construction Period Erosion and Sediment Controls (Standard Number 8): *Erosion and sediment controls must be implemented to prevent impacts during construction or land disturbance activities.*

The project as currently depicted will disturb greater than one acre of land; therefore, a Notice of Intent with EPA and a Stormwater Pollution Prevention Plan (SWPPP) is required. The project plans indicate the use of a stabilized construction entrance, silt sacks, and perimeter erosion controls (Filtermitt).

SW25. Provide perimeter controls along the southwestern border of the Site (e.g. where existing flows are directed to DP1). *MAI: Perimeter erosion controls have been added to the plan set, refer to Sheets C 1.0 and C 2.0. BETA2: Perimeter controls provided – issue resolved.*

SW26. Revise Temporary Stabilized Construction Entrance Detail to be a continuous width of 20 feet as depicted on the Layout, Grading, and Erosion Control Plan. *MAI: The temporary Stabilized Construction Entrance Detail has been revised to be a continuous width of 20 feet. BETA2: Detail revised – issue resolved.*

Operations/maintenance plan (Standard Number 9): *A Long-Term Operation and Maintenance Plan shall be developed and implemented to ensure that stormwater management systems function as designed.*

A Long-Term Operation and Maintenance (O&M) Plan has been provided.

SW27. Provide long-term maintenance measures for catch basins and Contech water quality units. *MAI: The Operation and Maintenance Plan has been revised accordingly. BETA2: Information provided – issue resolved.*

SW28. Provide a plan that shows the location of all stormwater BMPs as part of the O&M Plan. *MAI: A plan that depicts the stormwater BMP's has been added to the O&M Plan. BETA2: Plan provided – issue resolved.*

SW29. Provide an estimated O&M budget. *MAI: An estimated O&M Budget will be provided prior to construction. BETA2: To avoid a condition of approval that would require this information to be provided in the future, it is recommended to estimate the O&M budget at this time with the understanding that it can be modified prior to construction, if necessary. MAI2: An estimated annual budget of \$90,000 - \$95,000 has been added to the O&M. BETA3: Information provided – issue resolved.*

Illicit Discharges (Standard Number 10): *All illicit discharges to the stormwater management systems are prohibited.*

The Stormwater Management Report indicates that no illicit discharges are proposed, and a signed Illicit Discharge Compliance Statement will be provided prior to construction.

SW30. Provide a signature on the Illicit Discharge Compliance Statement. *MAI: A signature has been added to the Illicit Discharge Compliance Statement. BETA2: Signature provided – issue resolved.*

Mr. Anthony Padula, Chairman

September 24, 2020

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If we can be of any further assistance regarding this matter, please contact us at our office.

Very truly yours,
BETA Group, Inc.



Matthew J. Crowley, PE
Project Manager



Stephen Borgatti
Staff Engineer

cc: Amy Love, Planner
Jen Delmore, Conservation Agent



TOWN OF FRANKLIN

DEPARTMENT OF PUBLIC WORKS

Franklin Municipal Building
257 Fisher Street
Franklin, MA 02038-3026

September 24, 2020

Mr. Anthony Padula, Chairman
Members of the Franklin Planning Board
355 East Central Street
Franklin, MA 02038

RE: Special Permit & Site Plan – 164 Grove St, Dispensary

Dear Mr. Chairman and Members:

We have reviewed the submitted materials for the subject project and offer the following comments:

1. The existing Planet Fitness driveway has asphalt berm and drainage structures, but there is no identifiable turnaround areas for vehicles that pass by the main entrance for #164 Grove St.
2. There is an existing curb cut along Grove St for this property that will not be used. The revised plans show a granite curb radius along the widened Planet Fitness driveway and what appears an extension of granite curb along Grove St. The curb material to be used in extending along Grove St to close the existing curb cut should be identified on the plan.
3. The latest revisions show widening of the pavement on the Planet Fitness driveway to accommodate a turn lane. This additional new impervious area needs to be accounted for in the drainage model. If runoff from this area is going into an existing drainage system, the size and condition of the existing system should be verified to ensure it can handle the additional flow.
4. We note that the designer has taken the existing roadway drainage outfall into account in their design and will grade a swale to a drainage basin at the rear of the property. However, no design calculations have been provided for this basin as to its sizing or potential overflow. Any overflow from this area would flow onto the pavement in front of Planet Fitness.
5. Under the revised stormwater model, the peak elevation for the 100 yr storm exceeds the top of the stone for the infiltration bed.

Should you have any questions or require additional information, please do not hesitate to contact me.

Sincerely,

A handwritten signature in black ink, appearing to read 'M Maglio', with a long horizontal flourish extending to the right.

Michael Maglio, P.E.
Town Engineer



**FRANKLIN PLANNING & COMMUNITY
DEVELOPMENT**

355 EAST CENTRAL STREET, ROOM 120
FRANKLIN, MA 02038-1352
TELEPHONE: 508-520-4907

MEMORANDUM

DATE: September 23, 2020
TO: Franklin Planning Board
FROM: Department of Planning and Community Development
RE: 164 Grove Street – PharmaCann
Special Permit & Site Plan

The DPCD has reviewed the above referenced Special Permit & Site Plan Modification application for the Monday, September 28, 2020 Planning Board meeting and offers the following commentary:

General:

1. The site is approximately 1.5 acres and is located at 164 Grove Street in the Industrial Zoning and Marijuana Overlay District; Assessor's Map 306 Lot 004.
2. The Applicant is proposing to construct a 4,150 square foot building with 70 parking spaces. The main use of the building is for retail Marijuana. There will be no product manufacturing, testing or research operations at the Facility.
3. Applicant has filed the following Special Permits:
 - To allow Non-medical retail marijuana facility under 185 Attachment 3, Part II Section 2.23.
 - To allow Medical retail marijuana facility under 185-49 Attachment 4, Section 4.2 (a)
 - Common Driveway for 2 plus lots under 185-21(F).

Comments from the September 14, 2020 Meeting:

1. The Board expressed concern about the driveway access. Can the existing driveway handle the increase in traffic.
2. Is there a turn around area on the access driveway should a customer miss the entrance?
3. Applicant has indicated it will operate as Appointment only for the first 30 days, and requests that the Planning Board review this after the 30 days of opening.
4. Hours of operation will be 9:00am – 9:00pm seven days a week.
5. The Board expressed concern for employee parking. The applicant has provided 20 parking spaces for employees.

Comments from the July 27, 2020 Meeting:

1. Frontage Requirement – The Applicant has frontage on Grove Street and will access the site using a common driveway.
2. BETA is currently reviewing the Traffic study.
3. Fire Department should review the revised plans and provide a comment on the traffic flow through the site.
4. Will they operate as Appointment only, or will there be walk-ins accepted?
5. Traffic Concerns – Mr. Halligan requested a simple chart at the end of the applicant's report of the actual traffic counts for vehicles coming and going.
6. The applicant has not shown on the plans if there will be any signage on Grove Street or color renderings of the building. If any signage is installed, the Applicant is required to submit to Design Review Commission.

Records on File:

1. Application for Site Plan and Special Permit
2. Certificate of Ownership
3. Special Permit Criteria
4. Abutters certified mailing
5. Overview of Proposed project and Special Permit Findings
6. Site Plans
7. Stormwater Management Plans



September 24, 2020

Mr. Anthony Padula, Chairman
355 East Central Street
Franklin, MA 02038

Re: 164 Grove Street
Traffic Peer Review

Dear Mr. Padula:

BETA Group, Inc. (BETA) has reviewed the traffic related responses to comments and supplemental traffic related documents provided by the applicant for the proposed Site Plan Approval application, "*Permit Site Development Plans - 164 Grove Street, Franklin, Massachusetts.*" This letter is provided to outline findings, comments, and recommendations.

BASIS OF REVIEW

The following documents were received by BETA and formed the basis of the review:

- Plans (10 Sheets) entitled *Permit Site Development Plans* dated May 5, 2020, revised August 20, 2020 and prepared by Meridian Associates of Beverly, MA.
- *Traffic Impact Assessment*, dated August 18, 2020 and prepared by Greenman-Pederson, Inc., Wilmington, MA.
- *Response to Comments*, dated September 17, 2020 and prepared by Greenman-Pederson, Inc., Wilmington, MA.

COMPILED REVIEW LETTER KEY

BETA reviewed this project previously and provided review comments in a letter to the Board dated September 10, 2020 (original comments in standard text), Greenman-Pederson, Inc. (GPI) provided responses (responses in *italic text*), and BETA has provided response comments (status in standard bold text).

INTRODUCTION

The project site consists of 164 Grove Street, a vacant lot developed with a small cleared area and gravel driveway (the "Site"). The parcel contains an area of 1.5 Acres and is located along the eastern side of Grove Street. The Site and all surrounding properties are located within the Industrial Zoning District.

The existing Site includes a gravel driveway connecting to Grove Street which extends into the center of the Site. This central area is an undeveloped area surrounded by small trees. A bar gate located along the driveway restricts access into the Site. A chain link fence connects to this gate and surrounds the perimeter of the Site.

The Applicant proposes to remove the existing fence, driveway, and vegetation and construct a new 4,150 sq. ft. Non-Medical Marijuana Retail Establishment. Associated site developments will include two new paved parking lots, and two driveway aprons connecting to the existing driveway to the south adjacent to Planet Fitness and Franklin Tile which leads to Grove Street.

FINDINGS, COMMENTS AND RECOMMENDATIONS

The study area includes the following intersections.

- Grove Street at Site/Planet Fitness/Franklin Tile Driveway (unsignalized)
- Grove Street at Route 140/West Central Street (signalized)
- Grove Street at Washington Street (unsignalized)

BETA finds the study area to be acceptable.

Existing traffic conditions were determined by using manual turning movement counts (TMCs) and automatic traffic recorder (ATR) data from the traffic study for 162 Grove Street conducted by Tetra Tech. TMC data was collected on Thursday, February 6th, 2020 from 7:00 AM to 9:00 AM and 4:00 PM to 6:00 PM, and Saturday, February 8th, 2020 from 3:00 PM to 6:00 PM. ATR data was collected on Grove Street, south of 162 Grove Street, over a 72-hour period between Thursday, February 6th, 2020 and Saturday, February 8th, 2020.

Historical traffic count data collected by MassDOT were reviewed for the years 2017-2019 to determine the need for a seasonal adjustment. Traffic volumes in February were found to be 2.3% below average-month conditions. As a result, the volumes were increased by 2.3% to reflect average-month condition baseline volumes. Based on the three years of seasonal factor data, the month of February does not appear to have lower than average months for U4-U7 roadways.

- T1. Verify the 2.3% seasonal adjustment average for February. *GPI: GPI calculated the seasonal adjustment average based on the most recent three years of available data (2017-2019) for Factor Group U4-U7 (Minor arterial, major and minor collector, and local road).* BETA2: Information provided – issue resolved.

The existing traffic at the Grove Street at Site/Planet Fitness/Franklin Tile Driveway is atypical due to the COVID-19 related restrictions. To account for the vehicle movements into and out of the existing driveway, volumes were estimated using the Institute of Transportation Engineers (ITE) *Trip General Manual* land use code (LUC) 492 – Health/Fitness club and LUC 820 Shopping Center. BETA finds this approach to be reasonable.

Crash data were obtained from the MassDOT database for the most recent five-year period from 2014 to 2018. All crash rates were calculated to be less than the district and statewide averages. Crash data backup worksheets were not provided for the intersections of Grove Street at Route 140/West Central Street and Grove Street at Washington Street.

- T2. For completeness of the Appendix, provide the backup crash rate worksheets for the intersections of Grove Street at Route 140/West Central Street and Grove Street at Washington Street. *GPI: GPI has provided the backup crash rate worksheet as an Attachment to this letter. This information was originally included in the Transportation Impact Assessment for the Proposed*

Warehouse/Distribution Building located at 176-210 Grove Street in Franklin Massachusetts prepared by Vanasse & Associates, Inc. (VAI) and was referenced in the TIA for the development at #164 Grove Street. BETA2: Information provided – issue resolved.

Vehicle speeds were measured via ATR along Grove Street. The posted speed limit on Grove Street is 40 miles per hour (mph). The 85th percentile speeds were measured at 40 mph northbound and 41 mph southbound, which are acceptable for a posted 40 mph roadway.

The available intersection (ISD) and stopping sight distance (SSD) at the Grove Street driveway were measured and found to exceed the minimum required SSD based on measured vehicle speeds. Additionally, a Sight Distance Plan was provided. BETA concurs with the sight distance analysis.

Background development-related traffic growth that may increase traffic within the study area was identified. The following proposed projects were included in the background development:

- 160 Grove Street
- 162 Grove Street
- 176-210 Grove Street

It should be noted that the 162 Grove Street trip generation data utilized in this report is reflective of the proposed trips prior to the change to a Reserve Ahead Only type facility. The Reserve Ahead Only facility generates significantly lower peak hour trips. Therefore, the trips applied to the traffic volumes in the TIA provides more conservative traffic volumes.

No-Build traffic volumes were determined by applying a 1 percent per year growth rate over a seven-year period to 2027. This growth rate is consistent with studies prepared for recent developments in Franklin.

Project-generated traffic volumes were determined by utilizing trip-generation statistics published by the Institute of Transportation Engineers (ITE) for LUC 882 – Marijuana Dispensary. Based on the Institute of Transportation Engineers (ITE) for LUC 882 – Marijuana Dispensary, the site would generate a total of 1,048 new trips on an average weekday and with 91 (46 entering, 45 exiting) during the weekday afternoon peak hour. The Saturday daily trips of 1,076 and afternoon peak trips are 151 (76 entering, 75 exiting).

T3. Clarify if the facility will be open during the morning peak period. If it will be open, then provide trip data for the morning peak period. *GPI: The Applicant has confirmed that the hours of operation will be from 9:00 AM to 9:00 PM seven days per week. Therefore, the proposed facility is not anticipated to generate a significant volume of traffic during the weekday AM peak hour.* BETA2: Information provided – issue resolved.

T4. The TIA states that “each employee was assumed to generate two vehicle trips” but it is unclear whether those trips were allocated during the peak periods or if those trips were included at all. Please clarify. *GPI: Employee-related trips were included in the calculations of Weekday Daily trips generated by the proposed facility. However, employee shifts will be scheduled to avoid travel during the weekday PM and Saturday midday peak hours, as these also represent peak hours of sales for the proposed facility. Therefore, no employee trips were allocated during these peak periods. Subsequent to completion of the TIA and in response to comments T5 and T6, GPI collected empirical trip generation counts at a similar facility operated by the Applicant in*

Shrewsbury, MA. The results of these counts are described in the responses to T5 and T6. BETA2: Information provided – issue resolved.

Additionally, empirical trip data collected at a similar facility in Wareham were provided. The empirical data shows less peak hour trips are generated than the ITE data. Therefore, the proponent utilized the ITE data to provide a more conservative condition.

- T5. Clarify how many registers will be provided in the proposed Franklin facility and at the existing Wareham facility. *GPI: "The existing facility in Wareham operates with seven (7) registers. The proposed facility will provide a total of ten (10) registers; however, the facility will have only five (5) registers open for approximately 85% of the time. The remaining registers will be available to accommodate opening conditions, peak hours of operation, or temporary influxes in patrons. Therefore, the proposed facility is expected to have similar patronage to the Wareham facility. The Applicant currently operates a facility on Boston Turnpike in Shrewsbury with a similar model of ten (10) registers. Therefore, empirical trip generation counts were collected at this location between 12:00 PM and 4:30 PM on Friday, September 12 and Saturday, September 13, 2020 to provide a comparison to the Institute of Transportation Engineers (ITE) trip rates. This location was selected over the Wareham location as it operates with the same number of registers and has a separate parking lot that accommodates all patrons, while the Wareham facility utilizes on-street parking. With a 10-register model, typically five (5) of the registers will remain open all day, with the ability to open additional registers if an influx of customers occurs or certain customers require additional assistance. Although the sales on Saturday represented the highest sales the Shrewsbury location has had since it opened in July, the facility was operating with only 80 percent (4 of 5) of its registers open. Therefore, the counts collected on Friday and Saturday were increased by a factor of 1.25 to represent 100 percent of the registers being open...the trip generation counts collected at the Shrewsbury, MA location were lower than the site-generated trips estimated based on ITE trip rates and Applicant-provided data on projected patronage. Therefore, the traffic projections included in the TIA represent a conservative (worse than expected) analysis condition. "*
BETA2: Information provided – issue resolved.

New trips were distributed based on 2010 U.S. Census data with 50 percent of traffic heading to and from Washington Street and the remaining 50 percent heading to and from Route 140. BETA finds this distribution reasonable.

Traffic operations analysis was performed with Synchro software based on the HCM 6th Edition methodologies. The site driveway would experience a LOS C or better during the Build conditions.

During the weekday PM Peak Hour, the Grove Street northbound right-turn movement onto Route 140 would degrade from a LOS D during the Existing conditions to LOS F during the Build conditions.

The analysis results indicate that the Grove Street southbound movement at Washington Street would experience LOS F delays during the Build condition. The Synchro analysis modeled the Grove Street southbound approach to Washington Street as a two-lane approach, which does not accurately reflect the Grove Street lane configuration at the intersection. If the intersection were reanalyzed to accurately reflect the field conditions, the results would reveal even more significant delays and queue lengths.

The TIA indicates that the number of parking spaces anticipated for the peak period is 48 parking spaces based on parking generation rates provided in the ITE *Parking Generation Manual*. There are currently 70 proposed parking spaces.

T6. If available, empirical data of 15-minute interval parking demands for a similar facility, such as the Wareham location, should be provided to further support the proposed parking supply. GPI: *"GPI conducted parking counts at a location in Shrewsbury, MA on Friday, September 11, 2020 and Saturday, 12, 2020 from 12:00 – 4:30 PM which are the peak hours of operation. This location was chosen over the Wareham location because the client confirmed that there are 10 registers provided at this location which will be more similar to the proposed Franklin facility. In addition, the Shrewsbury location has its own dedicated off-street parking lot to accommodate all patron parking. The facility in Shrewsbury provides a total of 30 parking spaces for patron use. Employees park in an off-site lot and are shuttled to the site. During the peak hours of operation, as many as 15 employees may be on-site during the peak hours of operation. Therefore, the parking demand for employees was assumed to be 15 vehicles. During the peak time period, a total of 13 parking spaces were occupied on-site. It should be noted that although the day of the count represented a record-sales day for the Shrewsbury location, only 80 percent of the registers were open on that day. Therefore, the parking demand for patrons was increased accordingly to represent a full operation day with all registers open. Therefore, the anticipated peak parking demand for the Shrewsbury location was estimated to be 32 parking spaces. On Saturday, September 12, 2020, the Shrewsbury location experienced a record of 235 customers. Based on Applicant-provided data on projected customers, the Franklin location could service up to 500 customers on a maximum operation day with all registers open, which would require up to 15 employees on-site at any time. Adjusting the patron parking demand from the Shrewsbury location by a factor of 2.13 to represent maximum operations of 500 patrons per day, would result in a total patron parking demand of 28 parking spaces and an employee demand of 15 spaces, for a total demand of 40 parking spaces. Therefore, the proposed parking supply of 70 spaces will be adequate to accommodate the parking demand generated on a maximum operations day with additional available parking to avoid excess recirculation of vehicles to find empty spaces in the lot and will ensure patrons do not park in spaces at #166 Grove Street."* BETA2: Information provided – issue resolved.

The TIA indicates that a six-month post-occupancy traffic monitoring program should be conducted to evaluate whether improvements are warranted at the Grove Street and Washington Street and Grove Street and Route 140 intersections post COVID-19 traffic volume changes. The proponent agrees to provide a monetary contribution towards funding the monitoring study.

T7. For clarification purposes, does the proponent intend on funding the entire post-occupancy study or a portion of the study? GPI: *"The Applicant has committed to providing a total of \$25,900.00 as a fair-share commitment to the Town of Franklin to be used toward conducting a post-occupancy monitoring study and constructing improvements at the study area intersections. The estimation of this contribution assumes that the Applicant will fund 100 percent of the estimated cost of the post-occupancy monitoring, although this study will include the impacts of multiple other developments in the area. The total estimated cost of the monitoring study is \$8,500...The remaining \$17,400.00 of the Applicant's contribution can be used toward the design or construction of improvements along Grove Street or at the intersections of Grove Street with Route*

140 or Washington Street...To estimate this contribution, GPI prepared a preliminary construction cost estimate for improvements at the Grove Street intersections with Route 140 and Washington Street. The detailed calculations are provided as an Attachment to this letter..."
BETA2: The estimated construction cost by GPI to signalize the Grove Street and Washington Street intersection is \$244,500. The estimated cost does not include any right-of-way impacts or roadway widening which will be inevitable as part of the signalization effort with the addition of a turn lane on Grove Street.

T8. BETA recommends that the intersection of Grove Street and Site/Planet Fitness/Franklin Tile Driveway be included in the post-occupancy data collection effort. *GPI: The Applicant agrees to include the Grove Street and Site/Planet Fitness/Franklin Tile Driveway as part of the post-occupancy monitoring study as described in the draft scope in response to Comment T7.* BETA2: Intersection included – issue resolved.

T9. BETA suggests that the proponent also collect the driveway data prior to opening and when the driveway businesses are fully operational as a baseline. *GPI: The Applicant concurs with BETA's suggestion to collect baseline counts at the driveway when the Planet Fitness and Franklin Tile Carpet One Floor & Home are fully operational as described in response to Comment T7.* BETA2: Intersection baseline included – issue resolved.

Additionally, the proponent agrees to provide a fair share contribution towards implementation of improvements at the intersections proportional to the percentage increase in traffic generated by the development.

T10. Clarify if a "Reserve Ahead Only" option was considered for the proposed facility in order to control the amount of traffic generated on the adjacent roadways during peak periods and on site? *GPI: The Applicant plans to open under a reservation system to manage the number of patrons on site at any time. While orders can be placed ahead of time online for rapid pick-up, patrons will also be able to make selections while on-site at the time of their appointment. Following initial opening and lifting of COVID-related regulations on occupancy, the Applicant may allow walk-in service in the future, but will still maintain an online reservation system to manage traffic flow and allow patrons to guarantee rapid service.* BETA2: BETA defers to the Board regarding the approval timeline for walk in service.

A capacity analysis was done for the proposed shared driveway improvement which would consist of an exclusive left-turn lane and an exclusive 100-foot right turn lane exiting the shared driveway onto Grove Street. The analysis shows the driveway will operate at LOS D or better during the peak periods with minimal queuing.

BETA's initial sight distance review considered that standard setbacks were not used to determine the sight distances due to the Grove Street curvature approaching the driveway from the south. A turning vehicle from the existing driveway needs to move up closer to Grove Street to obtain sufficient sight distance. Now that an additional exit lane is being proposed, the vehicle turning right would most likely now be able to exit until the left turning vehicle exits due to the sight obstruction.

T11. Based on concerns expressed by the Board and the addition of a two-lane driveway approach, the designer should confirm how the site distance was originally calculated and verify any safety

Mr. Anthony Padula, Chairman
September 24, 2020
Page 7 of 7

impacts presented by the two lane configuration considering the existing curvature of Grove Street.

If we can be of any further assistance regarding this matter, please contact us at our office.

Very truly yours,
BETA Group, Inc.



Jaklyn Centracchio, PE, PTOE
Senior Project Engineer

cc: Amy Love, Planner
Job No: 4830-65



September 17, 2020

NEX-2020163.00

Mr. Anthony Padula, Chairman
 Town of Franklin
 355 East Central Street
 Franklin, MA 021038

SUBJECT: 164 Grove Street – Traffic Impact Assessment
 Response to Comments

Dear Mr. Padula:

Greenman-Pedersen, Inc. (GPI) previously prepared a *Traffic Impact Assessment (TIA)* dated August 18, 2020 for the proposed marijuana dispensary to be located at 164 Grove Street in Franklin, Massachusetts. The findings of this study were reviewed by the Town’s Peer Review consultant BETA Group, Inc. (BETA) in a letter dated September 10, 2020. This letter is intended to respond to the comments raised by BETA. The original comments are provided in italics and GPI’s responses are provided in normal text. In addition, since completion of the TIA, the site plan has been modified to address comments from the adjacent property owner at #166 Grove Street regarding the shared access driveway. This letter provides an updated analysis of the operations at the Grove Street / Site Driveway intersection with the modified access layout.

Responses to Peer Review Traffic Comments

Comment T1: *Verify the 2.3% seasonal adjustment average for February.*

Response T1: GPI calculated the seasonal adjustment average based on the most recent three years of available data (2017-2019) for Factor Group U4-U7 (Minor arterial, major and minor collector, and local road). The seasonal adjustment factors for the month of February are summarized below in Table 1.

TABLE 1
Existing Traffic Volume Summary

U4-U7	Weekday Seasonal Factors
2017	1.05
2018	1.02
2019	1.00
Average	1.023

Comment T2: *For completeness of the Appendix, provide the backup crash rate worksheets for the intersections of Grove Street at Route 140/West Central Street and Grove Street at Washington Street.*

Response T2: GPI has provided the backup crash rate worksheet as an Attachment to this letter. This information was originally included in the Transportation Impact Assessment for the Proposed Warehouse / Distribution Building located at 176-210 Grove Street in Franklin Massachusetts prepared by Vanasse & Associates, Inc. (VAI) and was referenced in the TIA for the development at #164 Grove Street.

Comment T3: *Clarify if the facility will be open during the morning peak period. If it will be open, then provide trip data for the morning peak period.*

Response T3: The Applicant has confirmed that the hours of operation will be from 9:00 AM to 9:00 PM seven days per week. Therefore, the proposed facility is not anticipated to generate a significant volume of traffic during the weekday AM peak hour.

Comment T4: *The TIA states that “each employee was assumed to generate two vehicle trips” but it is unclear whether those trips were allocated during the peak periods or if those trips were included at all. Please clarify.*

Response T4: Employee-related trips were included in the calculations of Weekday Daily trips generated by the proposed facility. However, employee shifts will be scheduled to avoid travel during the weekday PM and Saturday midday peak hours, as these also represent peak hours of sales for the proposed facility. Therefore, no employee trips were allocated during these peak periods. Subsequent to completion of the TIA and in response to comments T5 and T6, GPI collected empirical trip generation counts at a similar facility operated by the Applicant in Shrewsbury, MA. The results of these counts are described in the responses to T5 and T6.

Comment T5: *Clarify how many registers will be provided in the proposed Franklin facility and at the existing Wareham facility.*

Response T5: The existing facility in Wareham operates with seven (7) registers. The proposed facility will provide a total of ten (10) registers; however, the facility will have only five (5) registers open for approximately 85% of the time. The remaining registers will be available to accommodate opening conditions, peak hours of operation, or temporary influxes in patrons. Therefore, the proposed facility is expected to have similar patronage to the Wareham facility.

The Applicant currently operates a facility on Boston Turnpike in Shrewsbury with a similar model of ten (10) registers. Therefore, empirical trip generation counts were collected at this location between 12:00 PM and 4:30 PM on Friday, September 12 and Saturday, September 13, 2020 to provide a comparison to the Institute of Transportation Engineers (ITE) trip rates. This location was selected over the Wareham location as it operates with the same number of registers and has a separate parking lot that accommodates all patrons, while the Wareham facility utilizes on-street parking.

With a 10-register model, typically five (5) of the registers will remain open all day, with the ability to open additional registers if an influx of customers occurs or certain customers require additional assistance. Although the sales on Saturday represented the highest sales the Shrewsbury location has had since it opened in July, the facility was operating with only 80 percent (4 of 5) of its registers open. Therefore, the counts collected on Friday and Saturday were increased by a factor of 1.25 to represent 100 percent of the registers being open.

The detailed count data is provided as an Attachment to this letter and is compared to the previously submitted trip generation data using ITE trip rates and Applicant-provided data on projected sales within Table 2.

As shown in Table 2, the trip generation counts collected at the Shrewsbury, MA location were lower than the site-generated trips estimated based on ITE trip rates and Applicant-provided data on projected patronage. Therefore, the traffic projections included in the TIA represent a conservative (worse than expected) analysis condition.

TABLE 2
Trip Generation Comparison

Time Period	Estimated Site-Generated Trips		
	ITE Trip Rates	Applicant-Provided Data	Empirical Counts in Shrewsbury
Weekday PM Peak Hour:			
Enter	46	40	35
Exit	<u>45</u>	<u>40</u>	<u>35</u>
Total	91	80	70
Saturday Afternoon Peak Hour:			
Enter	76	60	31
Exit	<u>75</u>	<u>60</u>	<u>33</u>
Total	151	120	64

Comment T6: *If available, empirical data of 15-minute interval parking demands for a similar facility, such as the Wareham location, should be provided to further support the proposed parking supply.*

Response T6: GPI conducted parking counts at a location in Shrewsbury, MA on Friday, September 11, 2020 and Saturday, 12, 2020 from 12:00 – 4:30 PM which are the peak hours of operation. This location was chosen over the Wareham location because the client confirmed that there are 10 registers provided at this location which will be more similar to the proposed Franklin facility. In addition, the Shrewsbury location has its own dedicated off-street parking lot to accommodate all patron parking.

The facility in Shrewsbury provides a total of 30 parking spaces for patron use. Employees park in an off-site lot and are shuttled to the site. During the peak hours of operation, as many as 15 employees may be on-site during the peak hours of operation. Therefore, the parking demand for employees was assumed to be 15 vehicles. During the peak time period, a total of 13 parking spaces were occupied on-site. It should be noted that although the day of the count represented a record-sales day for the Shrewsbury location, only 80 percent of the registers were open on that day. Therefore, the parking demand for patrons was increased accordingly to represent a full operation day with all registers open. Therefore, the anticipated peak parking demand for the Shrewsbury location was estimated to be 32 parking spaces.

On Saturday, September 12, 2020, the Shrewsbury location experienced a record of 235 customers. Based on Applicant-provided data on projected customers, the Franklin location could service up to 500 customers on a maximum operation day with all registers open, which would require up to 15 employees on-site at any time. Adjusting the patron parking demand from the Shrewsbury location by a factor of 2.13 to represent maximum operations of 500 patrons per day, would result in a total patron parking demand of 28 parking spaces and an employee demand of 15 spaces, for a total demand of 40 parking spaces. Therefore, the proposed parking supply of 70 spaces will be adequate to accommodate the parking demand generated on a maximum operations day with additional available parking to avoid excess recirculation of vehicles to find empty spaces in the lot and will ensure patrons do not park in spaces at #166 Grove Street.

Comment T7: *For clarification purposes, does the proponent intend on funding the entire post-occupancy study or a portion of the study?*

Response T7: The Applicant has committed to providing a total of \$25,900.00 as a fair-share commitment to the Town of Franklin to be used toward conducting a post-occupancy monitoring study and constructing improvements at the study area intersections. The estimation of this contribution assumes that the Applicant will fund 100 percent of the estimated cost of the post-occupancy monitoring, although this study will include

the impacts of multiple other developments in the area. The total estimated cost of the monitoring study is \$8,500.00, which includes the following scope of work:

- Conduct baseline manual turning movement counts at the Grove Street / #166 Grove Street Driveway intersection during the weekday AM (7:00 – 9:00 AM), weekday PM (4:00 – 6:00 PM), and Saturday midday (11:00 AM – 2:00 PM) peak periods to verify the existing traffic volumes on the driveway when the businesses are fully operational and prior to opening of the marijuana use.
- Conduct post-occupancy, post-COVID manual turning movement counts at the following intersections during the weekday AM (7:00 – 9:00 AM), weekday PM (4:00 – 6:00 PM), and Saturday midday (11:00 AM – 2:00 PM) peak periods:
 - Route 140 / Grove Street
 - Grove Street / #166 Grove Street Driveway
- Verify that actual Project-generated trips is consistent with or less than the trips estimated as part of the TIA at the site driveway.
- Conduct capacity and queue analyses for each of the three analysis time periods at the Route 140 / Grove Street intersection to assess the need for signal timing modifications.
- If determined necessary, develop signal timing plans for the proposed new timings at the Route 140 / Grove Street intersection.
- Conduct manual turning movement counts over a 12-hour period (7:00 AM to 7:00 PM) on a weekday and during the Saturday midday (11:00 AM to 2:00 PM) on a Saturday at the Grove Street / Washington Street intersection.
- Evaluate whether the existing traffic volumes at the Grove Street / Washington Street intersection meet any of the volume-related warrants for installation of a traffic signal.
- Conduct capacity and queue analyses for the three analysis time periods at the Grove Street / Washington Street intersection and assess the need for any geometric measures to improve intersection operations.
- Prepare a memorandum summarizing the findings and recommendations of the post-occupancy monitoring study.

The remaining \$17,400.00 of the Applicant's contribution can be used toward the design or construction of improvements along Grove Street or at the intersections of Grove Street with Route 140 or Washington Street. To estimate this contribution, GPI prepared a preliminary construction cost estimate for improvements at the Grove Street intersections with Route 140 and Washington Street. The detailed calculations are provided as an Attachment to this letter and a summary is provided in Table 3 below. GPI estimated the Applicant's contribution based on the percentage increase in traffic generated by the proposed development through each of the study area intersections during the weekday PM peak hour (the time period for which the improvements are required). The cost estimate assumed implementation of signal timing improvements at the Route 140 / Grove Street intersection, with installation of a new traffic signal and associated roadway work to accommodate dedicated turning lanes and signalized pedestrian crossings at the Grove Street / Washington Street intersection.

TABLE 3
Applicant’s Fair Share Contribution Summary

Improvement Measure	Estimated Construction Cost	Applicant’s % Increase Thru Location	Applicant’s Fair-Share Contribution
Post-Occupancy Monitoring Study	\$8,500.00	100%	\$8,500.00
Route 140 / Grove Street Signal Timing Modifications	\$5,000.00	4%	\$200.00
Washington Street / Grove Street Traffic Signal Installation and Roadway Widening	\$244,500.00	7%	\$17,200.00
TOTAL	\$258,000.00	--	\$25,900.00

Comment T8: BETA recommends that the intersection of Grove Street and Site/Planet Fitness/Franklin Tile Driveway be included in the post-occupancy data collection effort.

Response T8: The Applicant agrees to include the Grove Street and Site/Planet Fitness/Franklin Tile Driveway as part of the post-occupancy monitoring study as described in the draft scope in response to Comment T7.

Comment T9: BETA suggests that the proponent also collect the driveway data prior to opening and when the driveway businesses are fully operational as a baseline.

Response T9: The Applicant concurs with BETA’s suggestion to collect baseline counts at the driveway when the Planet Fitness and Franklin Tile Carpet One Floor & Home are fully operational as described in response to Comment T7.

Comment T10: Clarify if a “Reserve Ahead Only” option was considered for the proposed facility in order to control the amount of traffic generated on the adjacent roadways during peak periods and on site?

Response T10: The Applicant plans to open under a reservation system to manage the number of patrons on site at any time. While orders can be placed ahead of time online for rapid pick-up, patrons will also be able to make selections while on-site at the time of their appointment. Following initial opening and lifting of COVID-related regulations on occupancy, the Applicant may allow walk-in service in the future, but will still maintain an online reservation system to manage traffic flow and allow patrons to guarantee rapid service.

Proposed Shared Driveway Improvements

The proposed marijuana dispensary will be accessed via a shared driveway with the adjacent Planet Fitness at #166 Grove Street and Franklin Tile Carpet One Floor & Home (Franklin Tile) at #168 Grove Street. The existing driveway is approximately 24 feet wide, providing adequate width for a single travel lane in each direction entering and exiting the driveway. To facilitate traffic exiting the driveway onto Grove Street, the owner of #166 Grove Street has requested that the driveway be widened to provide separate left- and right-turn lanes exiting onto Grove Street. Therefore, the Applicant is requesting that the Planning Board approve an amended site plan that widens the shared driveway to 37 feet to accommodate two 11-foot wide lanes exiting the site and one 15-foot lane entering the site. In addition, the egress from the proposed marijuana dispensary onto the shared driveway will be restricted to right-out-only.

GPI previously conducted an analysis of the capacity and queuing of the site driveway intersection with Grove Street as part of the TIA using the Synchro analysis software. GPI has updated the analysis of the 2027 Build condition to include the separate left- and right-turn lanes exiting the site driveway. The detailed analysis

worksheets are included as an Attachment to this letter and the results are summarized in Table 4 in comparison to the 2020 Existing and 2027 No-Build conditions. As shown in Table 1, with the proposed improvements and the additional traffic generated by the proposed marijuana dispensary, all movements at the site driveway intersection with Grove Street are anticipated to operate at acceptable levels-of-service (LOS D or better) under all analysis time periods, with queues not exceeding two vehicles.

TABLE 4
Intersection Capacity Analysis Summary

Intersection/Peak Hour/Lane Group	2020 Existing				2027 No-Build				2027 Build with Improvements			
	V/C ^a	Del. ^b	LOS ^c	Queue ^d	V/C	Del.	LOS	Queue	V/C	Del.	LOS	Queue
Grove Street at Planet Fitness Driveway												
<i>Weekday PM:</i>												
Site Driveway WB	0.13	13.5	B	--/<25	0.18	17.0	C	--/<25	--	--	--	--/--
Site Driveway WB left/through	--	--	--	--/--	--	--	--	--/--	0.27	32.9	D	--/28
Site Driveway WB right-turn	--	--	--	--/--	--	--	--	--/--	0.10	11.3	B	--/<25
Grove Street SB	0.04	7.9	A	--/<25	0.04	8.3	A	--/<25	0.07	8.4	A	--/<25
<i>Saturday Afternoon:</i>												
Site Driveway WB	0.10	10.8	B	--/<25	0.13	12.7	B	--/<25	--	--	--	--/--
Site Driveway WB left/through	--	--	--	--/--	--	--	--	--/--	0.22	20.1	C	--/<25
Site Driveway WB right-turn	--	--	--	--/--	--	--	--	--/--	0.11	10.4	B	--/<25
Grove Street SB	0.03	7.7	A	--/<25	0.03	7.9	A	--/<25	0.07	8.1	A	--/<25

^a Volume-to-capacity ratio.

^b Average control delay in seconds per vehicle.

^c Level of service.

^d Average/95th percentile queue length in feet per lane (assuming 25 feet per vehicle).

Mr. Anthony Padula
September 17, 2020
Page 8

Based on the results of the analysis, the driveway has been designed to provide separate 11-foot left- and right-turn lanes with 100 feet of storage to accommodate the anticipated queues of two vehicles, and provide space for an additional one to two vehicles to account for peaking of traffic and delivery trucks exiting the site.

Should you have any questions, or require additional information, please contact me at (603) 766-5223.

Sincerely,

GREENMAN-PEDERSEN, INC.

A handwritten signature in blue ink, appearing to read 'Rebecca L. Brown', is positioned below the company name.

Rebecca L. Brown, P.E.
Senior Project Manager

Enclosures:

Crash Rate Worksheets
Empirical Trip Generation & Parking Demand Counts
Fair-Share Contribution Calculations
Synchro Analysis Worksheets

cc: Jaklyn Centracchio, P.E., PTOE – BETA Group, Inc.
Amy Love – Town of Franklin Planner
David Kelley, P.E. – Meridian Associates, Inc.

MassHighway

CRASH RATE WORKSHEET

CITY/TOWN : Franklin MA COUNT DATE : 2020

DISTRICT : 3 UNSIGNALIZED : SIGNALIZED :

MHD USE ONLY

Source #

~ INTERSECTION DATA ~

MAJOR STREET : Route 140

ST #

MINOR STREET(S) : Grove Street

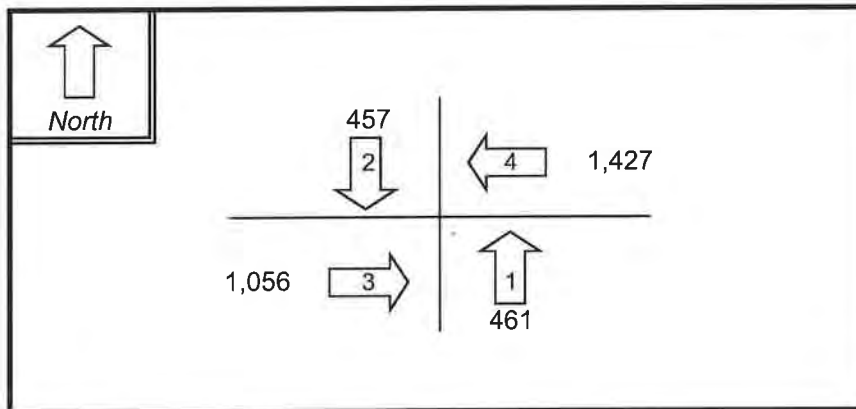
ST #

ST #

ST #

ST #

**INTERSECTION
DIAGRAM**
(Label Approaches)



INTERSECTION
REF #

Peak Hour Volumes

APPROACH :	1	2	3	4	5	Total Entering Vehicles
DIRECTION :	NB	SB	EB	WB		
VOLUMES (PM) :	461	457	1,056	1,427		3,401

"K" FACTOR : APPROACH ADT : ADT = TOTAL VOL/"K" FACT.

TOTAL # OF ACCIDENTS : # OF YEARS : AVERAGE # OF ACCIDENTS (A) :

CRASH RATE CALCULATION : RATE = $\frac{(A * 1,000,000)}{(ADT * 365)}$

Comments : Accident Rate for District 3 signalized intersections = 0.89

Accident Rate for District 3 unsignalized intersections = 0.61

Statewide Accident Rate for Signalized Inteserction = 0.78 and Unsignalized/Inteserction = 0.57

MassHighway

CRASH RATE WORKSHEET

CITY/TOWN : Franklin MA COUNT DATE : 2020

DISTRICT : 3 UNSIGNALIZED : SIGNALIZED :

MHD USE ONLY

Source #

~ INTERSECTION DATA ~

MAJOR STREET : Washington Street

ST #

MINOR STREET(S) : Grove Street

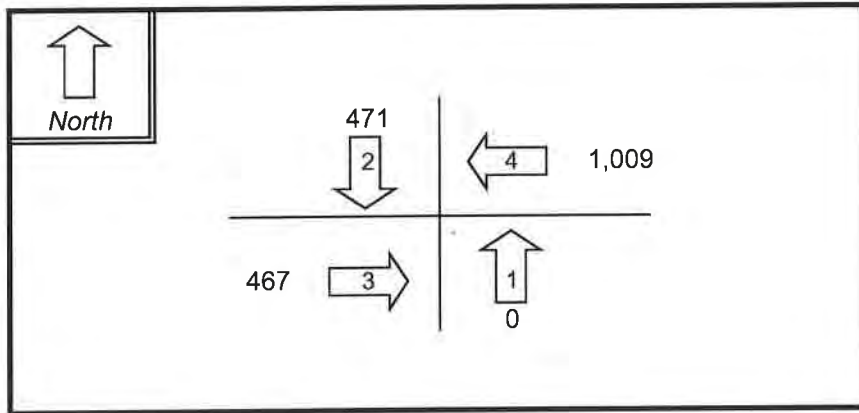
ST #

ST #

ST #

ST #

**INTERSECTION
DIAGRAM**
(Label Approaches)



INTERSECTION
REF #

Peak Hour Volumes

APPROACH :	1	2	3	4	5	Total Entering Vehicles
DIRECTION :	NB	SB	EB	WB		
VOLUMES (PM) :		471	467	1,009		1,947

"K" FACTOR : APPROACH ADT : ADT = TOTAL VOL/"K" FACT.

TOTAL # OF ACCIDENTS : # OF YEARS : AVERAGE # OF ACCIDENTS (A) :

CRASH RATE CALCULATION : RATE = $\frac{(A * 1,000,000)}{(ADT * 365)}$

Comments : Accident Rate for District 3 signalized intersections = 0.89
Accident Rate for District 3 unsignalized intersections = 0.61

Statewide Accident Rate for Signalized Inteserction = 0.78 and Unsignalized/Inteserction = 0.57

TRIP GENERATION COUNT DATA

Location: 939 Boston Turnpike, Shrewsbury, MA (Verilife)
 Project: Proposed Marijuana Dispensary - 164 Grove St. - Franklin, MA
 Weather: Sunny, Clear, Low 70s

Date: 9/11/2020
 Time: 12:00 - 16:30
 Job #: NEX-2020163.00

Raw Data (80% Open Registers / 235 Customers)					
Start Time	Parked Vehicles	IN	OUT	TOTAL	PEAK HOUR
12:00	7	2	2	4	
12:15	6	4	5	9	
12:30	7	2	1	3	
12:45	7	1	1	2	18
13:00	9	4	2	6	20
13:15	12	6	3	9	20
13:30	8	0	4	4	21
13:45	9	1	0	1	20
14:00	12	6	3	9	23
14:15	10	4	6	10	24
14:30	9	5	6	11	31
14:45	8	0	1	1	31
15:00	10	4	2	6	28
15:15	13	7	4	11	29
15:30	6	6	13	19	37
15:45	8	2	0	2	38
16:00	10	6	4	10	42
16:15	13	14	11	25	56
PEAK HOUR	13	28	28	56	56

Adjusted Data (100% Open Registers)					
Start Time	Parked Vehicles	IN	OUT	TOTAL	PEAK HOUR
12:00	9	3	3	5	
12:15	8	5	6	11	
12:30	9	3	1	4	
12:45	9	1	1	3	23
13:00	12	5	3	8	25
13:15	15	8	4	11	25
13:30	10	0	5	5	26
13:45	12	1	0	1	25
14:00	15	8	4	11	29
14:15	13	5	8	13	30
14:30	12	6	8	14	39
14:45	10	0	1	1	39
15:00	13	5	3	8	35
15:15	17	9	5	14	36
15:30	8	8	16	24	46
15:45	10	3	0	3	48
16:00	13	8	5	13	53
16:15	17	18	14	31	70
PEAK HOUR	17	35	35	70	70

TRIP GENERATION COUNT DATA

Location: 939 Boston Turnpike, Shrewsbury, MA (Verilife)
 Project: Proposed Marijuana Dispensary - 164 Grove St. - Franklin, MA
 Weather: Sunny, Clear, Low 70s

Date: 9/12/2020
 Time: 12:00 - 16:30
 Job #: NEX-2020163.00

Raw Data (80% Open Registers / 235 Customers)					
Start Time	Parked Vehicles	IN	OUT	TOTAL	PEAK HOUR
12:00	9	6	6	12	
12:15	10	5	4	9	
12:30	7	5	8	13	
12:45	7	2	2	4	38
13:00	7	6	6	12	38
13:15	9	5	3	8	37
13:30	6	2	5	7	31
13:45	10	8	4	12	39
14:00	11	5	4	9	36
14:15	7	0	4	4	32
14:30	7	1	1	2	27
14:45	10	7	4	11	26
15:00	9	3	4	7	24
15:15	12	6	3	9	29
15:30	11	7	8	15	42
15:45	10	9	10	19	50
16:00	6	2	6	8	51
16:15	11	7	2	9	51
PEAK HOUR	12	25	26	51	51

Adjusted Data (100% Open Registers)					
Start Time	Parked Vehicles	IN	OUT	TOTAL	PEAK HOUR
12:00	12	8	8	15	
12:15	13	6	5	11	
12:30	9	6	10	16	
12:45	9	3	3	5	48
13:00	9	8	8	15	48
13:15	12	6	4	10	46
13:30	8	3	6	9	39
13:45	13	10	5	15	49
14:00	14	6	5	11	45
14:15	9	0	5	5	40
14:30	9	1	1	3	34
14:45	13	9	5	14	33
15:00	12	4	5	9	30
15:15	15	8	4	11	36
15:30	14	9	10	19	53
15:45	13	11	13	24	63
16:00	8	3	8	10	64
16:15	14	9	3	11	64
PEAK HOUR	15	31	33	64	64

APPLICANT'S FAIR SHARE CONTRIBUTION CALCULATION SUMMARY

Improvement Measure	Estimated Design / Permitting / Construction Fee	Applicant's % Increase Thru Location	Applicant's Fair-Share Contribution
Post Occupancy Monitoring Study	\$ 8,500.00	100%	\$ 8,500.00
<i>Post Monitoring Study</i>	<i>\$ 4,400.00</i>	<i>100%</i>	<i>\$ 4,400.00</i>
<i>Traffic Counts</i>	<i>\$ 3,900.00</i>	<i>100%</i>	<i>\$ 3,900.00</i>
Route 140 / Grove Street	\$ 5,000.00	4%	\$ 200.00
<i>Signal Timing Adjustments</i>	<i>\$ 5,000.00</i>	<i>4%</i>	<i>\$ 200.00</i>
Washington Street / Grove Street	\$ 244,500.00	7%	\$ 17,200.00
<i>Signal Construction</i>	<i>\$ 170,600.00</i>	<i>7%</i>	<i>\$ 11,942.00</i>
<i>Grove Street Reconstruction</i>	<i>\$ 43,300.00</i>	<i>7%</i>	<i>\$ 3,031.00</i>
<i>Washington Street Reconstruction</i>	<i>\$ 23,700.00</i>	<i>7%</i>	<i>\$ 1,659.00</i>
<i>Striping Updates</i>	<i>\$ 6,500.00</i>	<i>7%</i>	<i>\$ 455.00</i>
TOTAL TOWN-FUNDED PROJECTS	\$ 258,000.00	--	\$ 25,900.00

Traffic Count Estiamte Worksheet

Project Number: 20163

Project Name: Franklin, MA - Proposed Marijuana Dispensary

Fill in Grey Areas Only

Inhouse Counts? N Y/N

Turning Movement Counts

Number of People Required: 1

Number of Time Periods: 3

Hours of counts needed: 7

Total Manhours: 7

Mileage from Wilmington: 57

Travel Time: 1

Total Travel Time: 3

Rate: \$70.00 /hr

Subtotal of TMC cost: \$700

Automatic Traffic Recorder

In house ATR count N Y/N

Number Required:

Days Needed: 3

Speeds: Y Y/N

Cost per ATR: \$400 with speeds

Total ATR cost: \$0

Coordinators Time

Is coordinator going to site? N Y/N

Travel Time: 0

Mileage Cost: \$0.00

Mileage Cost w/ Trips: \$0.00

Count Setup Time: 2

Count Download Time:

Inventory:

Subtotal of Coordinator Cost: \$130.00

Count Company Contact:

Proposal Sent: Y/N

Proposal Signed: Y/N

*Company Contacted: Y/N

*Count company should be contacted for both approvals and denials.

Total Cost of Counts: **\$830.00** \$1,000.00

1.2 factor applied

NOTES:

Traffic Count Estiamte Worksheet

Project Number: 20163
 Project Name: Franklin, MA - Proposed Marijuana Dispensary
 Inhouse Counts? N Y/N

Fill in Grey Areas Only

Turning Movement Counts

Number of People Required: 3
 Number of Time Periods: 3
 Hours of counts needed: 7

 Total Manhours: 29

 Mileage from Wilmington: 57
 Travel Time: 1

 Total Travel Time: 3

 Rate: \$70.00 /hr

Automatic Traffic Recorder

In house ATR count N Y/N

 Number Required:

 Days Needed: 3

 Speeds: Y Y/N

 Cost per ATR: \$400 with speeds

 Total ATR cost: \$0

Subtotal of TMC cost: \$2,240

Coordinators Time

Is coordinator going to site? N Y/N
 Travel Time: 0
 Mileage Cost: \$0.00
 Mileage Cost w/ Trips: \$0.00
 Count Setup Time: 2
 Count Download Time:
 Inventory:

Subtotal of Coordinator Cost: \$130.00

Count Company Contact:

Proposal Sent: Y/N
 Proposal Signed: Y/N
 *Company Contacted: Y/N

*Count company should be contacted for both approvals and denials.

Total Cost of Counts: **\$2,370.00** \$2,900.00 1.2 factor applied

NOTES:

JOB BUDGETING SHEET

Project:	Franklin, MA - Proposed Marijuana Dispensary
Project No.:	NEX-2020163.00
Date:	8/13/2020

Task	Heather Monticup	Rebecca Brown	Susie	Doug	Donald	Nicole	John Diaz	Tim Letton	TOTAL
Project Initiation/Kickoff		1		1					2
Traffic Study Outline									
Field Reconnaissance									
Revis									
Existing Conditions									
Site Location Map									
Geometrics									
Traffic Volumes				3					3
Collisions									
Public Transportation									
Pedestrian/Bicycle Access									
Traffic Signal Warrant		1		1					2
Existing Networks				1					1
No-Build Conditions									
Coordination with Town				1					1
Traffic Growth				1					1
No-Build Networks				1					1
Build Conditions									
Trip Gen./Distribution									
Build Networks									
Analysis									
Intersections		1		6					7
Roadways									
Drive-Through Queue									
Turn Lane Warrant Analysis									
Mitigation									
Identify/Discuss		2		2					4
Analysis				1					1
Concept Imp. Plans									
TDM Program									
Memo Preparation				4					4
QA/QC		4							4
Final Revisions		1		1					2
Miscellaneous				2					2
TOTAL HOURS:		10		25					35
BILLING RATE:	\$200.00	\$165.00	\$110.00	\$110.00	\$85.00	\$110.00	\$250.00	\$265.00	
COST:		\$1,650		\$2,750					\$4,400.00

Task not included as part of scope
Site is 60 minutes - 57 miles from Wilmington (good traffic)

- PM/SAT
• Grove St / Route 140
• Grove St / Washington St

Distribution for Options #1 and #2

* 0.5 hrs per intersection per time period per condition for analysis = (.5 hr x 2 intersections x 2 time periods for set-up) plus (0.5 hrs x 2 intersections x 2 time periods x 2 analysis condition for analysis) = 6 hours

TIAS = \$4,400
Concept =
TIAS Outline =
TOTAL = \$4,400

Budget: _____
Spent Thru XX/XX/XX: _____
Remaining: _____

GPI Calculations

Greenman-Pedersen, Inc.

Location Franklin, ma Job Number NEX-2020163.00
Title Signal Timing Adjustments
Calculated By DSH Checked By _____

Item Number	Quantity	Unit	Description	Unit Price	Total
Design	1	EA	Analysis of Intersections with Plans	\$5,000.00	\$ 5,000.00
				TOTAL	\$5,000.00
				Construction Contingency	\$0
				Construction Engineering	\$0
				Police Detail	\$0
				Utility Pole Relocation	\$0
				Total	\$5,000
				Estimate	\$5,000.00

QUICK ESTIMATE OF ROADWAY CONSTRUCTION COSTS
(Based on MassDOT Average Unit Prices)
As of 7/23/20

Grove Street - Basic Construction (no curbing or sidewalk)

 = Boxes to be revised

Roadway Width =		24	feet	Quantity (per foot) of road		Unit	Unit Price	Total	Notes
Item No.	Item	Quantity	Unit	of road	Unit	Unit Price	Total	Notes	
120.	Earth Excavation	16	In. (depth)	1.19	CY	\$35.00	\$41.48		
151.	Gravel Borrow	12	In. (depth)	1.11	CY	\$50.00	\$55.56	Includes 25% swell factor	
450.22	Superpave Surface Course	4	In. (depth)	0.60	T	\$95.00	\$56.75		
170.	Fine Grading	24	Ft. (width)	2.67	SY	\$6.00	\$16.00		
201.	Catch Basins	2	Each	0.0089	EA	\$4,225.00	\$37.56	Assume 2 per 225 feet	
202.	DMH	1	Each	0.0044	EA	\$4,235.00	\$18.82	Assume 1 per 225 feet	
241.15	15" REINFORCED CONC PIPE	1	Ft. (length)	1	FT	\$100.00	\$100.00	Assumed 15" average diameter for all pipes.	
300	Water Items	1	Ft. (length)	1	FT	\$120.00	\$120.00	Includes all mains, fittings, hydrants, etc.	
	Stormwater System (retention/detention, etc.)						\$44.62	Assumed 10% of Subtotal A	
	Underground Utilities						\$31.23	Assumed 7% of Subtotal A	
							\$522.01		
	All other items (bounds, trees, contingencies, etc.)						\$104.40	Assumed 20% of Subtotal B	
							\$826.41		
							Say \$630.00	per lin. ft	

Grove Street - Basic Mill & Overlay (no curbing or sidewalk)

Roadway Width =		24	feet	Quantity (per foot) of road		Unit	Unit Price	Total	Notes
Item No.	Item	Quantity	Unit	of road	Unit	Unit Price	Total	Notes	
415.2	Fine Milling	2	In. (depth)	0.22	SY	\$7.50	\$1.67		
450.23	Superpave Surface Course	2	In. (depth)	0.30	T	\$135.00	\$40.32		
201.	Catch Basins	2	Each	0.0089	EA	\$4,225.00	\$37.56	Assume 2 per 225 feet	
241.12	12" REINFORCED CONC PIPE	1	Ft. (length)	1	FT	\$106.00	\$106.00	Assumed 12" average diameter for all pipes.	
							\$186.00		
							Say \$186.00	per lin. ft	

Additional Items

Add Granite Curbing (per side)

Item No.	Item	Quantity	Unit	(per foot) of road	Unit	Unit Price	Total	Notes
482.3	Sawcutting Asphalt Pavement			1	FT	\$4.00	\$4.00	per lin. ft

Add Granite Curbing (per side)

Item No.	Item	Quantity	Unit	(per foot) of road	Unit	Unit Price	Total	Notes
506.	Granite Curb - Straight			1	FT	\$50.00	\$50.00	per lin. ft

Add Granite Edging (per side)

Item No.	Item	Quantity	Unit	(per foot) of road	Unit	Unit Price	Total	Notes
511.1	Granite Edging - Straight			1	FT	\$37.00	\$37.00	per lin. ft

Add 12" Asphalt Berm (per side)

Item No.	Item	Quantity	Unit	(per foot) of road	Unit	Unit Price	Total	Notes
570.2	Hot Mix Asphalt Curb, Type 2			1	FT	\$14.00	\$14.00	per lin. ft

Add HMA Sidewalk (per side)

Sidewalk Width =		5	feet	Quantity (per foot) of road		Unit	Unit Price	Total	Notes
Item No.	Item	Quantity	Unit	of road	Unit	Unit Price	Total	Notes	
120.	Earth Excavation	9	In. (depth)	0.14	CY	\$35.00	\$4.86		
151.	Gravel Borrow	6	In. (depth)	0.12	CY	\$50.00	\$5.79	Includes 25% swell factor	
702.	Asphalt Walk	3	In. (depth)	0.09	T	\$240.00	\$22.40		
							\$33.05		
							Say \$40.00	per lin. ft	

Add Cement Concrete Sidewalk (per side)

Sidewalk Width =		5	feet	Quantity (per foot) of road		Unit	Unit Price	Total	Notes
Item No.	Item	Quantity	Unit	of road	Unit	Unit Price	Total	Notes	
120.	Earth Excavation	12	In. (depth)	0.19	CY	\$35.00	\$6.48		
151.	Gravel Borrow	8	In. (depth)	0.15	CY	\$50.00	\$7.72	Includes 25% swell factor	
701.	Cement Conc. Walk	4	In. (depth)	0.56	SY	\$75.00	\$41.67		
							\$55.86		
							Say \$56.00	per lin. ft	

Examples: For a 24-foot road with edging and HMA sidewalk on one side, and only berm on the other, the cost per linear foot would be: **\$721.00** per lf

For a 24-foot road with curbing and cement concrete sidewalks on both sides, the cost per linear foot would be: **\$842.00** per lf

Assume 150 lf or reconstruction	\$27,900.00	
Roadway Mill & Overlay	\$96.00	
Sawcutting	\$7,500.00	assume new curbing for changed radii
Add Granite Curbing (per side)	\$560.00	assume 10' of new sidewalk on Westerly side
Add Cement Concrete Sidewalk (per side)		

Subtotal	\$36,056.00
20% Contingency	\$7,211.20
Total (Does not include utility relocation, traffic police, construction engineering)	\$43,267.20

QUICK ESTIMATE OF ROADWAY CONSTRUCTION COSTS
(Based on MassDOT Average Unit Prices)
As of 7/23/20

Washington Street - Basic Construction (no curbing or sidewalk)

 = Boxes to be revised

Roadway Width = 40 feet

Item No.	Item	Quantity	Unit	Quantity (per foot) of road	Unit	Unit Price	Total	Notes
120.	Earth Excavation	16	In. (depth)	1.98	CY	\$35.00	\$69.14	
151.	Gravel Borrow	12	In. (depth)	1.85	CY	\$50.00	\$92.59	Includes 25% swell factor
450.22	Superpave Surface Course	4	In. (depth)	1.00	T	\$95.00	\$94.58	
170.	Fine Grading	40	Ft. (width)	4.44	SY	\$6.00	\$26.67	
201.	Catch Basins	2	Each	0.0089	EA	\$4,225.00	\$37.56	Assume 2 per 225 feet
202.	DMH	1	Each	0.0044	EA	\$4,235.00	\$18.82	Assume 1 per 225 feet
241.15	15" REINFORCED CONC PIPE	1	Ft. (length)	1	FT	\$100.00	\$100.00	Assumed 15" average diameter for all pipes.
300	Water Items	1	Ft. (length)	1	FT	\$120.00	\$120.00	Includes all mains, fittings, hydrants, etc.
	Stormwater System (retention/detention, etc.)						\$55.94	Assumed 10% of Subtotal A
	Underground Utilities						\$39.15	Assumed 7% of Subtotal A
							\$654.44	
	All other items (bounds, trees, contingencies, etc.)						\$130.89	Assumed 20% of Subtotal B
							\$785.33	
	Say						\$790.00	per lin. ft

Washington Street - Basic Mill & Overlay (no curbing or sidewalk)

Item No.	Item	Quantity	Unit	Quantity (per foot) of road	Unit	Unit Price	Total	Notes
415.2	Fine Milling	2	In. (depth)	0.22	SY	\$7.50	\$1.67	
450.23	Superpave Surface Course	2	In. (depth)	0.50	T	\$135.00	\$67.20	
201.	Catch Basins	1	Each	0.0044	EA	\$4,225.00	\$18.78	Assume 2 per 225 feet
241.12	12" REINFORCED CONC PIPE	1	Ft. (length)	1	FT	\$106.00	\$106.00	Assumed 12" average diameter for all pipes.
	Say						\$194.00	per lin. ft

Additional Items

Add Granite Curbing (per side)

Item No.	Item	Quantity	Unit	Quantity (per foot) of road	Unit	Unit Price	Total	Notes
482.3	Sawcutting Asphalt Pavement			1	FT	\$4.00	\$4.00	per lin. ft

Add Granite Curbing (per side)

Item No.	Item	Quantity	Unit	Quantity (per foot) of road	Unit	Unit Price	Total	Notes
506.	Granite Curb - Straight			1	FT	\$50.00	\$50.00	per lin. ft

Add Granite Edging (per side)

Item No.	Item	Quantity	Unit	Quantity (per foot) of road	Unit	Unit Price	Total	Notes
511.1	Granite Edging - Straight			1	FT	\$37.00	\$37.00	per lin. ft

Add 12" Asphalt Berm (per side)

Item No.	Item	Quantity	Unit	Quantity (per foot) of road	Unit	Unit Price	Total	Notes
570.2	Hot Mix Asphalt Curb, Type 2			1	FT	\$14.00	\$14.00	per lin. ft

Add HMA Sidewalk (per side)

Sidewalk Width = 5 feet

Item No.	Item	Quantity	Unit	Quantity (per foot) of road	Unit	Unit Price	Total	Notes
120.	Earth Excavation	9	In. (depth)	0.14	CY	\$35.00	\$4.86	
151.	Gravel Borrow	6	In. (depth)	0.12	CY	\$50.00	\$5.79	Includes 25% swell factor
702.	Asphalt Walk	3	In. (depth)	0.09	T	\$240.00	\$22.40	
	Say						\$33.05	
							\$40.00	per lin. ft

Add Cement Concrete Sidewalk (per side)

Sidewalk Width = 5 feet

Item No.	Item	Quantity	Unit	Quantity (per foot) of road	Unit	Unit Price	Total	Notes
120.	Earth Excavation	12	In. (depth)	0.19	CY	\$35.00	\$6.48	
151.	Gravel Borrow	8	In. (depth)	0.15	CY	\$50.00	\$7.72	Includes 25% swell factor
701.	Cement Conc. Walk	4	In. (depth)	0.56	SY	\$75.00	\$41.67	
	Say						\$55.86	
							\$56.00	per lin. ft

Examples: For a 24-foot road with edging and HMA sidewalk on one side, and only berm on the other, the cost per linear foot would be: **\$881.00** per lf

For a 24-foot road with curbing and cement concrete sidewalks on both sides, the cost per linear foot would be: **\$1,002.00** per lf

Assume 100 lf or reconstruction (50' in each direction)

Roadway Mill & Overlay	\$19,400.00
Sawcutting	\$320.00

Assume that curblines stay the same

Subtotal	\$19,720.00
20% Contingency	\$3,944.00
Total (Does not include utility relocation, traffic police, construction engineering)	\$23,664.00

Assume 23 sf per Single Arrow

	Single Arrow				
Grove Street	2	23 SF			
Washington Street	2	23 SF			
<hr/>					
Total		92 SF		<u>Unit Cost</u>	<u>Total</u>
				\$30.00 SF	\$2,760.00

6 INCH DURABLE WET REFLECTIVE RECESSED WHITE LINE (THERMOPLASTIC)

	<u>SWL</u>			<u>Unit Cost</u>		<u>Total</u>
Grove Street	150	FT		\$1.00	FT	\$150.00
Washington Street	250	FT		\$1.00	FT	\$250.00

6 INCH DURABLE WET REFLECTIVE RECESSED YELLOW LINE (THERMOPLASTIC)

	<u>DYCL</u>			<u>Unit Cost</u>		<u>Total</u>
Grove Street	300	FT		\$1.00	FT	\$300.00
Washington Street	300	FT		\$1.00	FT	\$300.00











12 INCH DURABLE WET REFLECTIVE RECESSED WHITE LINE (THERMOPLASTIC)

	<u>STOP</u>		<u>Crosswalk</u>		<u>Unit Cost</u>		<u>Total</u>
Grove Street	24	FT	345	FT	\$3.00	FT	\$1,107.00
Washington Street	33	FT	160	FT	\$3.00	FT	\$579.00

<hr/>						
Subtotal						\$5,446.00
20% Contingency						\$1,089.20
Total						\$6,535.20

Lanes, Volumes, Timings
 3: Grove Street & Planet Fitness Driveway

2027 Build w/ Mitigation
 Timing Plan: Weekday PM

						
Lane Group	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (vph)	44	61	352	47	67	622
Future Volume (vph)	44	61	352	47	67	622
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width (ft)	11	11	12	12	12	12
Storage Length (ft)	0	100		0	0	
Storage Lanes	1	1		0	0	
Taper Length (ft)	25				25	
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00
Frt		0.850	0.984			
Flt Protected	0.950					0.995
Satd. Flow (prot)	1711	1531	1833	0	0	1853
Flt Permitted	0.950					0.995
Satd. Flow (perm)	1711	1531	1833	0	0	1853
Link Speed (mph)	20		40			40
Link Distance (ft)	478		359			344
Travel Time (s)	16.3		6.1			5.9
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Shared Lane Traffic (%)						
Sign Control	Stop		Free			Free

Intersection Summary

Area Type: Other
 Control Type: Unsignalized

HCM 6th TWSC
 3: Grove Street & Planet Fitness Driveway

2027 Build w/ Mitigation
 Timing Plan: Weekday PM

Intersection						
Int Delay, s/veh	2.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	44	61	352	47	67	622
Future Vol, veh/h	44	61	352	47	67	622
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	100	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	48	66	383	51	73	676











Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	1231	409	0	0	434
Stage 1	409	-	-	-	-
Stage 2	822	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	196	642	-	-	1126
Stage 1	671	-	-	-	-
Stage 2	432	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	176	642	-	-	1126
Mov Cap-2 Maneuver	176	-	-	-	-
Stage 1	671	-	-	-	-
Stage 2	387	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	20.4	0	0.8
HCM LOS	C		

Minor Lane/Major Mvmt	NBT	NBR	WBLn1	WBLn2	SBL	SBT
Capacity (veh/h)	-	-	176	642	1126	-
HCM Lane V/C Ratio	-	-	0.272	0.103	0.065	-
HCM Control Delay (s)	-	-	32.9	11.3	8.4	0
HCM Lane LOS	-	-	D	B	A	A
HCM 95th %tile Q(veh)	-	-	1.1	0.3	0.2	-

Lanes, Volumes, Timings
 3: Grove Street & Planet Fitness Driveway

2027 Build w/ Mitigation
 Timing Plan: Saturday MIDDAY

						
Lane Group	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (vph)	63	77	237	65	77	349
Future Volume (vph)	63	77	237	65	77	349
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width (ft)	11	11	12	12	12	12
Storage Length (ft)	0	100		0	0	
Storage Lanes	1	1		0	0	
Taper Length (ft)	25				25	
Lane Util. Factor	1.00	1.00	1.00	1.00	1.00	1.00
Frt		0.850	0.971			
Flt Protected	0.950					0.991
Satd. Flow (prot)	1711	1531	1809	0	0	1876
Flt Permitted	0.950					0.991
Satd. Flow (perm)	1711	1531	1809	0	0	1876
Link Speed (mph)	20		40			40
Link Distance (ft)	478		359			496
Travel Time (s)	16.3		6.1			8.5
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Heavy Vehicles (%)	2%	2%	2%	2%	2%	0%
Shared Lane Traffic (%)						
Sign Control	Stop		Free			Free

Intersection Summary

Area Type: Other
 Control Type: Unsignalized

HCM 6th TWSC
3: Grove Street & Planet Fitness Driveway

2027 Build w/ Mitigation
Timing Plan: Saturday MIDDAY

Intersection

Int Delay, s/veh	3.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	63	77	237	65	77	349
Future Vol, veh/h	63	77	237	65	77	349
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	100	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	0
Mvmt Flow	68	84	258	71	84	379

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	841	294	0	0	329
Stage 1	294	-	-	-	-
Stage 2	547	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	335	745	-	-	1231
Stage 1	756	-	-	-	-
Stage 2	580	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	306	745	-	-	1231
Mov Cap-2 Maneuver	306	-	-	-	-
Stage 1	756	-	-	-	-
Stage 2	530	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	14.8	0	1.5
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBR	WBLn1	WBLn2	SBL	SBT
Capacity (veh/h)	-	-	306	745	1231	-
HCM Lane V/C Ratio	-	-	0.224	0.112	0.068	-
HCM Control Delay (s)	-	-	20.1	10.4	8.1	0
HCM Lane LOS	-	-	C	B	A	A
HCM 95th %tile Q(veh)	-	-	0.8	0.4	0.2	-



**FRANKLIN PLANNING & COMMUNITY
DEVELOPMENT**

355 EAST CENTRAL STREET, ROOM 120
FRANKLIN, MA 02038-1352
TELEPHONE: 508-520-4907

MEMORANDUM

DATE: September 23, 2020
TO: Franklin Planning Board
FROM: Department of Planning and Community Development
RE: 340 East Central St
Special Permit & Site Plan

The DPCD has reviewed the above referenced Site Plan application for the Monday, September 28, 2020 Planning Board meeting and offers the following commentary:

General:

- The site is approximately 6.5 acres and is located at 340 East Central Street. The property is within the Commercial II Zoning District and Water Resource District, Assessor's Map 285 Lot 009.
- The applicant seeks approval to construct a 42,080+/- sq/ft of residential, 15,219+/-sq/ft of retail and 2,250+/- for a coffee shop.
- The Applicant is seeking the following Special Permits: Four stories and fifty feet building height under the Chapter 185 Attachment 9, Maximum Height of Building and Chapter 185 Attachment 3, Part II 2.16 to allow the use of a Vehicle Service Establishment.
- Applicant has their first public hearing with the Conservation Commission on August 27, 2020.

ZBA Variances Granted

1. Minimum rear yard setback of 26 feet where 30 feet is required 185 Attachment 9
2. Allow for Multi-Family or apartment residential use in the Commercial II district which is otherwise prohibited 185 Attachment 7

Waiver Request:

1. Chapter 185-21 (B) – To Allow 268 parking spaces where as 301 is required
2. Chapter 300 Section 11(B)(2)(a) – Minimum cover is 42 inches above the top of the pipe
3. Chapter 300 Section 11(B)(2)(a) – To allow HDPE be allowed for oil/water separator

Comments from previous meetings:

1. Building Height – Current Zoning allows the height of the building to be 50 feet. From ground level to the top of the roof the building is 62 feet. The definition of building height for gable, hip and gambrel is measured “the mean height between the eaves and ridge”.
 - a. From the ridge line to the top, it is 20 feet. The first 4 floors make up 42 feet. This only leaves 8 feet for the mean. It appears the building height is at 52 feet. The Board expressed concern at the last meeting and the applicant has not addressed the height of the building. ***Applicant has provided revised drawings showing the accurate height of the building.***
2. The Board requested additional plantings be installed along the West of the property line. No additional plantings have been submitted. ***Applicant has indicated there is not enough space along the property line to add plantings.***
3. The Board expressed concern about the building and patio being so close to the roadway. Applicant has provided a sketch plan showing the distances in feet.
4. The Board inquired about deliveries and access around the Building D. ***Applicant has indicated there is a door on the side for deliveries.***
5. Change curbing detail to be reinforced concrete. ***Applicant has changed on the plans, however, the details still need to be added.***

Suggested Special Conditions:

1. Details for the Reinforced Concrete Curb should be added to the plans prior to Endorsement
2. All units will be maximum 2 bedrooms each.
3. Color renderings and landscape plan shall be included in the endorsed set.
4. Any signage for the property will need to be submitted to Design Review Commission.

Records on File:

1. Application for Site Plan and Special Permit
2. Certificate of Ownership
3. Special Permit Criteria
4. Abutters certified mailing
5. Overview of Proposed project and Special Permit Findings
6. Site Plans
7. Traffic Study
8. Stormwater Management Plans

ROLE CALL VOTE:

This determination shall be in addition to the following specific findings:

Special Permit VOTE for USE: §185 Attachment 9, Maximum Height of Building and §185 Attachment 3 Part II 2.16, to allow the use of a Vehicle Service Establishment

If you vote NO on any of the following, please state reason why you are voting NO:

(a) Proposed project addresses or is consistent with neighborhood or Town need.

Anthony Padula	YES	NO	William David	YES	NO
Rick Power	YES	NO	Gregory Rondeau	YES	NO

(b) Vehicular traffic flow, access and parking and pedestrian safety are properly addressed.

Anthony Padula	YES	NO	William David	YES	NO
Rick Power	YES	NO	Gregory Rondeau	YES	NO

(c) Public roadways, drainage, utilities and other infrastructure are adequate or will be upgraded to accommodate development.

Anthony Padula	YES	NO	William David	YES	NO
Rick Power	YES	NO	Gregory Rondeau	YES	NO

(d) Neighborhood character and social structure will not be negatively impacted.

Anthony Padula	YES	NO	William David	YES	NO
Rick Power	YES	NO	Gregory Rondeau	YES	NO

(e) Project will not destroy or cause substantial damage to any environmentally-significant natural resource, habitat, or feature or, if it will, proposed mitigation, remediation, replication or compensatory measures are adequate.

Anthony Padula	YES	NO	William David	YES	NO
Rick Power	YES	NO	Gregory Rondeau	YES	NO

(f) Number, height, bulk, location and siting of building(s) and structure(s) will not result in abutting properties being deprived of light or fresh air circulation or being exposed to flooding or subjected to excessive noise, odor, light, vibrations, or airborne particulates.

Anthony Padula	YES	NO	William David	YES	NO
Rick Power	YES	NO	Gregory Rondeau	YES	NO

(g) Water consumption and sewer use, taking into consideration current and projected future local water supply and demand and wastewater treatment capacity, will not be excessive.

Anthony Padula	YES	NO	William David	YES	NO
Rick Power	YES	NO	Gregory Rondeau	YES	NO

The proposed use will not have adverse effects which overbalance its beneficial effects on either the neighborhood or the Town, in view of the particular characteristics of the site and of the proposal in relation to that site.

Anthony Padula	YES	NO	William David	YES	NO
Rick Power	YES	NO	Gregory Rondeau	YES	NO

STANDARD CONDITIONS OF APPROVAL

1. This Special Permit shall not be construed to run with the land and shall run with the Site Plan as endorsed by the Planning Board. A new Special Permit shall be required from the Planning Board if any major change of use or major change to the site plan is proposed.
2. This Special Permit shall lapse if a substantial use or construction has not begun, except for good cause, within twenty four (24) months of approval, unless the Board grants an extension.- No final Certificate of Occupancy shall be issued until all requirements of the Special Permit have been completed to the satisfaction of the Board unless the applicant has submitted a Partial Certificate of Completion for the remainder of the required improvements and received approval by the Planning Board. The applicant's engineer or surveyor, upon completion of all required improvements, shall submit a Certificate of Completion. The Board or its agent(s) shall complete a final inspection of the site upon filing of the Certificate of Completion by the applicant. Said inspection is further outlined in condition #4.
3. Construction or operations under this Special Permit shall conform to any subsequent amendment of the Town of Franklin Zoning Bylaw (§185) unless the use or construction is commenced within a period of six (6) months after the issuance of this Special Permit and, in cases involving construction, unless such construction is continued through to completion as continuously and expeditiously as is reasonable.
4. **The Planning Board will use outside consultant services to complete construction inspections upon the commencement of construction.** The Franklin Department of Public Works Director, directly and through employees of the Department of Public Works and outside consultant services shall act as the Planning Board's inspector to assist the Board with inspections necessary to ensure compliance with all relevant laws, regulations and Planning Board approved plan specifications. Such consultants shall be selected and retained upon a majority vote of the Board.
5. Actual and reasonable costs of inspection consulting services shall be paid by the owner/applicant before or at the time of the pre-construction meeting. Should additional inspections be required beyond the original scope of work, the owner/applicant shall be required to submit fees prior to the issuance of a Final Certificate of Completion by the Planning Board (Form H). Said inspection is further outlined in condition #4.
6. No alteration of the Special Permit and the plans associated with it shall be made or affected other than by an affirmative vote of the members of the Board at a duly posted meeting and upon the issuance of a written amended decision.
7. All applicable laws, by-laws, rules, regulations, and codes shall be complied with, and all necessary licenses, permits and approvals shall be obtained by the owner/applicant.
8. Prior to the endorsement of the site plan, the following shall be done:
 - The owner/applicant shall make a notation on the site plan that references the Special Permit and the conditions and dates of this Certificate of Vote.
 - A notation shall be made on the plans that all erosion mitigation measures shall be in place prior to major construction or soil disturbance commencing on the site.

- All outstanding invoices for services rendered by the Town's Engineers and other reviewing Departments of the Town relative to their review of the owner/applicant's application and plans shall have been paid in full.
 - The owner/applicant shall submit a minimum of six copies of the approved version of the plan.
9. Prior to any work commencing on the subject property, the owner/applicant shall provide plans to limit construction debris and materials on the site. In the event that debris is carried onto any public way, the owner/applicant and his assigns shall be responsible for all cleanup of the roadway. All cleanups shall occur within twenty-four (24) hours after first written notification to the owner/applicant by the Board or its designee. Failure to complete such cleanup may result in suspension of construction of the site until such public way is clear of debris.
 10. The owner/applicant shall install erosion control devices as necessary and as directed by the Town's Construction Inspector.
 11. **Prior to construction activities, there shall be a pre-construction meeting with the owner/applicant, and his contractor(s), the Department of Public Works and the Planning Board's Inspector.**
 12. Any signage requires the Applicant to file with the Design Review Commission.
 13. Prior to the endorsement, the Certificate of Vote and Order of Conditions shall be added to the Site Plans.